



Vincent Street
CESSNOCK

13 April 2017

ORDINARY MEETING OF COUNCIL

WEDNESDAY, 19 APRIL 2017

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PLANNING PROPOSAL

**Amendment to the
*Cessnock Local Environmental Plan 2011***

21 Main Road and 43 Main Road Cliftleigh

Rezoning of

Part Lot: 200 DP: 1196167 & Part Lot: 20 DP: 1175757

from

**RE1 - Public Recreation to R2 - Low Density
Residential**

Version 1

19 April 2017

Contact: Mr B Moshage

Telephone: 02 4993 4241

Email: bo.moshage@cessnock.nsw.gov.au

Planning Proposal – Cliftleigh RE1 Public Recreation to R2 Low Density Residential
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PART 1: OBJECTIVES and OUTCOMES

The intended outcome is for this planning proposal is to:

- Rezone that part of Lot 200 DP 1196167 and that part of Lot 20 DP 1175757 currently zoned RE1 - Public Recreation to R2 - Low Density Residential.
- Apply a minimum lot size of 450m²

The part of the lots that is the subject of this Planning Proposal is shown in Figure 1.

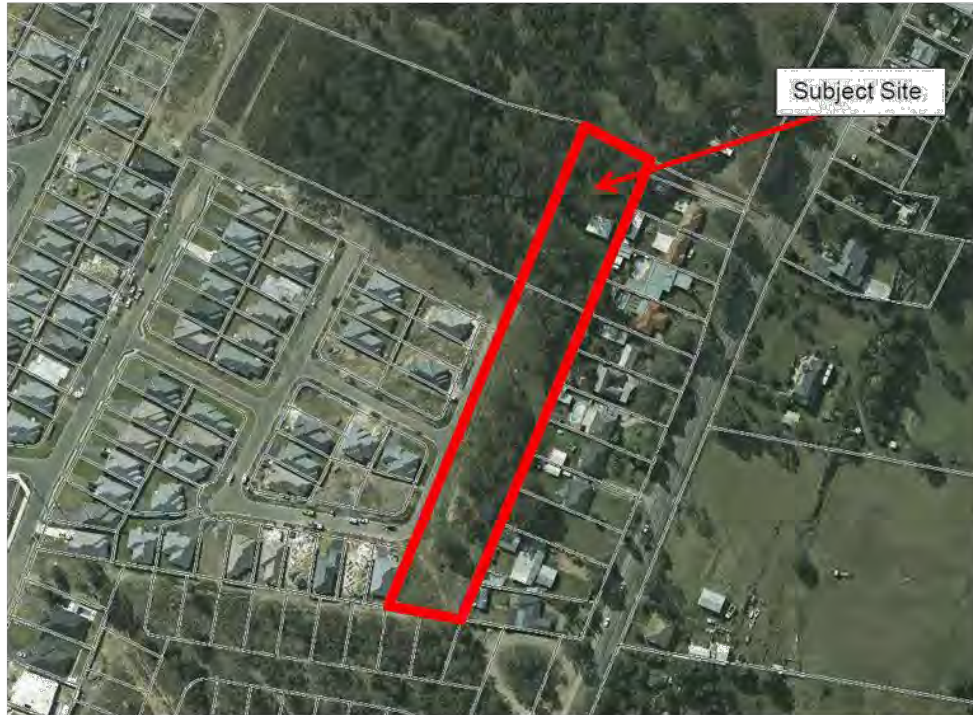


Figure 1: Locality Plan

The site is known as 21 and 43 Main Road, Cliftleigh, and can be legally identified as Lot 200 DP1196167 & Lot 20 DP1175757 respectively. The western portion of Lot 20 DP 1175757 is currently undeveloped bushland, it is zoned R2 - Low Density Residential and will ultimately be occupied by residential development.

It should be noted that while the land to the south of the subject site is shown as vegetated, the land has in fact been developed for residential purposes recently. Similarly, land to the north has also been cleared for residential development, being stage 9A which is anticipated to be registered later this year.

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PART 2: EXPLANATION of PROVISIONS

The Cessnock Local Environmental Plan 2011 is proposed to be amended in the following ways:

Land Zoning

Replace the RE1 - Public Recreation zoning with R2 - Low Density Residential on Land Zoning Map Sheet 1720_COM_LZN_009A_040_20150318 for that part of Lot 200 DP1196167 and Lot 20 DP1175757 as shown as Figure 1 in Appendix 2.

Lot Size

Amend the minimum lot size map to reflect a minimum lot size of 450m² on map sheet 1720_COM_LSZ_009A_040_20150518 for that part of Lot 200 DP1196167 and Lot 20 DP1175757 as shown as Figure 3 in Appendix 2.

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PART 3: JUSTIFICATION

In accordance with the Department of Planning and Environment's "Guide to Preparing Planning Proposals", this section provides a response to the following issues:

- Section A: Need for Proposal;
- Section B: Relationship to Strategic Planning Framework;
- Section C: Environmental, Social and Economic Impact; and
- Section D: State and Commonwealth Interests

Section A: Need for Proposal

1 Resulting from a Strategic Study or Report

The Planning Proposal seeks to rezone the subject site from RE1 - Public Recreation to R2 - Low Density Residential zone. It is proposed that an additional 13 allotments will be available for development within the Cliftleigh area if this Planning Proposal is pursued.

The RE1 zone was added by Council as part of the final consideration of the initial rezoning proposal. The site comprises a 40 metre wide strip between existing houses on the western side of Maitland Road to address concerns residence expressed in relation to the need for a visual screen to the (then) new Cliftleigh development. However, given the size of these lots and the reality that they are effectively surrounded by a residential zone, they were rezoned from rural to residential to be consistent with the zoning in the Cliftleigh URA. This makes Maitland Road the boundary between rural and residential zones in this location.

In determining the recreational and open space value of this land, the recommendations of Council's Recreation and Open Space Strategic Plan (ROSSP) 2009, Cycling Strategy 2016 and Recreation Needs Analysis (RNA) 2017 have been considered.

The subject site falls within the Kurri Kurri Planning Area, which includes Neath, Abermain, Weston, Mulbring, Heddon Greta and Cliftleigh, for which Council's ROSSP 2009 identifies a shortfall of recreation and open space facilities. However, the Cliftleigh URA provides an opportunity to address this, with the Testers Hollow District Park inclusive of an informal kick-a-bout area. In addition, regional sportsground facilities are anticipated to be delivered as part of the Hydro Planning Proposal, which will also address sporting facility shortfalls in this area.

Council's RNA aims to provide 'developed' open space within 500m of most residents. Developed open space includes passive, local, district and regional facilities. Figure 2 shows existing and proposed open space in the immediate vicinity of the subject site.

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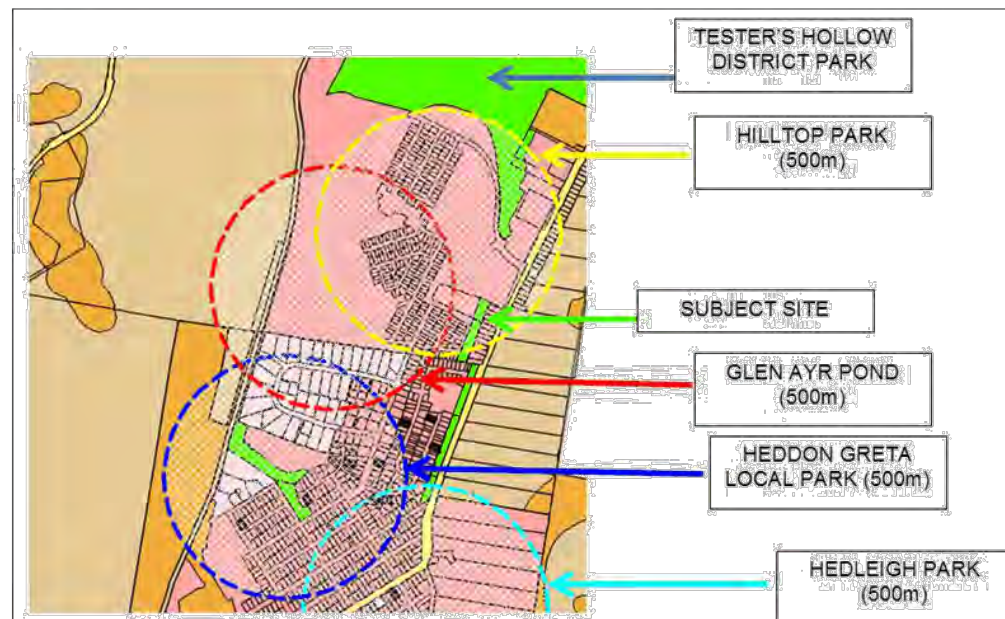


Figure 2: Existing and proposed developed open space

Whilst there is a portion of residents south of the subject site that fall outside the 500m open space area, it is considered that the broader planning area, incorporating Heddon Greta, Cliftleigh, Hedleigh and the proposed Hydro residential site, is currently or will be well serviced.

Notwithstanding these provisions, it is considered that the subject site is of limited recreational value due to the lineal shape not being ideal for the development as a local park in accordance with Open Space Design Guidelines of the ROSSP. Further, it is an isolated parcel of passive open space that has no connectivity to other open space corridors or existing and/or proposed cycleway networks and increases the potential for anti-social behaviour.

The difficulty with this site will be the ongoing management and maintenance of a relatively isolated parcel of open space of limited recreational value. While the site will have all weeds and rubbish removed by the developer prior to Council dedication, the ongoing maintenance, inclusive of all management costs, will subsequently fall to Council. This will have an impact on Council's existing maintenance program and current and future maintenance levels of open space in the area.

The exclusion of the subject site as open space will have minimal impact on the overall recreation provisions within the Cliftleigh urban precinct. In all, some 53 hectares (approximately 39% of the total Cliftleigh URA) has been designated for passive and active open space through the Testers Hollow District Park, Glen Ayr Pond Park and Hilltop Park.

2 Planning Proposal as best way to achieve to objectives

This planning proposal is the best means of achieving the objectives and intended outcomes. There are no other mechanisms to achieve the objectives or intended outcomes.

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Section B: Relationship to Strategic Planning Framework

3 Consistency with Objectives and Actions within Regional Strategies

Hunter Regional Plan 2016

The Planning Proposal is considered to be consistent with the objectives and actions of the Regional Plan.

4 Consistency with Council's Community Strategic Plan or other Local Strategic Plan

Community Strategic Plan - Our People, Our Place, Our Future

The Planning Proposal is consistent with Council's Operational Plan, in the better utilisation of existing open space:

- Our open spaces are distributed where people live.
- We have green corridors connecting our open space areas.
- We have high quality, centralised multi-purpose sporting and recreation facilities.

City Wide Settlement Strategy (2010)

The City Wide Settlement Strategy (CWSS) sets out strategic directions and implements a number of the outcomes and actions arising from the Lower Hunter Regional Strategy 2006.

There is no inconsistency between Council's CWSS and the Planning Proposal. The Cliftleigh URA has been approved for residential development since 2009.

Recreation and Open Space Strategic Plan (2009)

The Cessnock Recreation and Open Space Strategic Plan 2009 was introduced to assist Cessnock City Council to plan and manage the future development and maintenance needs of open space and recreation facilities throughout the Cessnock LGA. As part of this strategy, recreation supply throughout the LGA was examined. As a result, recommendations were provided regarding gaps in the provision of recreation settings.

It should be noted however, that the subject site is currently privately owned and thus it is unlikely this land was examined as part of the strategy. The land could be handed over to Council who would have ongoing maintenance liabilities; however, the proposal is considered a more efficient outcome given other open space areas in the locality and the wider Cliftleigh urban release area.

Recreation Needs Analysis (2017)

Council's Recreation Needs Analysis aims to provide 'developed' open space within 500m of most residents. Developed open space includes passive, local, district and regional facilities. Figure 2 shows existing and proposed open space in the immediate vicinity of the subject site.

5 Consistency with State Environmental Planning Policies

An assessment of relevant SEPPs against the planning proposal is provided in the table below.

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Table 1: Relevant State Environmental Planning Policies

SEPP	Relevance	Consistency and Implications
SEPP 14 – Coastal Wetlands	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 15 - Rural Land Sharing Communities	The SEPP provides for multiple occupancy development, with council consent, in rural and non-urban zones, subject to a list of criteria in the policy.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 19 – Bushland in Urban Areas	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 21 - Caravan Parks	The SEPP provides for development for caravan parks.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 26 – Littoral Rainforests	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 29 – Western Sydney Recreation Area	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 30 - Intensive Agriculture	The SEPP provides considerations for consent for intensive agriculture.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 32 - Urban Consolidation (Redevelopment of Urban Land)	The SEPP makes provision for the re-development of urban land suitable for multi-unit housing and related development.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 33 - Hazardous & Offensive Development	The SEPP provides considerations for consent for hazardous & offensive development.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 36 - Manufactured Homes Estates	The SEPP makes provision to encourage manufactured homes estates through permitting this use where caravan parks are permitted and allowing subdivision.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 39 – Spit Island Bird Habitat	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 44 - Koala Habitat Protection	This SEPP applies to land across NSW that is greater than 1 hectare and is not a National Park or Forestry Reserve. The SEPP encourages the conservation and management of natural vegetation areas that provide habitat for koalas to ensure permanent free-living populations will be maintained over their present range.	Consistent. The ecological investigations undertaken as part of the original rezoning and DA process for the Cliftleigh URA determined that the site did not constitute a "Potential Koala Habitat" under the provisions of SEPP 44
SEPP 47 – Moore Park Showground	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 50 - Canal Estate	The SEPP bans new canal estates from the date of gazettal,	Consistent. Nothing in the Planning Proposal impacts upon

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SEPP	Relevance	Consistency and Implications
Development	to ensure coastal and aquatic environments are not affected by these developments.	the operation of this SEPP
SEPP 52 – Farm Dams and Other works in Land and Water Management Plan Areas	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 55 - Remediation of Land	This SEPP applies to land across NSW and states that land must not be developed if it is unsuitable for a proposed use because of contamination	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 59 – Central Western Sydney Regional Open Space and Residential	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 62 - Sustainable Aquaculture	The SEPP relates to development for aquaculture and to development arising from the rezoning of land and is of relevance for site specific rezoning proposals.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 64 - Advertising and Signage	The SEPP aims to ensure that outdoor advertising is compatible with the desired amenity and visual character of an area, provides effective communication in suitable locations and is of high quality design and finish.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 65 - Design Quality of Residential Development	The SEPP relates to residential flat development across the state through the application of a series of design principles. Provides for the establishment of Design Review Panels to provide independent expert advice to councils on the merit of residential flat development.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 70 – Affordable Rental Housing (Revised Schemes)	The SEPP provides for an increase in the supply and diversity of affordable rental and social housing in NSW.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 71 – Coastal Protection	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP Affordable Rental Housing 2009	The aims of this Policy are as follows: (a) to provide a consistent planning regime for the provision of affordable rental housing, (b) to facilitate the effective	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP

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SEPP	Relevance	Consistency and Implications
	<p>delivery of new affordable rental housing by providing incentives by way of expanded zoning permissibility, floor space ratio bonuses and non-discretionary development standards,</p> <p>(c) to facilitate the retention and mitigate the loss of existing affordable rental housing,</p> <p>(d) to employ a balanced approach between obligations for retaining and mitigating the loss of existing affordable rental housing, and incentives for the development of new affordable rental housing,</p> <p>(e) to facilitate an expanded role for not-for-profit-providers of affordable rental housing,</p> <p>(f) to support local business centres by providing affordable rental housing for workers close to places of work,</p> <p>(g) to facilitate the development of housing for the homeless and other disadvantaged people who may require support services, including group homes and supportive accommodation.</p>	
SEPP Building Sustainability Index: BASIX 2004	The SEPP provides for the implementation of BASIX throughout the State.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP Exempt and Complying Development Codes 2008	The SEPP provides exempt and complying development codes that have State-wide application, identifying, in the General Exempt Development Code, types of development that are of minimal environmental impact that may be carried out without the need for development consent; and, in the General Housing Code, types of complying development that may be carried out in accordance with a complying development certificate.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP Housing for Seniors or People with a Disability	The SEPP aims to encourage provision of housing for seniors, including residential care	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP

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SEPP	Relevance	Consistency and Implications
2004	facilities. The SEPP provides development standards.	
SEPP Infrastructure 2007	The SEPP provides a consistent approach for infrastructure and the provision of services across NSW, and to support greater efficiency in the location of infrastructure and service facilities.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP (Kosciuszko National Park – Alpine Resorts) 2007	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Kurnell Peninsula) 1989	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP Major Development 2005	The SEPP defines certain developments that are major projects to be assessed under Part 3A of the Environmental Planning and Assessment Act 1979 and determined by the Minister for Planning. It also provides planning provisions for State significant sites. In addition, the SEPP identifies the council consent authority functions that may be carried out by Joint Regional Planning Panels (JRPPs) and classes of regional development to be determined by JRPPs.	
SEPP Mining, Petroleum Production and Extractive Industries 2007	The SEPP aims to provide proper management of mineral, petroleum and extractive material resources and ESD.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP Miscellaneous Consent Provisions 2007	The aims of this Policy are as follows: (a) to provide that the erection of temporary structures is permissible with consent across the State, (b) to ensure that suitable provision is made for ensuring the safety of persons using temporary structures, (c) to encourage the protection of the environment at the location, and in the vicinity, of temporary structures by specifying relevant matters for consideration,	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP

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SEPP	Relevance	Consistency and Implications
	(d) to provide that development comprising the subdivision of land, the erection of a building or the demolition of a building, to the extent to which it does not already require development consent under another environmental planning instrument, cannot be carried out except with development consent.	
SEPP Penrith Lakes Scheme 1989	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP Rural Lands 2008	The SEPP aims to facilitate economic use and development of rural lands, reduce land use conflicts and provides development principles.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP 53 Transitional Provisions 2011	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP State and Regional Development 2011	The SEPP aims to identify development and infrastructure that is State significant and confer functions on the Joint Regional Planning Panels (JRPPs) to determine development applications.	Consistent. Nothing in the Planning Proposal impacts upon the operation of this SEPP
SEPP (Sydney Drinking Water Catchment 2011)	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP Sydney Region Growth Centres 2006	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Three Ports_ 2013	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Urban Renewal) 2010	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Western Sydney Employment Area) 2009	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Western Sydney Parklands) 2009	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>

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6 Consistency with s.117 Ministerial Directions for Local Plan Making

An assessment of relevant s.117 Directions against the planning proposal is provided in the table below.

Table 2: Relevant s.117 Ministerial Directions

Ministerial Direction	Objective of Direction	Consistency and Implication
1. EMPLOYMENT AND RESOURCES		
1. Business and Industrial Zones	The objectives of this direction are to: (a) encourage employment growth in suitable locations, (b) protect employment land in business and industrial zones, and (c) support the viability of identified strategic centres.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
2. Rural Zones	The objective of this direction is to protect the agricultural production value of rural land.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
3. Mining, Petroleum Production and Extractive Industries	The objective of this direction is to ensure that the future extraction of State or regionally significant reserves coal, other minerals, petroleum and extractive materials are not compromised by inappropriate development.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
4. Oyster Aquaculture	<i>The objectives of this direction are: (a) to ensure that Priority Oyster Aquaculture Areas and oyster aquaculture outside such an area are adequately considered when preparing a planning proposal, (b) to protect Priority Oyster Aquaculture Areas and oyster aquaculture outside such an area from land uses that may result in adverse impacts on water quality and consequently, on the health of oysters and oyster consumers.</i>	<i>Not Applicable to LGA</i>
5. Rural lands	The objectives of this direction are to: (a) protect the agricultural production value of rural land,	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.

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Ministerial Direction	Objective of Direction	Consistency and Implication
	(b) facilitate the orderly and economic development of rural lands for rural and related purposes.	
2. ENVIRONMENT AND HERITAGE		
1. Environmental Protection Zones	The objective of this direction is to protect and conserve environmentally sensitive areas.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
2. Coastal Protection	<i>The objective of this direction is to implement the principles in the NSW Coastal Policy.</i>	<i>Not Applicable to LGA</i>
3. Heritage Conservation	The objective of this direction is to conserve items, areas, objects and places of environmental heritage significance and indigenous heritage significance.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
4. Recreation Vehicle Areas	The objective of this direction is to protect sensitive land or land with significant conservation values from adverse impacts from recreation vehicles.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
3. HOUSING, INFRASTRUCTURE AND URBAN DEVELOPMENT		
1. Residential Zones	The objectives of this direction are: (a) to encourage a variety and choice of housing types to provide for existing and future housing needs, (b) to make efficient use of existing infrastructure and services and ensure that new housing has appropriate access to infrastructure and services, and (c) to minimise the impact of residential development on the environment and resource lands.	Consistent – This Planning Proposal is considered to be consistent with this direction for the following reasons: <ul style="list-style-type: none">• Will broaden the choice of building and housing• Make efficient use of existing infrastructure and services• Not increase the urban fringe of Cliftleigh.
2. Caravan parks and Manufactured Home Estates	The objectives of this direction are: (a) to provide for a variety of housing types, and (b) to provide opportunities for caravan parks and manufactured home estates.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.

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Ministerial Direction	Objective of Direction	Consistency and Implication
3. Home Occupations	The objective of this direction is to encourage the carrying out of low-impact small businesses in dwelling houses.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
4. Integrating Land Use and Transport	The objective of this direction is to ensure that urban structures, building forms, land use locations, development designs, subdivision and street layouts achieve the following planning objectives: (a) improving access to housing, jobs and services by walking, cycling and public transport, and (b) increasing the choice of available transport and reducing dependence on cars, and (c) reducing travel demand including the number of trips generated by development and the distances travelled, especially by car, and (d) supporting the efficient and viable operation of public transport services, and (e) providing for the efficient movement of freight.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
5. Development Near Licensed Aerodromes	The objectives of this direction are: (a) to ensure the effective and safe operation of aerodromes, and (b) to ensure that their operation is not compromised by development that constitutes an obstruction, hazard or potential hazard to aircraft flying in the vicinity, and (c) to ensure development for residential purposes or human occupation, if situated on land within the Australian Noise Exposure Forecast (ANEF) contours of between 20 and 25, incorporates appropriate mitigation measures so that the development is not adversely affected by aircraft noise.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.

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Ministerial Direction	Objective of Direction	Consistency and Implication
6. Shooting Ranges	The objectives are: (a) to maintain appropriate levels of public safety and amenity when rezoning land adjacent to an existing shooting range, (b) to reduce land use conflict arising between existing shooting ranges and rezoning of adjacent land, (c) to identify issues that must be addressed when giving consideration to rezoning land adjacent to an existing shooting range.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
4. HAZARD AND RISK		
1. Acid Sulfate Soils	The objective of this direction is to avoid significant adverse environmental impacts from the use of land that has a probability of containing acid sulphate soils	The subject site is not affected by acid sulphate soils.
2. Mine Subsidence and Unstable Land	The objective of this direction is to prevent damage to life, property and the environment on land identified as unstable or potentially subject to mine subsidence.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
3. Flood Prone Land	The objectives of this direction are: (a) to ensure that development of flood prone land is consistent with the NSW Government's Flood Prone Land Policy and the principles of the Floodplain Development Manual 2005, and (b) to ensure that the provisions of an LEP on flood prone land is commensurate with flood hazard and includes consideration of the potential flood impacts both on and off the subject land.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
4. Planning for Bushfire Protection	The objectives of this direction are: (a) to protect life, property and the environment from bush fire hazards, by discouraging the establishment of	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.

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Ministerial Direction	Objective of Direction	Consistency and Implication
	incompatible land uses in bush fire prone areas, and (b) to encourage sound management of bush fire prone areas.	
5. REGIONAL PLANNING		
1. Implementation of Regional Strategies	The objective of this direction is to give legal effect to the vision, land use strategy, policies, outcomes, and actions contained in regional strategies.	There is no inconsistency between the Planning Proposal and the objectives or actions of the Hunter Regional Strategy.
2. Sydney Drinking Water Catchment	<i>The objective of this Direction is to protect water quality in the Sydney drinking water catchment.</i>	<i>Not Applicable to LGA</i>
3. Farmland of State and Regional Significance on the NSW Far North Coast	<i>The objectives of this direction are: (a) to ensure that the best agricultural land will be available for current and future generations to grow food and fibre, (b) to provide more certainty on the status of the best agricultural land, thereby assisting councils with their local strategic settlement planning, and (c) to reduce land use conflict arising between agricultural use and non-agricultural use of farmland as caused by urban encroachment into farming areas.</i>	<i>Not Applicable to LGA</i>
4. Commercial and Retail Development along the Pacific Highway, North Coast	<i>The objectives for managing commercial and retail development along the Pacific Highway are: (a) to protect the Pacific Highway's function, that is to operate as the North Coast's primary inter- and intra-regional road traffic route; (b) to prevent inappropriate development fronting the highway (c) to protect public expenditure invested in the Pacific Highway, (d) to protect and improve highway safety and highway efficiency,</i>	<i>Not Applicable to LGA</i>

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Ministerial Direction	Objective of Direction	Consistency and Implication
	<p>(e) to provide for the food, vehicle service and rest needs of travellers on the highway, and</p> <p>(f) to reinforce the role of retail and commercial development in town centres, where they can best serve the populations of the towns.</p>	
5. Development in the vicinity of Ellalong, Paxton and Millfield (Cessnock LGA)	(Revoked 18 June 2010)	No longer applicable to the LGA.
6. Sydney to Canberra Corridor	(Revoked 10 July 2008. See amended Direction 5.1)	Not Applicable to LGA
7. Central Coast	(Revoked 10 July 2008. See amended Direction 5.1)	Not Applicable to LGA
8. Second Sydney Airport: Badgerys Creek	The objective of this direction is to avoid incompatible development in the vicinity of any future second Sydney Airport at Badgerys Creek.	Not Applicable to LGA
9. North West Rail Link Corridor Strategy	<p>The objectives of this direction are to:</p> <p>(a) promote transit-oriented development and manage growth around the eight train stations of the North West Rail Link (NWRL)</p> <p>(b) ensure development within the NWRL corridor is consistent with the proposals set out in the NWRL Corridor Strategy and precinct Structure Plans.</p>	Not Applicable to LGA
6. LOCAL PLAN MAKING		
1. Approval and Referral Requirements	The objective of this direction is to ensure that LEP provisions encourage the efficient and appropriate assessment of development.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
2. Reserving Land for Public Purposes	<p>The objectives of this direction are:</p> <p>(a) to facilitate the provision of public services and facilities by reserving land for public purposes, and</p> <p>(b) to facilitate the removal of</p>	<p>Inconsistent - as it proposes to alter and reduce exiting zonings and reservations of land for public purposes.</p> <p>The inconsistency is considered to be of a minor significance as the</p>

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Ministerial Direction	Objective of Direction	Consistency and Implication
	reservations of land for public purposes where the land is no longer required for acquisition.	<p>property has little public benefit in being retained for public purposes if the land were transferred to Cessnock City Council.</p> <p>While the subject site is zoned RE1 Public Recreation, it should be noted that the land is currently privately owned and not used for public recreation purposes.</p> <p>The site is an isolated area of open space. The proposed rezoning is considered a more efficient outcome given other open space areas in the locality see Figure 2.</p> <p>It is considered that the proposed rezoning is consistent with the surrounding development, will reduce the potential for anti-social behaviour on the site and will minimise the potential for Council maintenance costs if the ownership of the land were transferred.</p>
3. Site Specific Provisions	The objective of this direction is to discourage unnecessarily restrictive site specific planning controls.	Consistent. Nothing in this Planning Proposal impacts upon the operation of this Direction.
7. Metropolitan Planning		
1. Implementation of A Plan for Growing Sydney	<i>The objective of this direction is to give legal effect to the planning principles; directions; and priorities for subregions, strategic centres and transport gateways contained in A Plan for Growing Sydney.</i>	Not Applicable to LGA
1.1 Implementation of Greater Macarthur Land Release Investigation	<i>The objective of this direction is to ensure development within the Greater Macarthur Land Release Investigation Area is consistent with the Greater Macarthur Land Release Preliminary Strategy and Action Plan (the Preliminary Strategy).</i>	Not Applicable to LGA
1.2 Parramatta Road Corridor Urban Transformation Strategy	<i>The objectives of this Direction are to:</i> (a) <i>facilitate development within the Parramatta Road Corridor that is</i>	Not Applicable to LGA

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Ministerial Direction	Objective of Direction	Consistency and Implication
	<p><i>consistent with the Parramatta Road Corridor Urban Transformation Strategy (November, 2016) and the Parramatta Road Corridor Implementation Tool Kit,</i></p> <p><i>(b) provide a diversity of jobs and housing to meet the needs of a broad cross-section of the community, and</i></p> <p><i>(c) guide the incremental transformation of the Parramatta Road Corridor in line with the delivery of necessary infrastructure</i></p>	

Planning Proposal – Cliftleigh RE1 Public Recreation to R2 Low Density Residential

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Section C: Environmental, Social and Economic Impact

7 *Impact on Threatened Species*

The EEC the subject of this Planning Proposal was offset under the original VPA biodiversity agreement with the OEH.

8 *Environmental Impact*

There are two on site Endangered Ecological Communities (EEC) - Lower Hunter Spotted Gum - Ironbark Forest and Kurri Sand Swamp Woodland. However, the removal of this vegetation has previously been offset under a biodiversity agreement with the (then) Department of Environment and Climate Change (now) Office of Environment and Heritage (OEH) associated with the original rezoning of for the Cliftleigh URA in 2008.

As a vegetation removal offset, it was agreed that the developer would contribute \$400,000.00 to allow the OEH to implement the Lower Hunter Spotted Gum - Ironbark Forest Recovery Plan Development and Implementation Plan.

9 *Social and Economic Impacts*

It is not considered that the Planning Proposal will conflict with the current residential land uses on the adjacent lands, as the proposed zoning is the same R2 - Low Density Residential zone.

Planning Proposal – Cliftleigh RE1 Public Recreation to R2 Low Density Residential

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Section D: State and Commonwealth Interests

10 Adequate Public Infrastructure

The Planning Proposal will not generate demand for additional public infrastructure as the Cliftleigh URA area has adequate infrastructure to support the proposal.

11 Consultation with State and Commonwealth Authorities

Consultation with State and Commonwealth public authorities will be undertaken in accordance with the Gateway Determination.

It is not proposed to consult with any authorities due to the nature and content of this Planning Proposal.

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PART 4: MAPPING

To achieve the intent of the Planning Proposal, it is proposed to amend the following map sheets:

Land Zoning

Replace the RE1 - Public Recreation zoning with R2 - Low Density Residential on map sheet 1720_COM_LZN_009A_040_20150318 for that part of Lot 200 DP1196167 and Lot 20 DP1175757 as shown as Figure 1 in Appendix 2.

Lot Size

Amend the minimum lot size map to reflect a minimum lot size of 450m² on map sheet 1720_COM_LSZ_009A_040_20150518 for that part of Lot 200 DP1196167 and Lot 20 DP1175757 as shown as Figure 3 in Appendix 2.

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PART 5: COMMUNITY CONSULTATION

The Planning Proposal is considered to be a low impact proposal and is proposed to be publicly exhibited for a minimum of 14 days in accordance with the Department's LEP Guide "A guide to preparing local environmental plans".

A low impact planning proposal is a planning proposal that is:

- consistent with the pattern of surrounding land use zones and/or land uses;
- consistent with the strategic planning framework;
- presents no issues with regard to infrastructure servicing;
- not a principal Local Environmental Plan;
- does not reclassify public land.

All adjoining property in addition to property owners in Tarrango Street, Taminga Road, and Cienna Street will be notified in writing of the Planning Proposal.

The exhibition material will be available from the following locations:

- Council's administration building;
- Cessnock Public Library;
- Kurri Kurri Public Library; and
- Council's Website at www.cessnock.nsw.gov.au

It is not proposed to consult with any government authorities or agencies due to the minor nature of this Planning Proposal.

The Department's Gateway determination may make prescriptions relating to government authority and/or agency consultation.

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PART 6: PROJECT TIMELINE

The Project Timeline will assist with tracking the progress of the Planning Proposal through the various stages of consultation and approval.

It is estimated that this amendment to the Cessnock Local Environmental Plan 2011 will be completed by November 2017.

Technical Studies have not been identified as a component of the Planning Proposal. The Department's Gateway determination may make prescriptions relating to technical studies and this may impact on the estimated completion date

	April 2017	June 2017	July 2017	Aug 2017	Sept 2017	Oct 2017	Nov 2017
STAGE 1 Submit Planning Proposal to DoP&E							
STAGE 2 Receive Gateway Determination							
STAGE 3 Preparation of documentation for Public Exhibition							
STAGE 4 Public Exhibition							
STAGE 5 Reviewal / consideration of submissions							
STAGE 6 Report to Council							
STAGE 7 Finalise Planning Proposal / request to the DoP&E that the amendment is made							

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Appendix 1: Council Report and Minutes

To be included following Council's consideration of the matter

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Appendix 2: Mapping Amendments

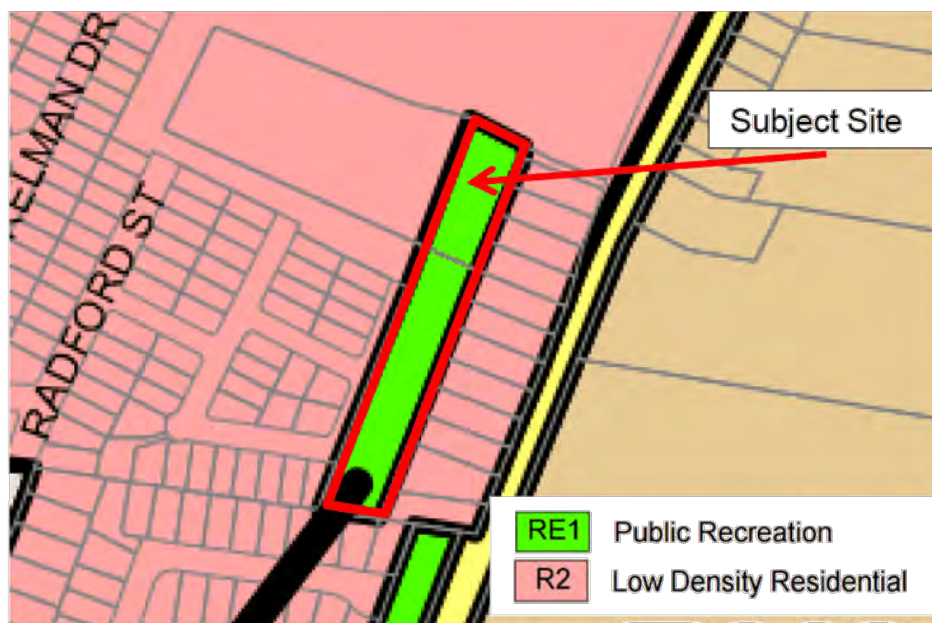


Figure 1 - Existing Zoning

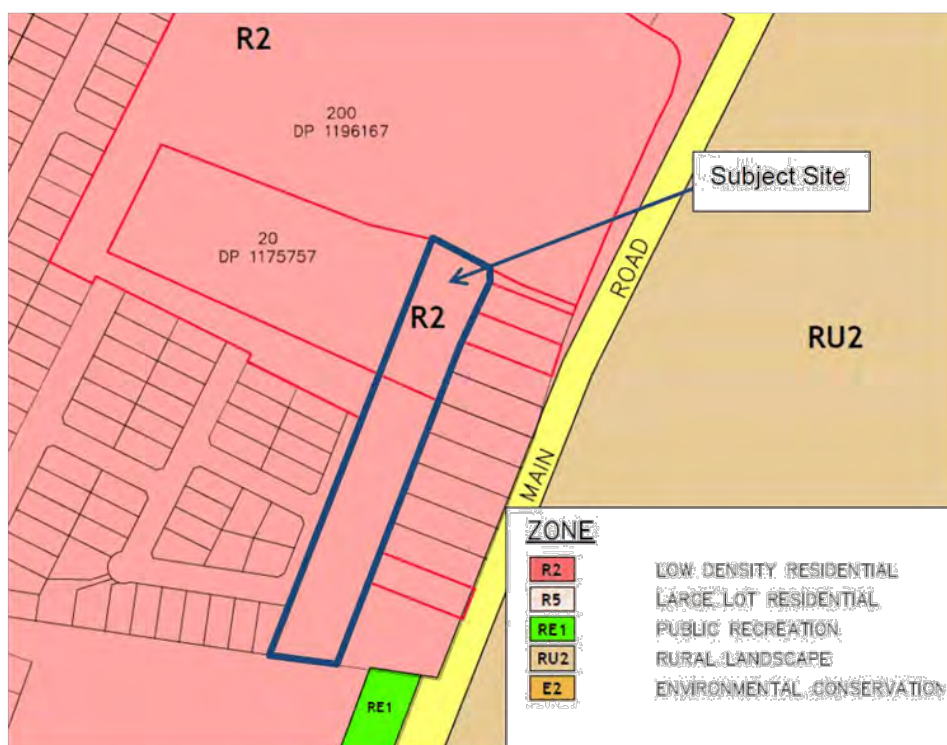


Figure 2 - Proposed Zoning

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Figure 3 - Existing Lot Size



Figure 4 - Proposed Lot Size

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CESSNOCK DEVELOPMENT CONTROL PLAN 2010

PART E – SPECIFIC AREAS



E16: CESSNOCK COMMERCIAL PRECINCT

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16.1.1 Overview

This Chapter of Cessnock Development Control Plan 2010 provides guidelines for the development of land comprising the 'Cessnock Commercial Precinct' as shown in **Figure 1**.

Cessnock is a Major Regional Centre that performs a multifaceted role as a service hub and as a gateway to the Hunter Region and surrounding National Parks.

The Commercial Precinct is used by residents, workers and visitors as an important centre for civic functions, business, employment, education, community facilities and services.

Visitors seek out Cessnock as an attractive and convenient place to stop on their journey, as a day trip destination, and also as a base from which to explore the many attractions of the Hunter Region.

16.1.2 Vision for the Cessnock Commercial Precinct

The Cessnock Commercial Precinct is 'The Gateway to Hunter Valley Wine Country' that celebrates the cultural landscape of the region and provides a quality environment where the community is actively encouraged to participate in community and civic life.

The Gateway to the Hunter vision builds upon the notion of the Lower Hunter Region as a notable productive landscape—a 'Wine Country' for its residents and surrounding cities and towns.

The region boasts a complex rural economy driven largely by livestock grazing, poultry farming and protected, broad acre and cultivated cropping and the region has established a sound international reputation for wine making and viticulture—a reputation which has impelled a thriving tourism industry alongside it.

Fulfilling its role as a regional centre, the City of Cessnock is a key location within the region as the first point of contact for visitors to the region from the south. Its location provides visitors to the region with a place to stop, orient and refresh themselves before setting off to explore the offerings of the Hunter's Wine Country. It also offers a base camp for those wishing to stay in the region longer, a place to eat, sleep and be entertained.

Cessnock has the opportunity to capitalise on the agricultural and viticulture identity of the region by incorporating the idea of the 'productive landscape' into its branding and open space strategy.

16.1.3 Chapter Objectives

The principal development objectives of this Chapter are to:

- (a) Facilitate redevelopment of the Cessnock Commercial Precinct as an important regional centre and tourist destination.
- (b) Ensure high quality design of local retail and businesses to activate the Main Street and surrounding retail precinct.
- (c) Ensure appropriate built form of new retail, commercial and residential development through building design, height, layout and setback controls that encourage development, yet are sensitive to existing heritage sites.

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- (d) Implement the Cessnock Commercial Precinct Public Domain Plan to provide for an accessible, safe, connected and enjoyable public domain.
- (e) Encourage and facilitate sustainable transport within the Cessnock Commercial Precinct including walking, cycling and public transport.
- (f) Encourage environmentally sustainable design for all new development.
- (g) Identify and incentivise the development of 'Catalyst Sites' that have the potential to transform the Commercial Precinct.
- (h) Provide objectives and controls that complement and supplement those of the other Chapters in the Cessnock Development Control Plan 2010.
- (i) Implement the Cessnock Local Environmental Plan 2011.

16.1.4 Application

This locality-specific Chapter applies to Cessnock Commercial Precinct, which is identified in Figure 1. This Chapter forms part of the Cessnock Development Control Plan 2010 that covers the whole Local Government Area (LGA). It consists of the written statement and plans referred to in this document.

This Chapter was adopted by Council on insert date.

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Figure 1: Cessnock Commercial Precinct

16.1.5 Purpose of this Chapter

The purpose of this Chapter is to give detailed guidance for development within the Cessnock Commercial Precinct. It provides detailed provisions specific to the Precinct that support the land use provisions provided in the Cessnock Local Environmental Plan (CLEP) 2011.

Council will consider the provisions of this Chapter in determining development applications. At Council's discretion, Council may consent to an application that departs from the provisions of this Chapter. Where this occurs, applications should be

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accompanied by a comprehensive written justification.

16.1.6 Relationship to other Plans and Chapters of CDCP 2010

This Chapter supplements the provisions of the CLEP 2011 and the other Chapters of the CDCP 2010.

This Chapter should be read in conjunction with all relevant Chapters of the CDCP 2010, including:

- Chapter C.1 Parking and Access;
- Chapter C.6 Access and Mobility;
- Chapter C.8 Social Impact Assessment and Crime Prevention through Environment Design (CPTED)
- Chapter D.2 Urban Housing;
- Chapter D.3 Industrial Development;
- Chapter D.5 Outdoor Signage;
- Chapter D.8 Temporary Events;
- Chapter D.9 Outdoor Dining;
- Chapter D.12 Heritage Conservation and Design Guidelines; and
- Any other relevant Council policies, including the Cessnock Commercial Precinct Public Domain Plan.

Where there is an inconsistency between this Chapter and other Chapters of CDCP 2010, the provisions of this Chapter prevail. If a development application has been made before the commencement of this Chapter in relation to land to which this Chapter applies and the application has not been finally determined before that commencement, the application must be determined as if this Chapter had not commenced.

Where there is any inconsistency between this Chapter and any environmental planning instrument that applies to the land, the provisions of the instrument prevail. An environmental planning instrument includes a State Environmental Planning Policy (SEPP), a Regional Environmental Plan (REP) or a Local Environmental Plan (LEP).

16.1.7 Structure Plan and Thematic Precincts

The Structure Plan and the Thematic Precincts Map at Figure 2 and Figure 3, respectively, have been developed to provide a considered, holistic scheme for development in the Commercial Precinct. These plans identify the basic development principles that apply to the land in the Commercial Precinct and functions as a guide for individual developments to enable them to contribute to the vision for the area.

Objective

- (a) Ensure development accords with the overall strategic vision for the Cessnock Commercial Precinct as outlined in the Structure Plan and Thematic Precinct Map.

Control

1. Development is to be generally consistent with the Structure Plan at Figure 2 and the Thematic Precincts Map at Figure 3.

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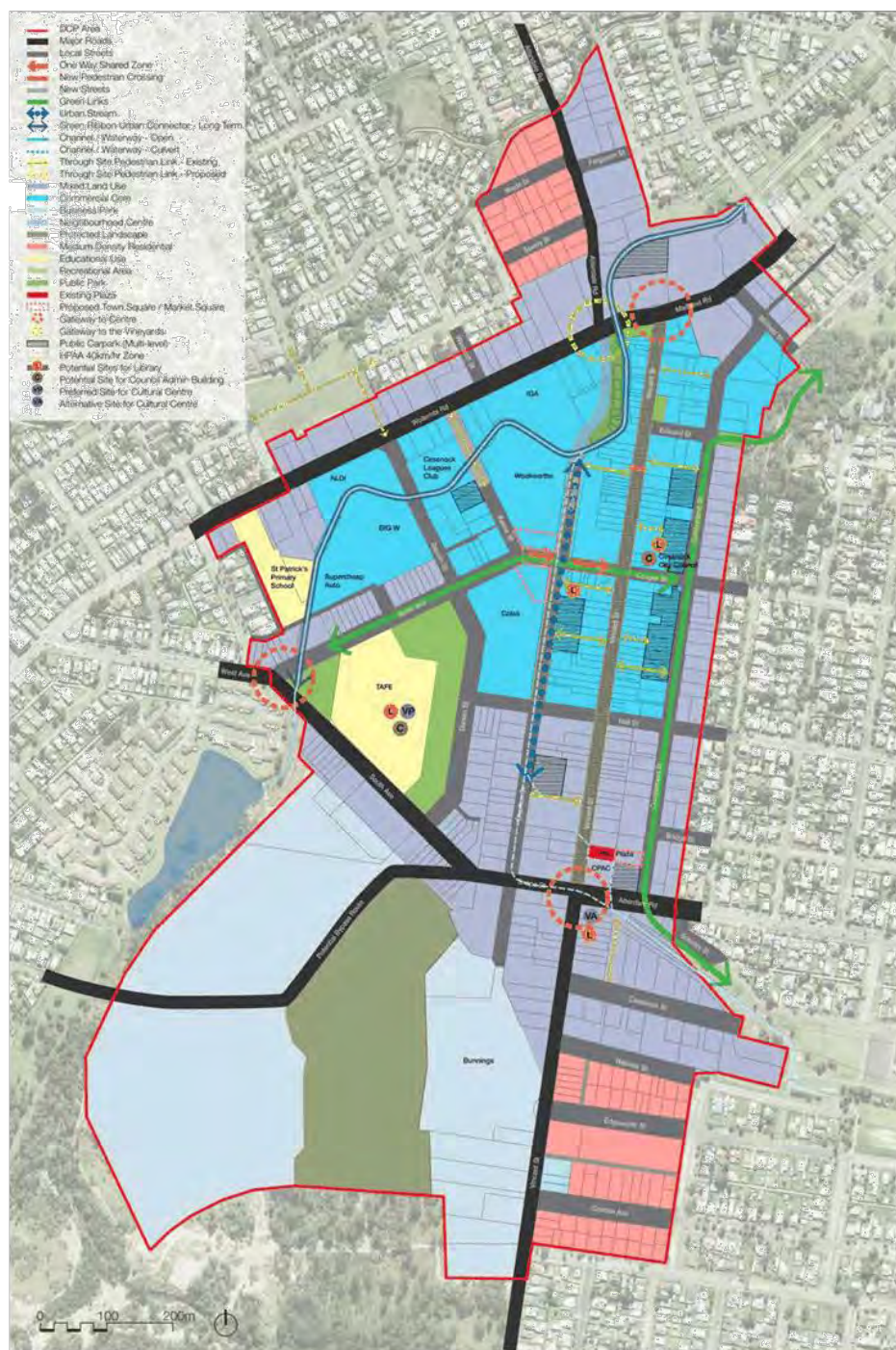


Figure 2: Structure Plan

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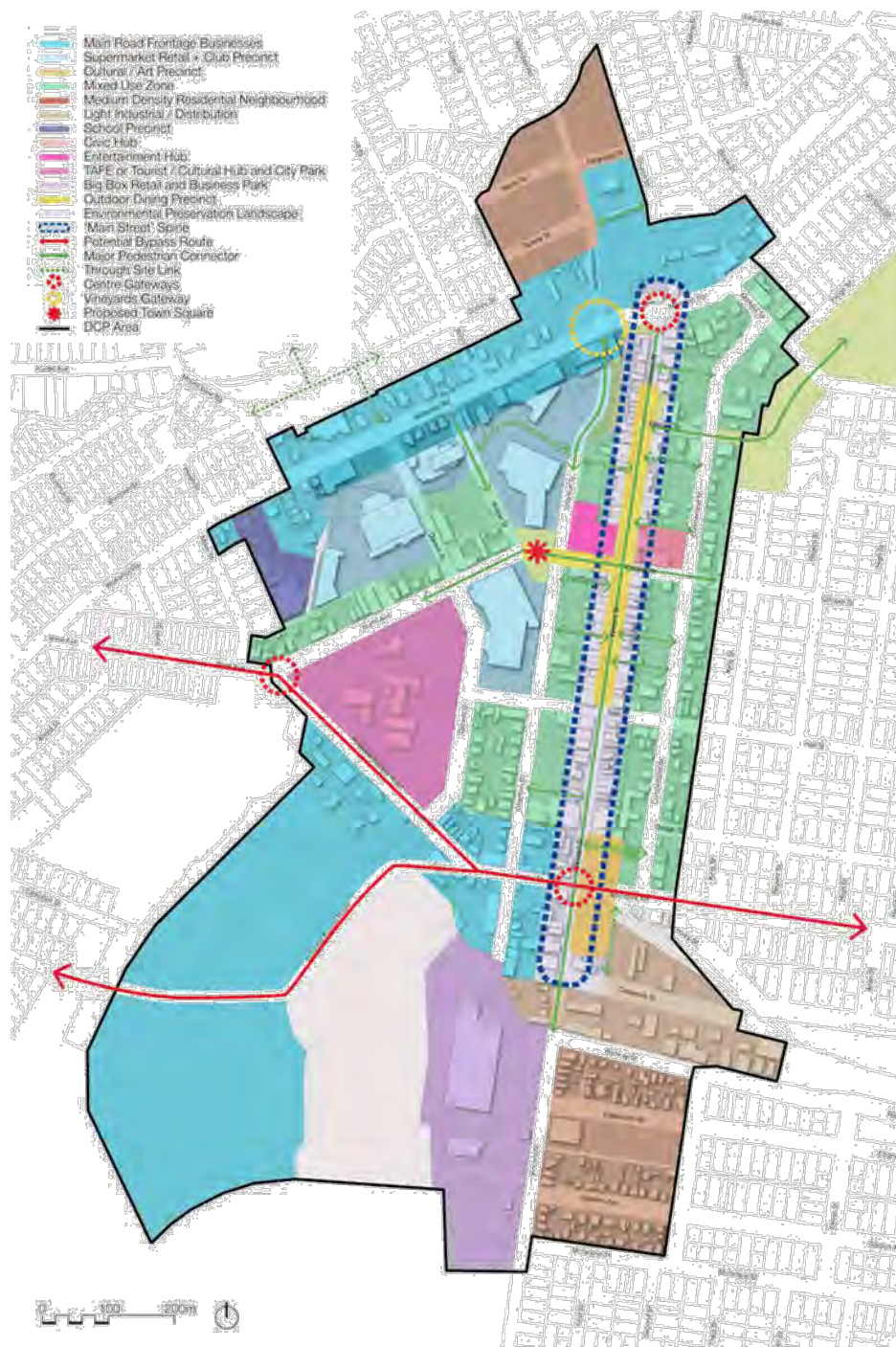


Figure 3: Thematic Precincts Map

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Part 1 – General Development Controls

16.1.8 Design Quality: Addressing the Street and Public Domain

High quality street frontages are important for achieving the desired character of the Commercial Precinct. Desirable design elements include attractive building entries, window displays, display cases, artworks, well-detailed architecture, facade modulation and clear glazed windows.

Objectives

- (a) Provide fine grain, high quality tenancy frontages that address the street and public domain in a positive manner.
- (b) Minimise and ameliorate the effect of blank walls to the street and public domain.
- (c) Reinforce street edge conditions that contribute to local heritage character.

Controls

- 1. Entries to retail and commercial premises are to be legible and accessible from the public domain.
- 2. Development with a blank wall is not acceptable. If no other design outcome is possible due to site constraints, walls are to minimise size and impact and be complimentary to the streetscape.
- 3. Corner buildings fronting a laneway or publicly accessible through-site link are to be designed to include elements that open out to or overlook the laneway (e.g. windows, entrance points, design articulation, murals).



Precedent images: public domain, laneway and through-site link treatments

16.1.9 Active Frontages and Awnings

Objectives

- (a) Promote lively, active streets in the Commercial Centre.
- (b) Ensure that development contributes to the quality, activity, safety and amenity of streets and public domain.
- (c) Provide for comfortable pedestrian environment through the provision of awnings.

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- (d) Contribute to the vitality of streets by maximising entries and display windows to retail uses and minimising blank walls.

Controls

1. Active frontages are to be provided in the locations nominated on the Active Frontages Map at Figure 4.
2. Active frontages are to be designed in accordance with the following criteria:
 - a. The ground floor level at the same level as the footpath.
 - b. At minimum, 70% of the ground floor frontage is to be transparent glazing.
 - c. Foyer spaces are not to occupy more than an 8m-width of a street frontage.
 - d. Enclosed glazed shopfronts are preferred to open shopfronts, except for food and drink premises, which are encouraged to provide open shopfronts.
 - e. Security grilles may only be fitted internally behind the shopfront and are to be fully retractable and at least 50% transparent when closed.
3. Driveways are not permitted on active frontages (refer to Figure 4) unless there is no alternative, and it is justified that the driveway would have no unacceptable impacts on pedestrian safety.
4. Awnings are to be provided on all required active street frontages, as indicated on the Active Frontages Map at Figure 4.



Precedent images: active frontages

Note: An active frontage is not required for any part of a building that is used for any of the following:

- a) entrances and lobbies (including as part of mixed use development);
- b) access for fire services; and
- c) vehicle access.

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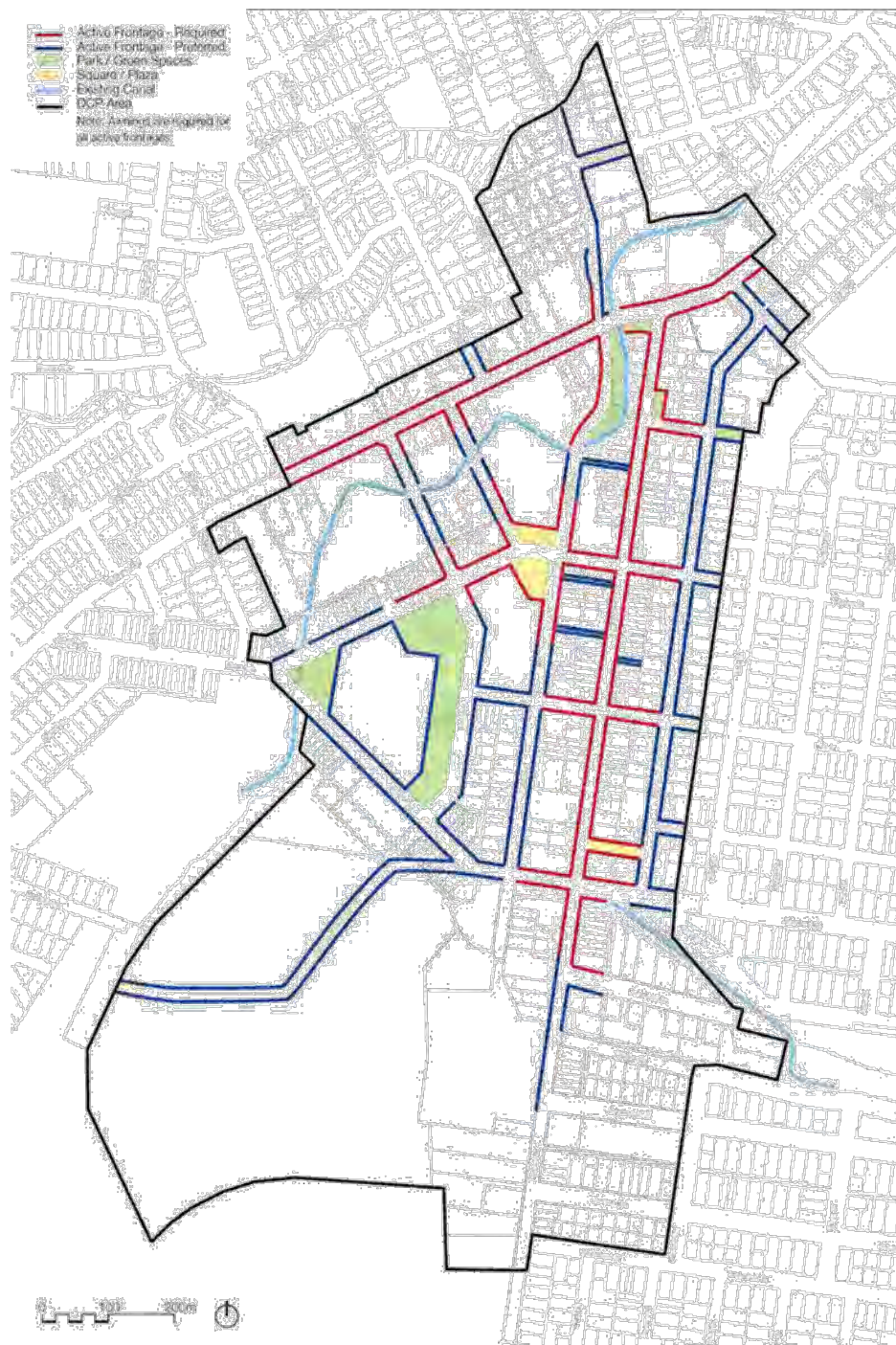


Figure 4: Active Frontages Map

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16.1.10 Building Materials, Finishes and Colours

This section applies to both heritage and non-heritage buildings within the Commercial Precinct.

Objectives

- (a) Encourage building materials, finishes and colours that create a finer texture and add visual interest to streetscapes.
- (b) Encourage use of sustainable building materials and methods.

Controls

- 1. Developments are to use a combination of colour and texture to provide visual interest in building facades (e.g. use colour to articulate vertical proportions of buildings or building entries).

16.1.11 Built Form, Setbacks and Street Hierarchy

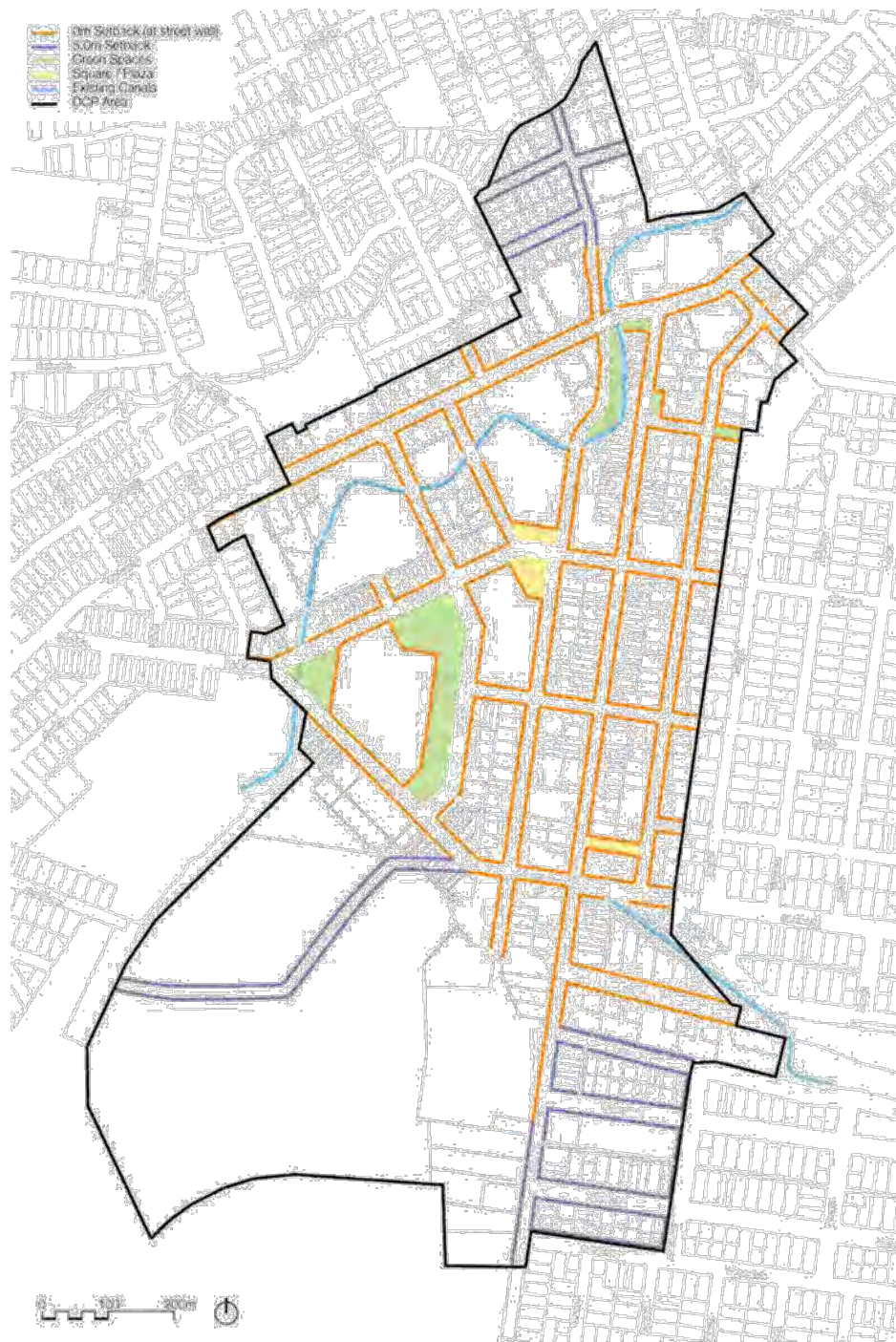
Objectives

- (a) Achieve strong, consistent built form definition of streetscapes and all other areas of public domain.
- (b) Ensure a well-scaled public domain that prioritises pedestrian over vehicular use.
- (c) Ensure that the scale, modulation and façade articulation of development responds to its context.
- (d) Ensure development reinforces the existing and desired character of the Commercial Precinct.
- (e) Avoid significant overshadowing of parks, public streets and places.
- (f) Ensure roads and access arrangements integrate pedestrian and cyclist access, landscaping, lighting, street furniture, and are suitable for the nature and volume of traffic in the Commercial Precinct.
- (g) Provide for a legible street environment with a good human scale.

Controls

- 1. Development to be consistent with the Front Setbacks Map at Figure 5.
- 2. Development, including street frontage height and setbacks above street frontages, is to be consistent with the relevant street hierarchy cross sections identified in Figure 7 through Figure 11. (Refer to Figure 6 to determine which cross section is applicable.)
- 3. Each building facade is to be well articulated to differentiate between the base, middle and top.
- 4. No windows are permitted on side or rear boundaries that abut another site (i.e. don't face onto the public domain).
- 5. Residential development must include minimum side and rear setbacks that Comply with the Apartment Design Guide.

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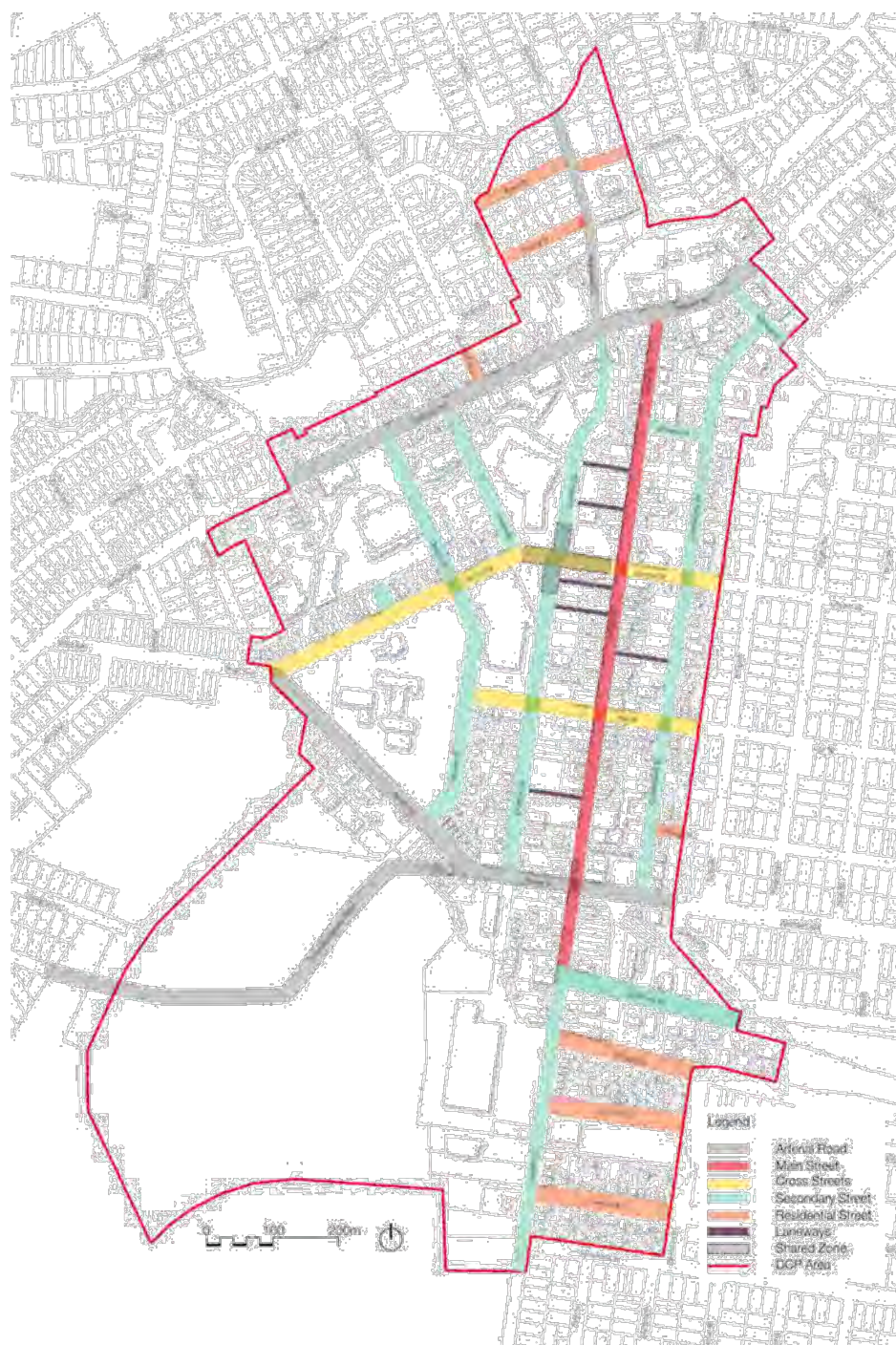


Figure 6: Street Hierarchy Map

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Figure 7: City Bypass/Arterial Road Typology

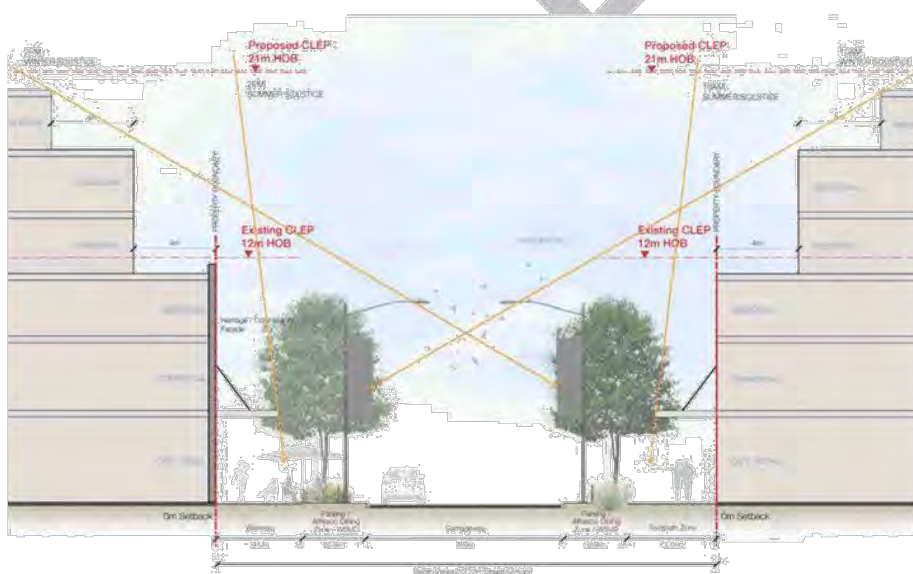


Figure 8: Main Street Typology

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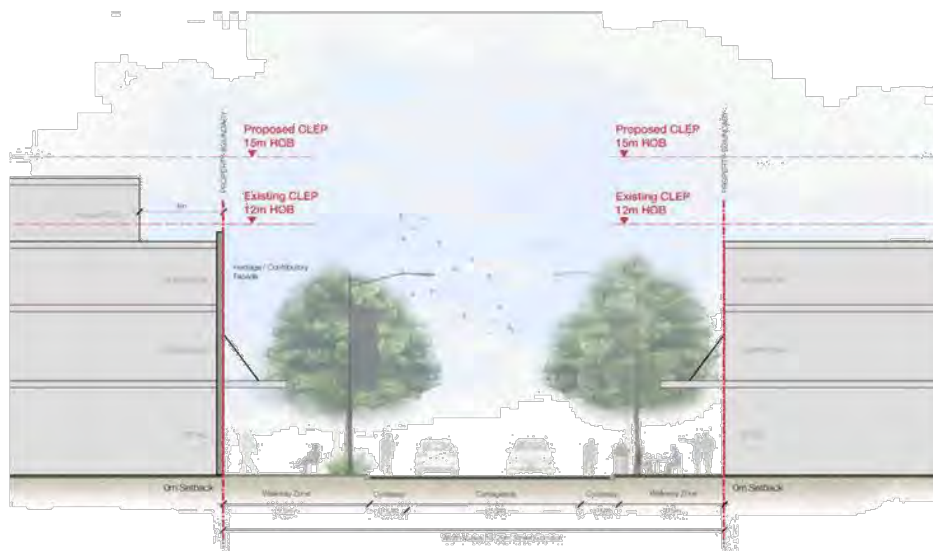


Figure 9: Cross Street and Secondary Street Typology



Figure 10: Residential Street Typology

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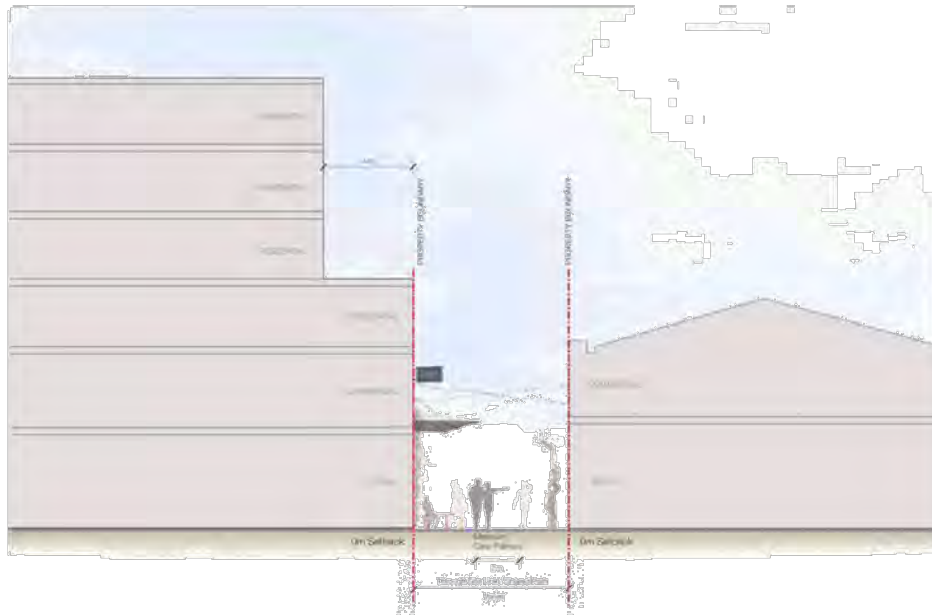


Figure 11: Laneway Typology

16.1.12 Height in Storeys

Objective

- (a) Ensure the height in storeys of development reinforces the desired character of the Cessnock Commercial Precinct.

Controls

1. Development is to be consistent with the Height in Storeys Map shown at Figure 12. In case of inconsistency the LEP prevails over the DCP.

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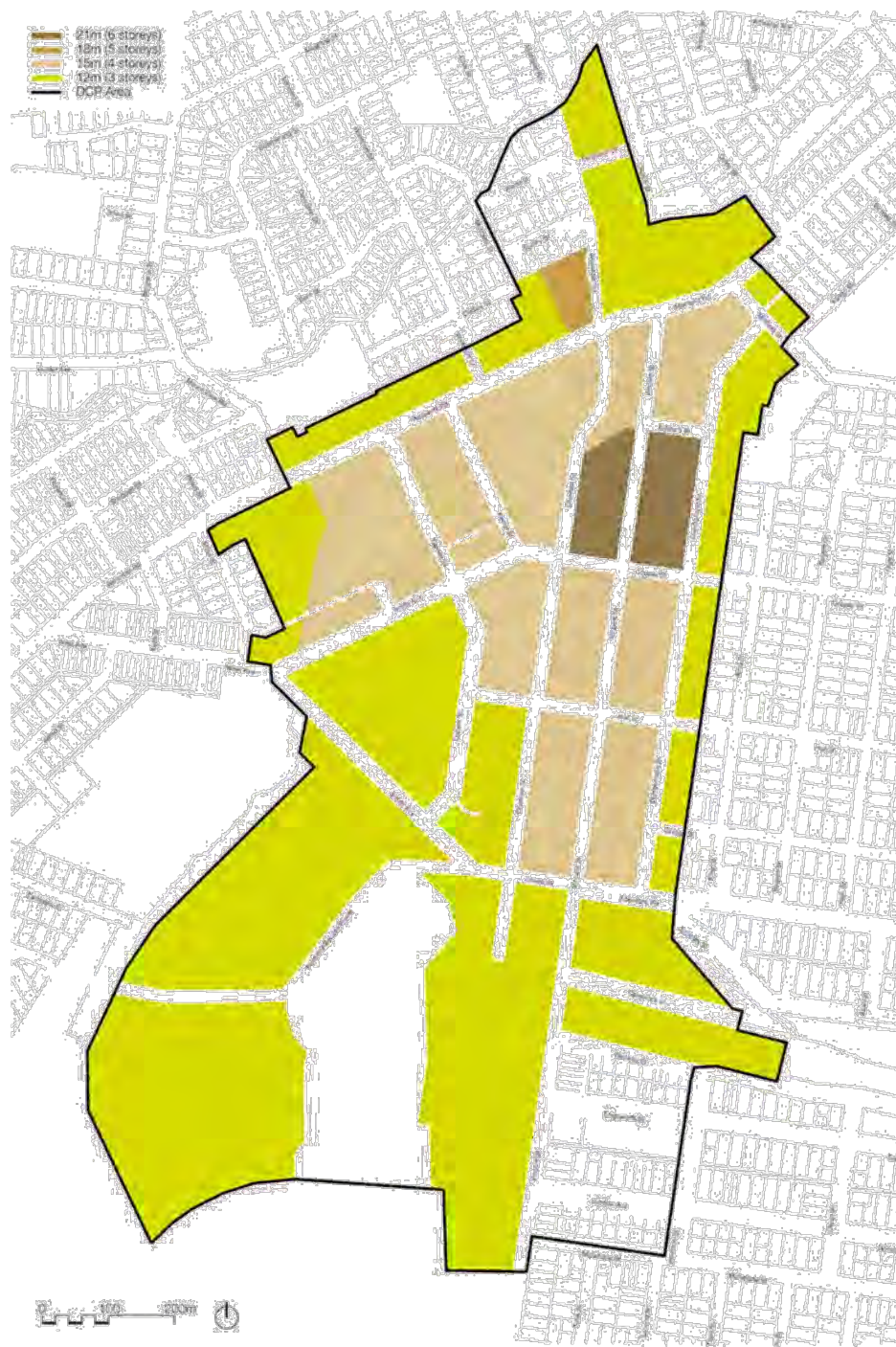


Figure 12: Height in Storeys Map

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16.1.13 Parking and Access

Objectives

- (a) Provide adequate car parking to meet the needs of the Commercial Precinct.
- (b) Encourage economic growth within the Commercial Precinct through reduced parking requirements for nominated uses.

Controls

- 1. Development in the Commercial Precinct is to provide on-site parking in accordance with Part C.1 'Parking and Access' of Cessnock Development Control Plan 2010.
- 2. Despite (1), no parking is required for development in the Commercial Precinct for the purposes of a neighbourhood shop (less than 90sqm in size), restaurant or café, take away food and drink premises, or small bar.

16.1.14 Pedestrian and Bike Network

Objectives

- (a) Prioritise pedestrian and bicycle access, connectivity and safety in the Commercial Precinct.
- (b) Ensure that development is consistent with the Cessnock Bicycle Strategy 2016.
- (c) Ensure that any new public pedestrian and cycling links are located on nominated sites.
- (d) Promote cycling through the mandatory provision of bike storage for nominated land uses.
- (e) Promote cycling through the strategic placement of bike storage facilities in the public domain.

Controls

- 1. Through-site links are to be provided in the locations shown on the Active Transport Strategy Map at Figure 13.
- 2. Through-site links are to be designed to:
 - a. Generally have a minimum width of 4m, or 6m where bike access is provided;
 - b. Generally be open to the sky or provide naturally ventilated glazed roofs for weather protection;
 - c. Have a clear line of sight between public places;
 - d. Be fully accessible 24 hours per day; and
 - e. Be well lit for the safety of users.
- 3. Provision of cycleways and public bicycle parking facilities is to be consistent with the locations identified in the Active Transport Strategy map at Figure 13.
- 4. Bike parking is to be provided in accordance with the rates and categories set out in Table 1 below. Where the proposed use is not shown in the table or the use does not meet the minimum amount of GFA, no bike parking is required.
- 5. Bicycle parking facilities are to be designed in accordance with the relevant Australian Standards [AS2890.3 (2015)].
- 6. An equivalent number of lockers must be provided for employee bicycle parking spaces.

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Table 1: On-site bike parking rates and categories*

Proposed use	Employees/residents	Customers/visitors
Office premises or business premises	1 space per 300m ² GFA (Category B)	-
Bulky goods premises	1 space per 100m ² GFA (Category B)	1 space per 1,500 m ² GFA (Category C)
Shopping centre	1 space per 200m ² GFA (50% Category B and 50% Category C)	1 space per 300m ² GFA (Category C)
Industry or warehouse or distribution centre	1 space per 20 staff (Category B)	-
Hotel or motel accommodation	1 space per 30 units or part thereof (Category B)	
Serviced apartments	1 space per 5 units or part thereof (Category B)	1 space per 30 units or part thereof (Category C)
Boarding house	1 space per 10 bedrooms or part thereof (Category B)	1 space per 20 bedrooms or part thereof (Category B)
Group home	1 space per 10 bedrooms or part thereof (Category B)	1 space per 20 bedrooms or part thereof (Category B)
Attached Dwellings, Multi Density Housing, Residential Flat Buildings, Shop Top Housing	1 space per dwelling (Category TBA)	1 space per 10 dwellings where more than 4 dwellings are within the same development (Category C)
Other retail	1 space per 100m ² GFA (Category B)	

* Category Definitions:

- Category A: Bike Locker (High security lockable individual storage units)
- Category B: Bike Storage Cage (sheltered)/Room (bring your own lock)
- Category C: Bike Racks (bring your own lock)



Precedent image: public bicycle parking facilities and public domain treatments

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Figure 13: Active Transport Network Map

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16.1.15 Public Transport

Objectives

- (a) Encourage public transport, access and connectivity through the Commercial Precinct so that residents, workers and visitors have access to safe and convenient public transport.
- (b) Encourage public transport (bus) use, frequency and amenity, including:
 - a. Shelters for weather protection (where no awnings);
 - b. Seating to suit all age groups;
 - c. Adequate lighting for safety;
 - d. Transport information signage;
 - e. Easy to read maps with key destinations illustrated; and
 - f. Provisions for the site impaired.

Controls

- 1. The public transport movement network in the Commercial Precinct is to be generally consistent with the Public Transport Network Map shown at Figure 14.
- 2. Development incorporating new public transport facilities is to be consistent with the Cessnock Commercial Precinct Public Domain Plan.
- 3. New development is to be generally consistent with the Public Transport Strategy shown at Figure 14.
- 4. Provisions for site impaired, such as tactile indicators, should be consistent with AS 1428.4.1:2009.

Milestone 9



Figure 14: Public Transport Network Map

16.1.16 Public Open Space and Landscaping

Objectives

- (a) To integrate high quality landscaping into the built environment of the Cessnock Commercial Precinct.
- (b) The public landscape should create visual interest and character that is interconnected to public open space as illustrated in the Open Space Strategy and Public Domain Plan.

Milestone 9

Controls

1. Development is to be consistent with the Open Space Strategy shown at Figure 15 and the Public Domain Plan.
2. Any public art installed at the locations identified in Figure 15 is to be themed with reference to the Hunter Valley's 'Wine Country' or 'Mining Heritage'. Please refer to Council's Public Art Policy for guidelines.
3. Landscape design should utilise plants that are suited to the soils and climate of the region.



Precedent images: public open space and landscaping

Milestone 9



Figure 15: Open Space Strategy

Milestone 9

16.1.17 Sustainable Development and Water Sensitive Urban Design

Objectives

- (a) Apply principles and processes in the private and public domain that contribute to Ecologically Sustainable Development (ESD).
- (b) Reduce the environmental impacts of development.
- (c) Reduce the causes and impacts of the urban heat island effect.
- (d) Increase the resilience of development to the effects of climate change.
- (e) Reduce the use of potable water.
- (f) Minimise waste.
- (g) Improve biodiversity.
- (h) Promote Water Sensitive Urban Design (WSUD), including the restoration of urban drainage systems, waterways and canals.



Precedent images: Water Sensitive Urban Design

Controls

- 1. For non-residential development, all new water fittings and fixtures (e.g. showerheads, water tap outlets, urinals and toilets) are to be the highest Water Efficiency Labelling Scheme (WELS) star rating available at the time of development.
- 2. Non-residential development is to be designed to minimise the need for active heating and cooling by incorporating passive design measures related to glazing, natural ventilation, thermal mass, external shading and vegetation.
- 3. Lighting for streets, parks and other public domain spaces to be provided as part of a development should be energy-efficient lighting, such as LED lighting in accordance with Section 10 of the Cessnock Commercial Precinct Public Domain Plan.
- 4. The placement of photovoltaic solar panels is to take into account potential building form on adjacent properties in accordance with the built form and setback controls outlined in Section 16.1.11 of this DCP.

Milestone 9

5. Development is to be designed so that the flow of pollutants from the site due to stormwater is reduced.
6. Developments 1,000sqm or greater in area are to incorporate Water Sensitive Urban Design (WSUD) techniques in their stormwater management systems to improve the quality of water entering the waterways (e.g. porous pavements, rain gardens and infiltration trenches). Development Applications are to be accompanied by a WSUD Report outlining the measures to be incorporated in the proposal.

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Milestone 9

Part 2 – Catalyst Sites



Figure 16: Catalyst Sites

Milestone 9

16.1.18 Catalyst Sites

Owing to their location, size and/or function, these sites have significant potential to positively shape the future of the Precinct. The Catalyst Sites are identified in Figure 16.

Objectives

- (a) Provide development incentives to enable design excellence and sustainable design for new development within Catalyst Sites
- (b) Identify redevelopment opportunities and development objectives for Catalyst Sites.
- (c) Reduce the isolated nature of the major shopping centre areas.
- (d) Improve the appearance and reduce the environmental impacts of public car parks.
- (e) Identify options for the redevelopment of the TAFE site as a regional cultural/educational centre and district-level park.
- (f) Identify the long term renewal plan for the Commercial Precinct's major stormwater canal as an attractive landscaped linear green space and access way.

Controls

1. Development of 'Catalyst Sites' identified in Figure 16 should respond to the development objectives outlined in Table 2.

Milestone 9

Table 2: Catalyst Sites + Open Space Opportunities

Ref.	Catalyst Site	Ownership	Area (Ha)	Significance	Opportunity	Development Objectives
1	South Ave properties	Private	1.31	Properties along South Avenue have important frontages (visually prominent) to proposed future Bypass route.	(To rezone as B6 Enterprise Corridor to optimise opportunities for employment and new business in the Centre - TBC, subject to confirmation of Bypass route.)	<ul style="list-style-type: none"> To encourage future development promotes commercial, retail and business activity.
2	TAFE	Department of Technical + Further Education	3.16	A major site in the west of the Centre - if educational use is to continue, a facility of this scale brings employment, retail demand and a vibrant student demographic to the Centre.	To redevelop as a renewed educational facility serving the region, a new library, and/or tourism information centre. Opportunity to better define and activate adjoining streets, rather than current significant setbacks. Potential to be the council relocation site.	<ul style="list-style-type: none"> To encourage the use of the TAFE site as an education hub, potentially in conjunction with a compatible civic use, such as a library and multi-purpose cultural centre (museum space, wine and mine centre etc.).
3	Coles supermarket	Private + Council (Operational)	2.00	A large, and key site in the heart of the Centre and a major retail destination. A key site on the Cooper Street cross axis link to 'Main Street' and with an important frontage to TAFE Park.	To better define and activate adjoining streets, and proposed Town Square, rather than current blank walls or activity setback from street frontages by large parking areas on the east and south. To better screen 'back of house' activities on west (TAFE Park frontage). To provide improved weather protection for pedestrians - to better link supermarket and 'Main Street' retail.	<ul style="list-style-type: none"> To aid in the development of an integrated commercial centre. To promote the development of a town square for the neighbourhood, accommodating areas for gathering, seating and children's play.
4	Cessnock Plaza (Woolworths)	Private + Hunter Water Corporation	1.82	Also a large, and key site in the heart of the Centre and a major retail destination. A key site on the Cooper Street cross axis link to 'Main Street' and with important street frontages.	To better define and activate adjoining streets, rather than current setbacks for parking on west, south and east. To upgrade the environment along the canal to the north. To coordinate/collaborate with IGA site to realise a better urban development outcome. To provide weather protection (e.g. awnings) for pedestrians - to better link supermarket and 'Main Street' retail.	<ul style="list-style-type: none"> To aid in the development of an integrated commercial centre. To promote the development of a town square for the neighbourhood, accommodating areas for gathering, seating and children's play.

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Milestone 9

Ref.	Catalyst Site	Ownership	Area (Ha)	Significance	Opportunity	Development Objectives
5	Cessnock Baths site	Council (Community)	0.69	The long term plan is for Cessnock Baths to be relocated to a new site outside the Centre, and, as a result, this prominent corner site, at the northern 'gateway' to Cessnock Commercial Centre, would become available for other uses.	To mark this important corner in the Centre with a high quality urban outcome - a 'model' mixed use development of high quality architecture with attractive landscaped street frontages, Enterprise Corridor retail to main road frontages and/or hotel.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design. To encourage future development that promotes a suitable mix of land uses.
6	Former Cinema & carpark	Private	0.60	A prominent 'Main Street' corner property with significant frontage to Cooper Street - an important west-east connector street. The former cinema provided one of the few night-time entertainment venues in the Commercial Centre.	To re-establish as a night-time entertainment venue, to better define and activate adjoining streets, particularly Charlton Street, and to screen existing parking area with active ground level frontages.	<ul style="list-style-type: none"> To encourage the ongoing use of the site for entertainment purposes.
7	Vincent St carpark site	Council (Operational) + Private	0.33	Consolidated, current carpark site in the heart of the CBD, with both Vincent Street and Cumberland Street frontages. Elevated with views west over the valley.	To realise a high quality urban development outcome - a 'model' mixed use development of high quality architecture with attractive landscaped street frontages.	<ul style="list-style-type: none"> Future development of the site is to achieve high quality architectural outcome that promotes retail, commercial and residential uses.
8	Cessnock City Council	Council (Operational)	0.52	Current Cessnock City Council, Council Chamber, offices, public address and associated car parking. A key consolidated site within the CBD with Vincent Street, Cooper Street and Cumberland Street frontages.	To realise a high quality urban development outcome - a 'model' civic development of high quality architecture with attractive landscaped street frontages.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design.
9	Public carpark	Council (Operational) + Private	0.59	Consolidated Commercial Centre site, currently used for public car park. Significant CBD corner/street frontages.	To develop as multi-level car park, sleeved with ground level retail/active street frontages. To develop rooftop as hotel/motel accommodation, private apartments or affordable housing. Potential to be a relocation site for Council.	<ul style="list-style-type: none"> Ensure future development promotes activity on the street and within the public domain.
10	Mixed use & public carpark site	Cessnock Leagues Club	0.54	Large consolidated site, including on site parking and green space.		<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design and public domain outcome.

Milestone 9

Ref.	Catalyst Site	Ownership	Area (Ha)	Significance	Opportunity	Development Objectives
11	Public carpark	Council (Operational)	0.16	Consolidated Commercial Centre site currently used for public car park. Significant CBD corner/street frontages.	To develop as multi-level car park, sleeved with ground level retail/active street frontages. To develop rooftop as hotel/motel accommodation, private apartments or affordable housing.	<ul style="list-style-type: none"> Future development is to encourage active street frontages.
12	Vincent St site	Council (Community)	0.41	Consolidated, prominent 'Main Street' corner site, currently three-quarters vacant.	To create a consolidated cultural/arts hub, consisting of Cessnock Library (relocated/expanded), Regional Art Gallery and potential alternative site for Visitor Centre - adjacent to existing Cessnock Performing Arts Centre.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design and public domain outcome.
13	Hall St site	Council (Operational)	0.07	Prominent Centre corner site, currently vacant.	To realise a high quality urban development outcome - a 'model' mixed use development of high quality architecture with attractive landscaped street frontages.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design and public domain outcome. To provide for active street frontages to promote activity on the street and within the public domain.
14	Potential Library site and public carpark	Council (Operational)	0.40	Consolidated Commercial Centre site currently used for public car park.	To develop a new multi-level public carpark with potential Cessnock Library on ground floor, or other active / retail use.	<ul style="list-style-type: none"> Future development is to encourage active street and laneway frontages. High quality architectural design and public domain outcome.

Open Space Opportunities

Ref.	Opportunity	Ownership	Area (Ha)	Significance	Opportunity	Development Objectives
A	TAFE site	Department of Technical and Further Education (TAFE)	1.51	A large green space at the 'heart' of the CBD that is currently underutilised, poorly defined, and lacks usable spaces and places to enjoy on a day to day basis (e.g. currently consists of a war memorial, gazebo, exposed lawn areas, scattered trees and parking areas).	To provide a District Level Playground and well-defined park areas.	<ul style="list-style-type: none"> To provide a district recreational park for the community, accommodating areas for gathering, picnicking, children's play and relaxation.

Milestone 9

Ref.	Opportunity	Ownership	Area (Ha)	Significance	Opportunity	Development Objectives
B	Stormwater canal (north)	(Hunter Water Corporation)	833m (length)	A major concrete stormwater canal, of significant width, depth and length, weaves through the urban fabric of five city blocks.	In the long term to establish an attractive landscaped linear green space and accessway, connecting the TAFE Site to bushland and park reserves to the northeast.	<ul style="list-style-type: none"> • Upgrade and incorporate WSUD of the existing stormwater canal.
C	Vincent St 'North Gateway' Park	Council (Community)	0.07	Prominent corner site at the north gateway to the Commercial Centre.	To mark/define the entry to Cessnock CBD with appropriate paving, signage, lighting, artwork, and feature landscaping at this key intersection. To activate the open/arrival space with adjoining building frontages both defining and opening to the space.	<ul style="list-style-type: none"> • Enhance the existing park signifying its gateway location.
D	HJ Sternbeck Lions Park	Private	0.08	Publically accessible, medium sized park located on a key corner of the 'Main Street'.	To improve the amenity and usefulness of the park for users and to activate the edges of the park through introducing retail uses on adjoining building frontages, to the north and east.	<ul style="list-style-type: none"> • Upgrade the existing park and open to surrounding public domain.
E	Cooper St & Shopping Centre car parks	Private	0.48	Undeveloped space at the 'heart' of the CBD, with connections from key shopping centres to the 'Main Street', currently only used for carparking.	To develop as a City Market - a multi-mode public plaza, suitable for use for regular weekly regional farmers markets and for special events/festivals, reverting to parking at other times. To provide lightweight sculptural glass/steel permanent weather protective roof for market days, potential to integrate solar PV power generation.	<ul style="list-style-type: none"> • To promote the development of a town square for the neighbourhood, accommodating areas for gathering, seating and children's play. • Develop Cessnock Market Strategy.
F	CPAC carpark	TBC	0.08	Important through-block public domain, pedestrian and visual connection from Vincent Street (existing plaza) through to Cumberland Street, adjoining CPAC.	To extend existing CPAC plaza east through to Cumberland Street, consolidating parking in a multi-level parking station, sleeved at ground level in retail/cultural uses.	<ul style="list-style-type: none"> • Encourage the expansion of the existing CPAC plaza through to Cumberland Street.
G	Western Gateway Triangle	TBC	0.37	Prominent corner, triangular site at the west gateway to the Commercial Centre.	To mark/define the entry to Cessnock CBD with appropriate paving, signage, lighting, artwork, and feature landscaping at this key intersection.	<ul style="list-style-type: none"> • Deliver a gateway park which enhancing streetscape.

Milestone 9

2. Development of 'Catalyst Sites' (5, 6, 7, 10 and 11) in accordance with the development opportunities and objectives outlined in Table 2 and in accordance with the Cessnock LEP 2011 are eligible for the following reduced car parking rates:
 - a. 25% less than the required by Chapter C.1 of this DCP for land uses including business & office premises; entertainment facilities; function centre; places of public workshop; restaurant; shops and general business; or
 - b. 50% less than that required by Chapter C.1 of this DCP for land uses including business & office premises; entertainment facilities; function centre; places of public workshop; restaurant; shops and general business where the development includes residential floor space equal to or greater than 50% of the total floor space of the development.

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Cessnock Commercial Precinct Project

Milestone 9

Prepared for: Cessnock City Council
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16005 | 11 April 2017

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Cessnock Commercial Precinct Project

Milestone 9

Cover Image: View along Cessnock Main Street (Vincent Street) to the Hills beyond.

Revision	Date	Description	By	Chk	App
01	3/10/2016	Initial Design	LEW	DA	
02	5/10/2016	Final Design - Final Design	DA	DA	
03	2/10/2016	Final Design - Final Design	DA	DA	
04	1/10/2016	Final Design - Final Design	DA	DA	
05	1/10/2016	Final Design - Final Design	DA	DA	
06	1/10/2016	Final Design - Final Design	DA	DA	
07	1/10/2016	Final Design - Final Design	DA	DA	
08	1/10/2016	Final Design - Final Design	DA	DA	
09	1/10/2016	Final Design - Final Design	DA	DA	
10	1/10/2016	Final Design - Final Design	DA	DA	

16005 11 April 2017



Prepared for: Cessnock City Council

By:

Conybeare Morrison International Pty Ltd
Architects & Urban Planning & Urban Design | 55-58 William Street, East Sydney, NSW 2011
Tel: 61 2 8244 8888 | info@cmplus.com.au | www.cmplus.com.au

Cessnock Commercial Precinct Project | 11 April 2017

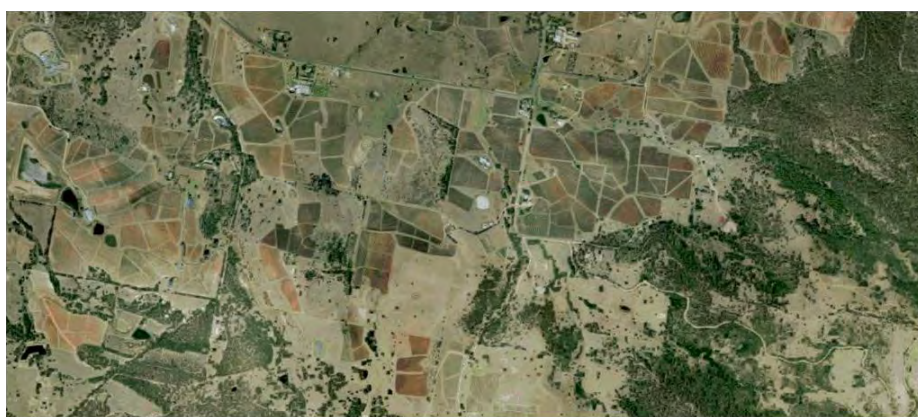
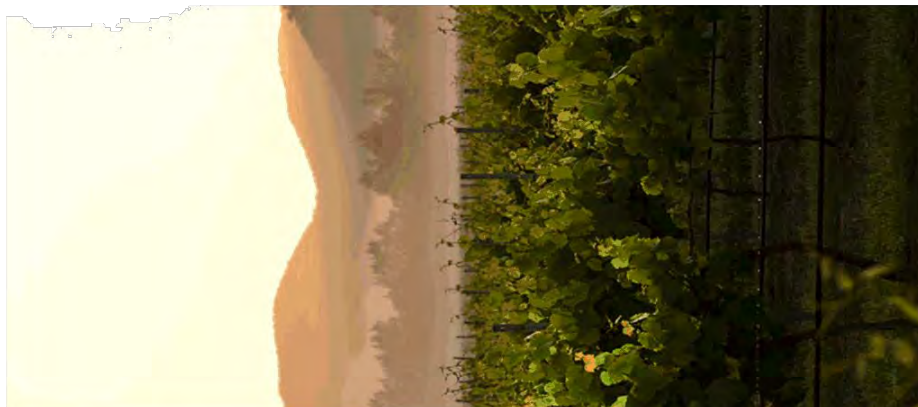
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Appendices

1. Preliminary Budget Estimate
2. Public Domain Furniture and Finishes Schedule
3. Wayfinding signage / Information Kiosk



11 April 2017 | Cessnock Commercial Precinct Project

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1.0 Introduction

1.1 The Project

'This Project aims to complement the Lower Hunter Regional Strategy in its vision for the city of Cessnock as a Major Regional Centre through the establishment of a site specific Development Control Plan and Public Domain Plan. In addition, the project aims to complete actions identified in the Cessnock CBD Masterplan, develop guidelines to improve the public domain of the Cessnock Commercial Precinct, and provide a document that will form the basis of a Developer Contributions Plan for public domain improvements.'

(Extract from Cessnock Commercial Precinct Project RFO, Section 2 – The Brief)

The three primary project deliverables are:

- Development Control Plan (DCP)
- Public Domain Plan (PDP)
- Implementation Plan (IP)

The project included a review of the CBD Masterplan prepared by Arup in 2012 and has developed the statutory framework (recommended changes to the current CLEP and specific Cessnock Commercial Precinct CDCP provisions) and a costed and prioritised concept design for the Commercial Precinct public domain. The outcomes of this urban planning work will provide a sound basis for the next implementation phase – the staged delivery of upgrade works over the next 25 years.

1.2 Study Area

The Cessnock Commercial Precinct study area is 110.6Ha in size and is generally defined by lands with a business zoning (refer to Figure 1).

1.3 Project Objectives

The project has the following Objectives:

- Develop a Development Control Plan and Public Domain Plan that will assist in establishing the Cessnock commercial precinct as a Major Regional Centre;
- Develop a set of achievable development incentives that will encourage good design within the Cessnock Commercial Precinct;
- Complete actions identified in the Cessnock CBD Masterplan;
- Ensure an adequate supply of housing (and appropriate mix of housing typologies) and employment land within the Cessnock Commercial Precinct for the next 25 years;
- Provide the base information that will inform future Contributions Plan funding improvements to the public domain of the Cessnock Commercial Precinct;
- Develop an Implementation Plan that:
 - Provides recommendations to improve CLEP 2011;
 - Schedules public domain and infrastructure improvements, identifying staging, priority, funding and contributions, timing and cost; and
- Develop and implement a Community Engagement Strategy that effectively engages the community of the Cessnock Commercial Precinct.



Figure 1: Study Area

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1.0 Introduction

1.4 Study Team

The Conybeare Morrison (CM⁺) led Consultant Team brings together significant urban planning experience, with specialist expertise in the following areas:

- Project Management
- Urban Design
- Stakeholder + Community Consultation
- Statutory Planning
- Development Economics
- Landscape Design
- Quantity Surveying
- Transport Planning
- 3D Visualisation

The project organisational structure is illustrated in Figure 2.

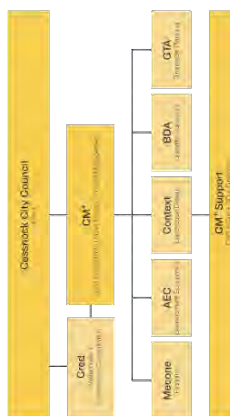


Figure 2: Project Team Structure.

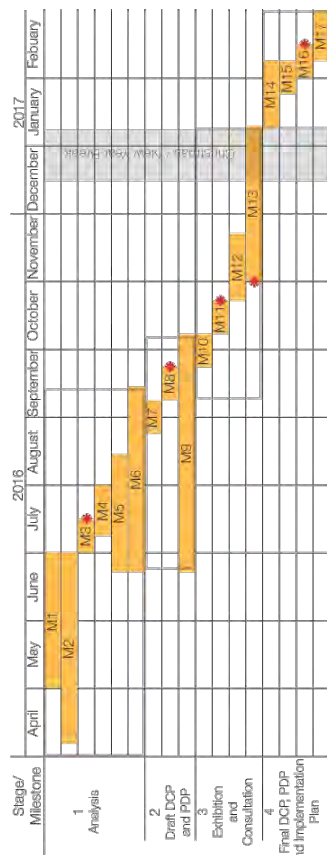
1.5 Study Methodology and Program

The CM⁺ consultant team commenced work on the project in April 2016 and will complete the project over an approximately one year timeline. The project program and milestones are illustrated in Figure 2. The following four stage methodology outlines the overall approach:

1. Undertake urban analysis;
2. Prepare draft:
 - a. Development Control Plan (DCP)
 - b. Public Domain Plan (PDP)
 - c. Implementation Plan (IP);
3. Undertake consultation and place on public exhibition; and
4. Finalise DCP, PDP and IP.

Stakeholder and community consultation lead by Cred Consultants is an important component of the project – informing the urban and landscape design at key intervals through the program.

The program incorporates workshops with the community and with Council planning officers, presentations to Cessnock City Council and a public exhibition of the draft plans to allow an opportunity for community and stakeholder comment and feedback.



- Milestones 1: Draft Supply and Demand Analysis, Draft Community Engagement Strategy
- Milestones 2: Draft CCP Workshop Presentation
- Milestones 3: Workshop
- Milestones 4: Final Supply and Demand Analysis, Community Engagement Strategy
- Milestones 5: Draft Background Reports
- Milestones 6: Draft DCP and PDP
- Milestones 7: Prepare Draft Workshop Presentation
- Milestones 8: Present to Council Staff
- Milestones 9: Draft Implementation Plan, final Background Reports, and Draft DCP and PDP
- Milestones 10: Prepare Draft DCP, PDP and Implementation Plan presentation
- Milestones 11: Present to Councilors
- Milestones 12: Exhibition Draft DCP, PDP and Implementation Plan
- Milestones 13: Community Consultation and Public Exhibition
- Milestones 14: Post Exhibition Draft DCP, PDP and Implementation Plan
- Milestones 15: Prepare Draft DCP, PDP and Implementation Plan presentation
- Milestones 16: Present to Councilors, Council Staff
- Milestones 17: Final DCP, PDP and Implementation Plan

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2.0 Planning Context

2.1 Regional Context

The Lower Hunter is the sixth largest urban area in Australia. It is located 160 kilometres north of Sydney, covers an area of 4,291 square kilometres and is a major population centre that includes the regional city of Newcastle, the State's second largest urban centre.

The Lower Hunter is a region of diverse landscapes and land uses. Its coastline, mountains, lakes, floodplains and rivers are key elements of the Region's identity, culture and lifestyle.

The Lower Hunter is characterised by large areas of rural, agricultural and forested lands. Based on a major river and coastal estuary, it contains areas of high quality agricultural land, important drinking water aquifers and internationally and nationally significant environmental assets. The region's rural and semi rural landscapes account for around 80 per cent of the Region's land, and are themselves further characterised by key industries of mining, wine production and tourism.

Cessnock is known as the 'gateway' to the Hunter Wine region which is located immediately to the northwest of the Centre (refer to Figure 4).

2.2 Local Context

Cessnock is a designated Major Regional Centre - identified in the Lower Hunter Regional Strategy prepared by the Department of Planning and Environment (DPE) in 2008. The strategy identifies significant population, dwelling and employment growth for the Lower Hunter projected through to 2031. However, growth in the Cessnock LGA and in the Commercial Centre is expected to be slower and more gradual. The subsequent Draft Hunter Regional Plan released by the DPE in November 2015 confirms this overall growth scenario.

The opening of the Hunter Expressway in March 2014 provided an attractive north-south route, removing through-traffic, including heavy vehicular traffic from the Cessnock 'Main Street': Vincent Street.

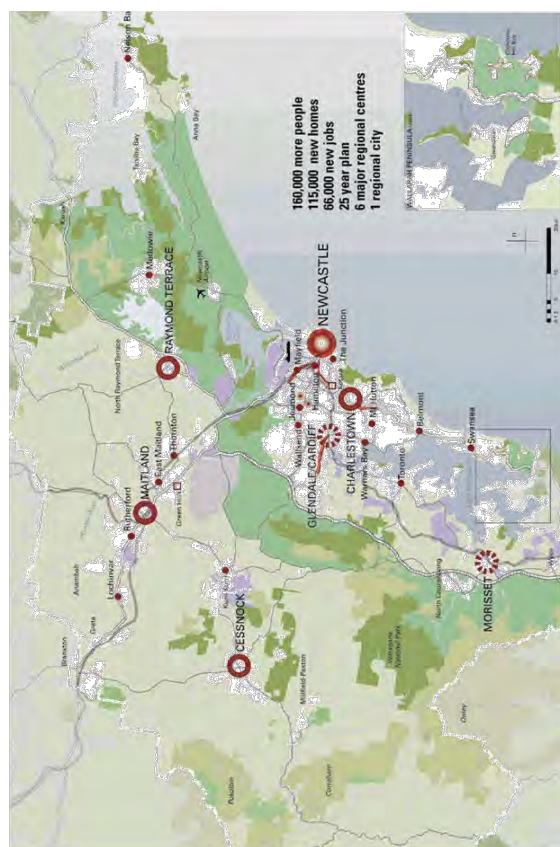


Figure 3: Lower Hunter Regional Strategy Map (Source: Lower Hunter Regional Strategy 2006 - 2031)

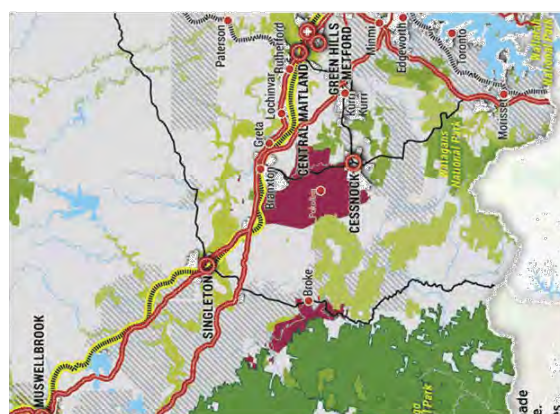
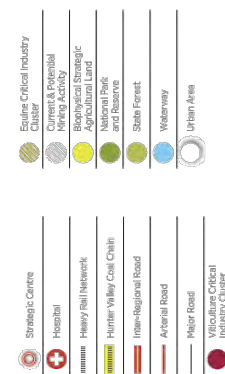


Figure 4: Western Hunter Regional Map (Source: Draft Hunter Regional Plan, 2015)



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2.0 Planning Context

2.3 Current Development Controls

A review was undertaken of the key development controls that currently apply to the Cessnock Commercial Precinct, and the findings are summarised below.

Land Zoning

Land Zones applying to the study area are:

- B3 Commercial Core
- B4 Mixed Use
- B7 Business Park
- RU2 Rural Landscape
- R3 Medium Density Residential
- RE1 Public Recreation

The Commercial Core (primarily retail) is concentrated in the north of the study area, with the remainder of the Centre Zoned Mixed Use. Medium Density Residential Zones surround the Centre, to the west, north and east, whilst Business Park and Rural Landscape zones occur to the south of the Centre. Refer to Figure 5.

Floor Space Ratio (FSR)

An FSR of 1.5 : 1 applies to the core of the Commercial Precinct, whilst the rest of the Centre is not subject to density controls. Refer to Figure 6.

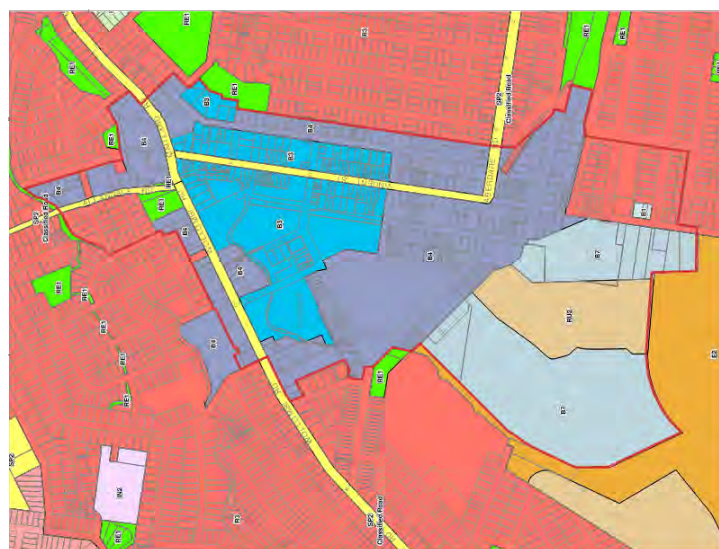


Figure 5: Zoning Map

Zone	
B1 Neighbourhood Centre	
B3 Commercial Core	
B4 Mixed Use	
B7 Business Park	
RU2 Rural Landscape	
R3 Medium Density Residential	
RE1 Public Recreation	
RE2 Playable Recreation	
RU2 Rural Landscape	
SP2 Infrastructure	
R2 Low Density Residential	

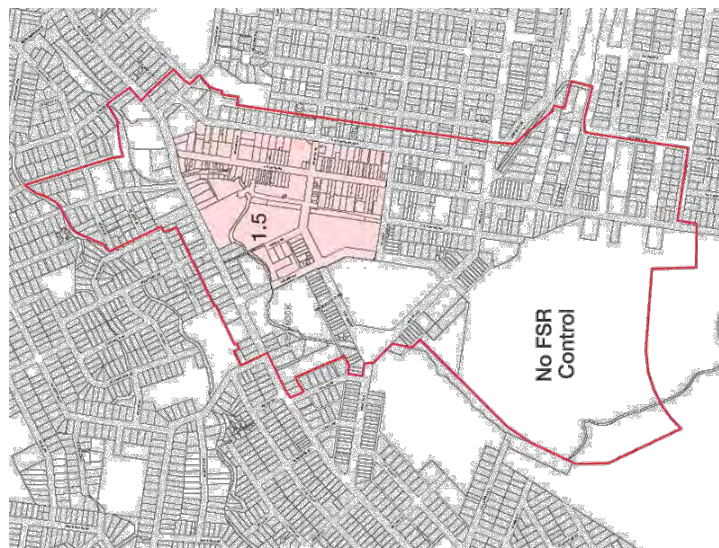


Figure 6: Floor Space Ratio Map

Maximum Floor Space Ratio (FSR)	
1.5	
No FSR Control	

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2.0 Planning Context

Height of Building (HOB)

A 12 metre height limit applies to three areas of the Commercial Precinct, refer to Figure 7; the Commercial Core and to the Business Park area in the south.

Allowing for a larger ground floor height, and for conventional roof structures, this height limit permits mixed-use buildings of three storey height in the Commercial Core, or in the Business Park, a two storey commercial, or large scale 'big box' retail development.

Heritage Items

Cessnock CBD's most distinctive feature is the significant number of historic buildings which contribute a distinct identity and local character. Refer to Figure 8.

There are 29 Local Heritage Items and one State Heritage Item in the study area. Most of the heritage buildings are located along Vincent Street, however, there is also large areas to the South that were former mining areas, and the former South Maitland Railway, a landscape heritage item, which is listed as an item of State Heritage.



Figure 8: Heritage Item - Royal Oak Hotel

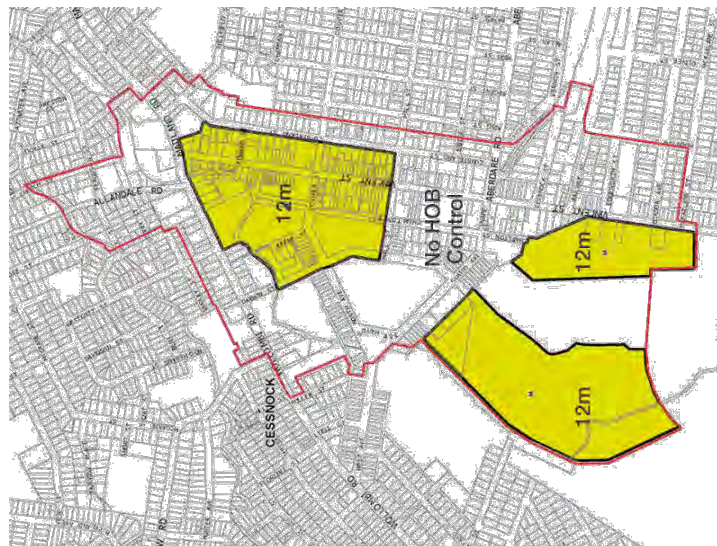


Figure 7: Height of Building Map

Maximum Building Height (m)
12



Figure 8: Heritage Map

Heritage
Item - General
Item - Acknowledged
Item - Landscape

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2.0 Planning Context

2.4 Precinct Constraints

Mine Subsidence

The Commercial Precinct has historically been subject to extensive sub-surface mining, including a zone of shallow underground workings that crosses the Precinct from the southwest, through to the northeast.

Recent flood events have seen 'sink holes' of significant size open up to the south of the Centre, in the Business Park. Refer to Figure 10.

Flood Risks

The Cessnock Commercial Precinct is located within the Black Creek Catchment area. Two tributaries of Black Creek drain from south of Cessnock, through the Centre, merging in the north, to form a single larger drainage line that continues northeast. Given the numerous creeks converging in Cessnock, flooding has occurred regularly including in 1949, 1977, 1980, 1982 and 2007.

Much of the Centre to the west of Vincent Street is subject to 1 in 100 years flood events. Drainage is addressed in the Centre with large scale open concrete canals or subsurface culverts. Refer to Figure 11.

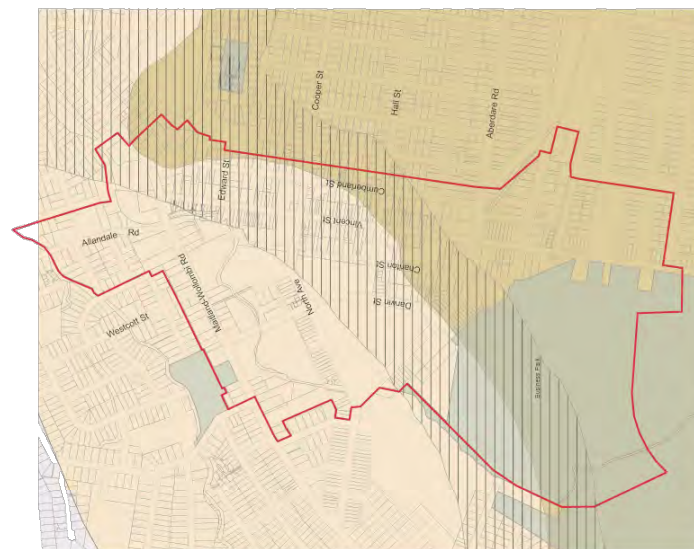


Figure 10: Mining Subsidence Map

Shallow Underground
Working
MSA_DMP_CHASS
Tombain Mine
Subsidence District
Great Coal Measures
Previous Coal Measures
DCP area

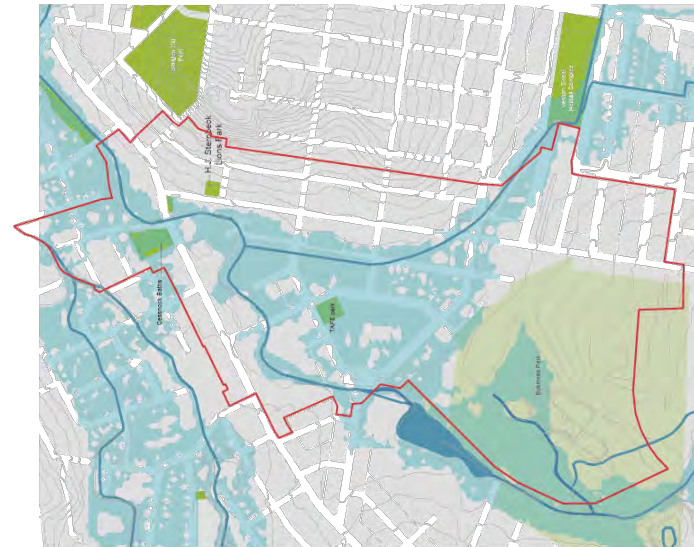


Figure 11: Flooding Map

100 year flood line
Creeks, channels
Green public space
Other open space
Contour - 2m
DCP area

2.0 Planning Context

2.5 Property Ownership

Most of the lands in the Cessnock Commercial Precinct are privately owned. In the Commercial Core, several large shopping centres, occupy the largest private land parcels, they are Cessnock Plaza Shopping Centre, Coles Supermarket, Big W, ALDI Supermarket, IGA Supermarket, Cessnock Leagues Club and the Supercheap Auto store.

To the rear of the Vincent Street city blocks there are significant parcels of land, owned by council and utilised for public car parks. A large triangular block is situated in the west of the CBD and is owned by Department of Technical Education.

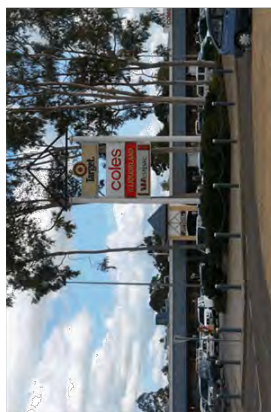


Figure 13: Cessnock City Centre Shopping Centre



Figure 14: Cessnock Plaza Shopping Centre



Figure 15: Cessnock TAFE Park

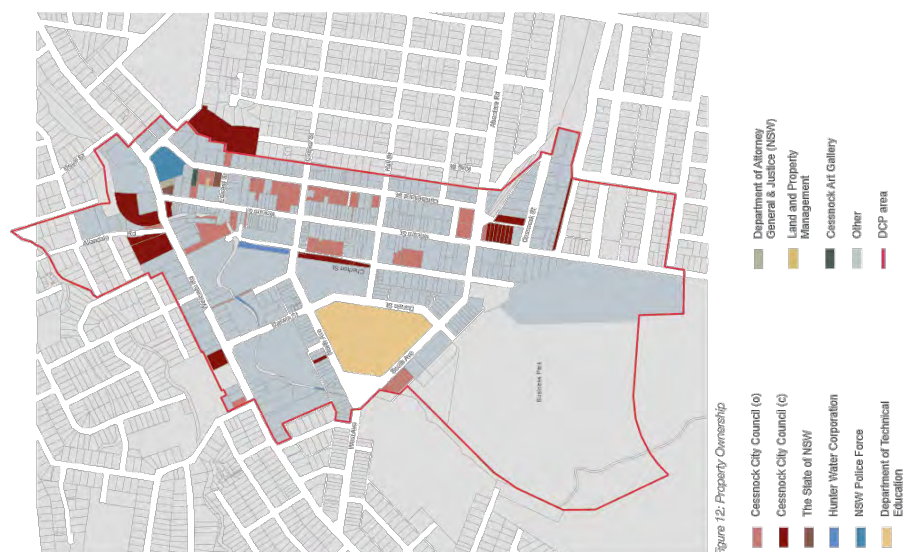


Figure 12: Property Ownership



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3.0 Review of CBD Masterplan

3.1 Introduction

This chapter provides a review of the Cessnock CBD Masterplan and summarises the commitments made to improve the public domain of the Cessnock Commercial Precinct.

3.2 Masterplan Vision: 'Attractive / Thriving / Welcoming'

The CBD masterplan identified the important role Cessnock plays as the focus of civic, health, education, cultural life and local identity for people in the Hunter Region. The masterplan strategy outlines the path towards a revitalised CBD with improved liveability, aesthetics and increased opportunities for investment.

The Cessnock City community Plan 2020 identified the following 'vision' for Cessnock:

'Cessnock will be a cohesive and welcoming community living in an attractive and sustainable rural environment with a diversity of business and employment opportunities supported by accessible infrastructure and services which effectively meet community need.'

The Masterplan developed a suite of inter-related Objectives, summarised as:

Attractive

- A place that celebrates its history
- A safe environment
- A healthy place to live and work

Thriving

- An inclusive CBD for everyone
- A vibrant main street
- A CBD built on its strengths

Welcoming

- A strong community heart
- A connected and accessible place
- A friendly destination

3.3 Review of Spatial Initiatives

The Masterplan recommends providing better urban infrastructure, including:

- Completion of a number of existing city streets;
- Key road realignments;
- Improving pedestrian links;
- Beautification of drainage channels;
- Strengthening of the CBD 'gateways';
- Strengthening cultural and civic institutions;
- Enhancing 'Main Street' and city parks; and
- Optimising public car parking facilities.

At a social level, better connections and more public spaces for both locals and visitors in the centre of town are proposed.



Figure 17: Artist's impression of the potential future look and feel of Vincent Street



Figure 18: Artist's impression of the potential future look and feel of TAFE parklands

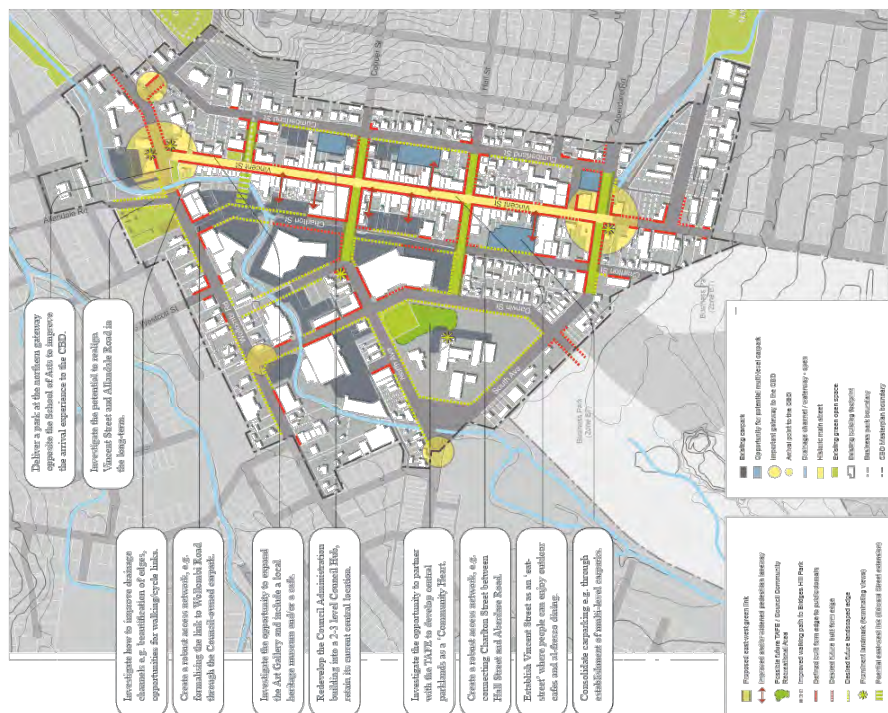


Figure 16: Key Spatial Initiatives Summary

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3.0 Review of CBD Masterplan

3.4 Review of Access and Movement

The Masterplan 'Access and Movement Framework' seeks to improve the CBD's pedestrian environment, and general CBD accessibility, connectivity and safety.

Proposed initiatives include:

- New pedestrian crossings;
- Upgrade of the laneways to public carparks;
- East-west green links;
- New street links; and
- Consolidated public car parking (multi-level).

The upgrade of Vincent Street in 2004 has greatly improved accessibility in Cessnock. This was achieved by upgrading paving, landscaping, lighting and street furniture. However, west of 'Main Street' the urban fabric falls away and streets and spaces are ill defined, lack amenity and feel unsafe. The opening of the Hunter Expressway reduces traffic flow along Vincent Street, offering opportunities to improve pedestrian amenity in the CBD 'Main Street'.

3.5 Review of Public Domain Framework

The Masterplan 'Public Domain Framework' seeks to improve the CBD pedestrian safety, legibility and overall street level experience. The CBD Masterplan aspires to:

"A public domain that provides places where children can play, friends can meet for a coffee and workers can have lunch will signal that Cessnock is lively, buzzing, successful - in short: a place desirable to be at."

Proposed initiatives include:

- Extension of the low speed environment to include key east-west streets;
- New and upgraded public spaces and recreational parks;
- Highlighting the CBD 'gateways'; and
- A comprehensive signage strategy.



Figure 20: Public Domain Framework

- Channel / waterway
- Topography contours (5m)
- Existing green open space
- Business park boundary
- Study site boundary
- Proposed closed-road easement (in addition to Vincent Street)
- Important public / civic space in the CBD
- Proposed improvement of H.J. Stomack Linear Park
- Public Art / WPE / Council Community Recreational Area
- Proposed east-west green link
- Potential east-west link (Edward Street extension)
- Proposed location of CBD-related signage
- Important gateway to the CBD



Figure 18: Access + Movement Framework

- Major road
- Local road
- Improved pedestrian link to car park
- Existing bus stop
- Significant intersection
- Important gateway to the CBD
- Existing green open space
- Business park boundary
- Study site boundary
- Existing car park (Council-owned)
- Existing car park (privately owned)
- Opportunity for potential multi-level car park
- Proposed additional pedestrian crossing
- Proposed new street / link
- Proposed east-west green link
- Potential east-west link (Edward Street extension)

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3.0 Review of CBD Masterplan

3.6 Review of Urban Form + Aesthetics Framework

The Masterplan 'Urban Form + Aesthetics Framework' seeks to improve the visual experience of the CBD, to enhance the aesthetic appeal, legibility and overall urban experience.

Proposed initiatives include:

- Respect and renew historic buildings to the CBD, to strengthen the unique local character;
- Frame vistas to the natural surroundings e.g. local hills;
- Enhance street views to key CBD buildings and city corners;
- Strengthen landmarks and prominent reference points e.g. prominent buildings;
- Highlight important cultural and community facilities; and
- Introduce new green links and green spaces within the CBD.

The ten initiatives are summarised:

1. TAFE Central Parklands + Community Gardens.
2. Vincent Street al-fresco 'Eat Street'.
3. Upgrade Open Spaces e.g. H J Sternbeck Park
4. Beautify Open Drainage Channels.
5. Permeable Urban Fabric/Consolidate Car Parking/Attract Residents to the CBD/Promote Night-time Economy.
6. Improve Lighting Throughout the CBD.
7. Encourage People to Live + Visit the CBD.
8. Public Art Strategy.
9. Signage + Interpretive Strategy.
10. Upgrade Street Furniture 'Brand'

All of these measures have been reviewed and the majority have been integrated into the preparation of the DCP and PDP documents.

3.7 Review of Economic Opportunities Framework

The Masterplan 'Economic Opportunities Framework' seeks to improve the collective economic outcomes for all CBD stakeholders.

Proposed initiatives include:

- Exploring opportunities to increase density along the 'Main Street' commercial spine;
- Promote weekend markets and events that will draw new visitors to the CBD;
- Continue to improve the appearance and amenity of the historic 'Main Street'; and
- Invest in cultural, civic and community facilities as drawcards and new visitor attractions.

3.8 Implementation Plan Commitments

The Implementation Plan identified ten public domain 'initiatives/Actions' and assigned an 'order of cost', priority, delivery timeframe and responsible stakeholder/s.



Figure 21: Urban Form + Aesthetics Framework

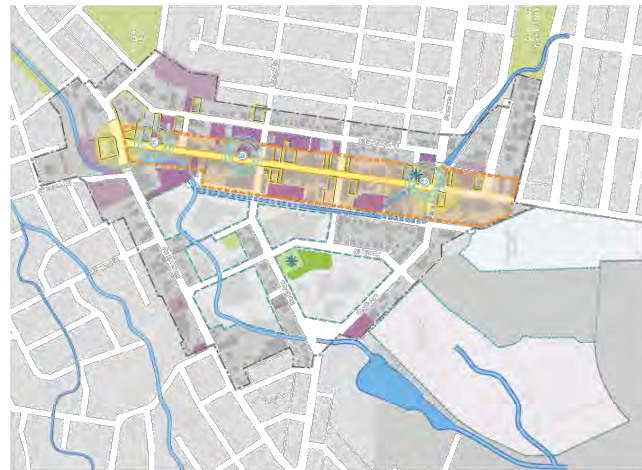
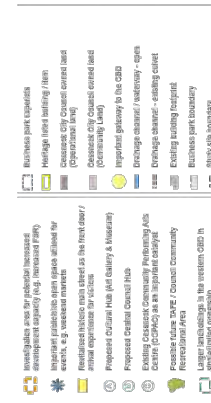


Figure 22: Economic Opportunities Framework





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4.0 Development Options Review

A Development Options Review (DOR) has been undertaken for the Cessnock Commercial Precinct by the AEC Group (AEC). The following is a summary of the DOR study, including identifying the key findings and conclusions, and AEC's recommendations for the future economic planning of the Cessnock Commercial Precinct.

4.1 Background

The precursor to this study, the Supply and Demand Analysis sought to understand the nature of demand for residential and employment floor space in the Precinct, particularly the socio-economic and socio-demographic trends that influence this demand. The analysis additionally examined supply conditions, and if current planning controls facilitate feasible development of required building types and floor space.

The study reviews catalyst development sites identified in the Indicative Layout Plan and explores if they are likely to be financially feasible to develop, or if alternate controls may be required.

4.2 Key Observations

A number of observations emerge from the review of select sites.

- Modest End Sale Values of Residential Units**
Overall, the relatively modest end sale value of residential units constrains the financial feasibility of higher density development. The Cessnock market for higher density living is less established, with households generally displaying a preference for medium density product (e.g. villas, townhouses and duplexes) over higher density product. This market trend is likely to prevail until such time that the benefits of city-living (in the City Centre) result in higher pricing levels for higher density residential product.
- Cost of Site Consolidation**
The financial feasibility of higher density living is additionally constrained by the relatively high cost to consolidate a site in the Commercial Precinct. In instances where a vacant site is able to be consolidated, a lower density threshold is required for feasible development to occur. Where sites are improved (with buildings), a higher FSR is generally required to displace existing uses.
- For non-residential uses, end sale values are generally lower than residential uses. As such, development of non-residential uses is likely to occur on vacant sites or comprise refurbishment / extension of existing space.

- Basement Parking v Deck Parking**
In the case of Council-owned car park sites, if a redevelopment was required to at least replace the car spaces in a basement structure, a relatively high FSR of 4:1 is conceivably required. Should the car spaces be able to be incorporated in a multi-deck structure, a lower FSR of around 2:1 could be sufficient to cross-subsidise delivery of those car spaces. At current pricing levels of residential units, basement car parking is not feasible.

4.3 Conclusion and Recommendations

4.3.1 Observations and Influencing Factors Development and Market Activity

Residential development is observed to be responding incrementally to market need as the population of Cessnock grows. Residential flat buildings and shop top housing are not observed to be widespread. Rather, medium density product (villas, townhouses, duplexes) are observed to be the most

dominant development type in the broader Cessnock area, owing not only to market preference but also the availability of cheaper consolidation opportunities in residential zones.

The broad area of Cessnock generally accommodates a lower socio-economic profile and residential product is accordingly priced. Furthermore, the market for higher density living can be considered to be in its infancy, the current level of pricing not as yet at a level that would incentivise large scale development of higher density product in the Precinct.

In order for a lift in the Cessnock Commercial Precinct's profile and commensurate increase in prices paid for completed residential product, an improvement in the overall amenity of city-living is required. Whilst yet to be witnessed, development in the Precinct and progress of Council's public domain plan will help improve the overall amenity and desirability of the area.

Development and market activity can be observed to be reflective of these market dynamics:

- Development to higher density in the Precinct has only occurred where a site is able to be consolidated for \$500/sqm of site area, or less.
- Lower density development (low rise residential and medium density) observed outside the Precinct, where vacant sites in residential zones are able to be consolidated.
- The principle of substitution can be observed to be working in a number sub-markets. The price of existing detached houses generally forms a ceiling for prices able to be achieved by higher density product. For example, if a three bedroom detached house is available outside the Precinct for \$300,000, there is low prospect of a unit selling for a price in the same order of magnitude. For this reason, the price threshold of new product in some scenarios is below the economic price that is required for feasible development.

Industry	Supply (2016-2041)	Demand (2016-2041, sqm)	2011	2016	2041
Mining	-	3,209	2,576	3,227	
Construction	548	6,542	6,622	9,037	
Manufacturing	-	8,617	8,819	9,413	
Accommodation and Food Services	19,779	10,793	11,574	16,014	
Retail Trade	65,460	34,353	35,135	44,322	
Health Care and Social Assistance	6,916	7,713	8,468	9,729	
Education and Training	327	24,485	24,621	30,148	
Public Administration and Safety	7,029	8,401	8,841	12,300	
Rental, Hiring and Real Estate Services	2,838	1,931	1,706	2,220	
Other Services	12,433	9,036	9,069	12,172	
Financial and Insurance Services	4,451	2,526	2,272	1,819	
Administrative and Support Services	1,515	5,290	5,856	7,211	
Professional, Scientific and Technical Services	3,432	2,835	2,803	3,434	
Transport, Postal and Warehousing	5,968	16,979	17,095	21,871	
Wholesale Trade	1,816	3,400	3,248	2,943	
Arts and Recreation Services	14,969	878	707	754	
Agriculture, Forestry and Fishing	-	3,438	3,659	4,851	
Information Media and Telecommunications	2,056	1,958	1,370	954	
Electricity, Gas, Water and Waste Services	-	1,670	1,625	2,005	
Sub-total	149,226	155,763	157,106	194,565	
Not Specified and Unknown	594				
Total (excluding Residential)	150,820	155,763	157,106	194,565	

Table 1: Employment Floor Supply v Demand, Cessnock City Centre (2011-2041)

Category of Use	2011	2016	2021	2026	2031	2036	2041	Change (2016-41)
Commercial Premises								
Retail	47,113	49,564	52,691	55,791	58,770	60,913	63,001	13,437
Office	16,851	16,324	16,634	17,409	18,224	19,332	19,698	3,374
Business	34,862	35,405	37,475	39,591	41,326	42,122	42,692	7,287
Tourist and Visitor Accommodation	3,624	3,866	4,151	4,433	4,722	5,037	5,377	1,491
Total	102,449	105,179	110,951	117,224	123,042	127,004	130,768	25,589
Retail Premises	47,113	49,564	52,691	55,791	58,770	60,913	63,001	13,437
Shops	24,950	26,170	27,845	29,555	31,167	32,320	33,307	7,137
Food and Drink	7,169	7,688	8,214	8,771	9,342	9,995	10,538	2,950
Bulky Goods	14,994	15,706	16,632	17,465	18,261	19,526	19,095	3,950

Table 2: Employment Floorspace Projections for Specified Categories, Cessnock City Centre (2011-2041)

Dwelling Type	2011	2016	2021	2026	2031	2036	2041	Change (2016-41)
Detached dwellings								
Detached dwellings	1,759	1,842	1,928	2,003	2,081	2,124	2,160	318
Medium density dwellings*	238	261	286	310	335	355	377	116
Flats/units (and shop top housing)	-	4	9	14	19	25	30	26
Other	18	19	20	21	22	22	23	4
Total	2,015	2,126	2,243	2,348	2,457	2,527	2,590	464

*Including dual occupancies, multi-dwelling housing

Table 3: Distributed Dwelling Projections, Cessnock City Centre (2011-2041)

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4.0 Development Options Review

<p>New retail and commercial development is observed to be minimal, not surprisingly given the relative demand v supply situation, where an economy presently outstrips demand. Until market rents are at an economic level, in many instances there is little incentive for landowners of strip retail to reinvest in their shopfronts. A medical centre proposed at 275 Vincent Street takes advantage of the availability of vacant land in the B7 Business Park zone.</p> <p>Existing Uses and Hierarchy of Land Uses</p> <p>Some sites (particularly vacant sites) have better prospects from a financial feasibility perspective if they are in single ownership, i.e. no premium payment is necessary to incentivise numerous landowners to sell. The reality of brownfield or mill development though (and particularly in centres), is that sites are typically small in size and owned by a number of parties, necessitating payment over and above market value.</p> <p>Landowner expectations generally shift with planning controls, i.e. higher expectations accompany higher permissible densities and higher order uses. As an example, B7 Business Park land that permits a wider variety of uses (and potentially a more intensive use of land) is valued higher by the market than IN1 and IN2 zoned land. Industrial uses do not generally respond to density, typically incorporating relatively low densities, after allowing for loading and vehicle circulation space.</p> <p>Land use and intensity of use generally underpins land values. This is due to the different and sale values achievable when sites are redeveloped. A relationship between permitted uses and land values can be observed, all things being equal. This is also referred to as the 'hierarchy of land values'. The following illustrates:</p> <ul style="list-style-type: none"> • Vacant industrial sites (zoned IN1 or IN2) in the Cessnock/Kurti site area. • Kurt general area can range between \$50/sqm and \$75/sqm of site area. • By contrast, vacant business park sites (zoned B7) generally achieve \$75/sqm to \$125/sqm of site area. Under the Cessnock LEP the Business Park zone accommodates a greater diversity of land use and development typologies, compared to industrial zones and is accordingly considered more valuable by the market. <p>Where there is vacant land available and at lower prices compared to improved sites, the market will understandably gravitate to acquiring more cost-effective development opportunities. For this reason, until such time that vacant land within the Cessnock Business Park (B7 zone) is absorbed, it is unlikely that sites along</p>	<p>South Avenue will be taken-up for development opportunities in a B6 zone.</p> <p>Development Activity and Take-up</p> <p>Development take-up is subject to demand-side and supply-side factors. Development activity in Cessnock is generally characterised by residential product and small and medium size developments (<50 dwellings). This reflects the developer profile that is active in Cessnock - smaller builder/developers who are based locally. This is also reflective of the pace of market demand for new residential product. As identified earlier there is minimal development of non-residential uses.</p> <p>The direct relationship between population growth and housing need requires no explanation. In decided where and what they will be accommodated in, households will have regard to factors including cost, location and convenience to their place of work by selecting accommodation of the type and quality within their financial capability.</p> <p>It is effective demand that underpins development activity, developers responding to the willingness and ability of households to pay for new accommodation. Given the comparably lower income profile of households in the Cessnock area, the financial capability of households to pay for housing is fragile.</p> <p>The feasibility of development in Cessnock is generally positive outside the Commercial Precinct where existing uses are cheaper to assemble. Many retail/commercial properties that are well positioned in the core of the Commercial Precinct are at present too valuable to be redeveloped. Development outside the Precinct is expected to endure as developers assemble vacant and dilapidated sites.</p> <p>While poor feasibility at lower densities would appear to suggest that higher densities are required to displace existing uses in the Precinct, higher density development is not without its challenges, particularly as end sale prices of units do not facilitate the provision of basement car parking.</p> <p>New development in Cessnock's Commercial Precinct (both residential and non-residential) is challenged from the perspective of costly site amalgamation and fragile end sale values of completed product. Exceptions to this observation are where a vacant site may have been procured (e.g. 38 Vincent Street), or where a site has been owned for a number of years and is released from land-banking.</p>	<p>4.3.2 Recommendations</p> <p>Market research and feasibility analysis suggest that higher density corridors do not necessarily incentivise change on all sites. Vacant sites and sites with minimal buildings present the most immediate opportunities for redevelopment.</p> <p>While planning and development controls are intended at stimulating redevelopment and renewal in Cessnock, large scale development is difficult and challenged for a number of reasons. The availability of suitable sites is a major impediment to development in any established urban area. In some instances market cycles and factors (e.g. valuable existing uses, poor effective demand) are the reason for the lack of development activity, this beyond the control of planning authorities.</p> <p>Not all development will be feasible in the Commercial Precinct (particularly where existing uses are valuable or sites are fragmented in ownership). These are market issues, beyond the control of planning authorities.</p> <p>We recommend Council consider the following:</p> <ul style="list-style-type: none"> • Improve walkability within the Precinct Each of the retail sub-precincts within the Precinct are inward focused, leading to a lack of connectivity and synergy within the Precinct. Travel is very much car-based, there being much opportunity for pedestrian foot traffic to improve patronage levels in the Precinct even without increasing the number of visitors to the Precinct. • Invest in public domain and aesthetic presentation Outward presentation and impressions of an area are critical to business and consumer confidence. The investment by Council into the public domain (be it improved signage, street furniture and lighting, pedestrian amenity, etc.) will make the Precinct a more pleasant place for consumers to visit and linger and for businesses to invest. • Work with landowners keen to upgrade/refurbish their properties Anecdotal feedback from local real estate agents suggests some frustration by landowners during the DA assessment process. A smooth process that facilitates reinvestment into existing buildings will benefit renewal outcomes of the Precinct.
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5.0 Urban Form Analysis

5.1 Introduction

This urban form analysis is informed by the Cessnock CBD Masterplan and the Supply and Demand Analysis undertaken by the AEC Group. The Analysis explores precinct development at timelines of 2021, 2031 and 2041, and addresses planning, 3D built form and development yield for each scenario.

The urban form strategy has considered the protection of heritage buildings, key view corridors, development incentives to encourage best practice urban design, the pedestrian and cycle network of the Precinct, and the strategic location of public car parking in the CBD.

5.2 Supply and Demand Analysis Outcomes

The Supply and Demand Analysis and Development Options Review undertaken for the Cessnock Commercial Precinct by the AEC Group, projected that there would be modest growth in retail, office, business and tourist and visitor accommodation over the next twenty five years.

The resulting Precinct building footprints and urban form will progressively evolve over the period rather than radically transform.

Retail, Office/Business and Tourist / Visitor Accommodation

An audit of existing Retail Trading space in the Commercial Precinct identified approx. 65,000sqm available in 2016, whilst the demand for Retail Premises is only projected to reach approx. 63,000sqm in 2041. In terms of future supply, the current allocation (and general footprint) of retail floor space in the Precinct is adequate, and could theoretically be left unchanged into the foreseeable future.

On face value, retail will not be a development 'driver' for the Centre over the next 25 years – with no impact on the Precinct urban form. However, several factors could potentially alter this scenario:

- Tourism Driven Demand** - there is the potential to grow tourism within the Precinct (that does not rely on population growth) and thereby encourage private sector redevelopment of 'Main Street' sites (for café, restaurant and accommodation uses), and also 'drive' the development of cultural and community facilities, by the public sector, such as new art gallery, library facilities, and upgrade to Council's administration offices.
- Wines and Mine - Visitor Centre** - the provision of a drawcard and architecturally striking facility that would attract visitors from Sydney, Newcastle and regional NSW, (recommended for the

former TAFE site) would bring back to life the west of the Precinct.

- Regional Shopping Centres** - the Commercial Precinct is an important and successful retail shopping centre hub serving the region. Consolidation, development and measured growth of these centres over the next 25 years could provide opportunities to improve the urban form of the west of the Precinct, linking the Vincent Street 'spine', west to the shopping centres, via the east-west Cooper Street axis. And also creating a new urban place within the Centre - Cessnock Town Square, Bypass Route Development - the reconfiguring of properties due to the southern bypass and the new exposure properties would enjoy from passing vehicles, would likely trigger new development to emerge along the route i.e. South Avenue / Snape Street and within the west of the part of the Business Park.
- 'Main Street' Retail Focus** - the overall quantum of retail space is unlikely to change significantly, however the quality of the retail on offer is envisaged, and should be encouraged, to improve and develop over time. It is recommended, due to the extended length of Vincent Street, to focus street retail activity to the northern city blocks, and to strengthen connections west along Cooper Street to the Coles, Woolworths and IGA shopping centres.

The small projected increase in Office and Business floor space over the next 25 years can easily be accommodated in either the upgrade of existing 'Main Street' premises or in the development of new buildings on the currently vacant business park lands.

Residential Dwellings

Projections for new dwellings, once Detached Dwellings are excluded (that would occur outside of the study area), indicate a very small number (146) of new dwellings would be constructed in the Commercial Precinct over the next 25 years. New dwellings will consist of Medium Density residences and Apartments (incl. shop top housing). A small number of 'Main Street' consolidations may occur, whilst the former Cessnock Baths site and a smattering of other CBD sites will likely be developed.



Figure 23: Existing Built Form - View from North West



Figure 24: Existing Built Form - View from South East



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5.0 Urban Form Analysis

5.3 Commercial Precinct Urban Development

The preferred Commercial Precinct urban form has considered the strategic urban planning objectives for the Precinct, and has specifically addressed the following criteria:

View Corridors

The CBD Masterplan identified the key views and vistas to be considered in the planning of the Precinct. Refer to Figure 28. Key CBD street views include Vincent Street north to Cessnock Hotel, and west along Cooper Street and Hall Street. Also along Aberdare Road, west to the Royal Oak Hotel.

Pedestrian + Bicycle Network

The Active Transport Strategy is illustrated in Figure 87, and includes key pedestrian connections and through-site links and the proposed cycleway network. The enhancement of the network of 'on' and 'off-road' cycleways and parking facilities is consistent with the Cessnock Bicycle Strategy (2016) and will prioritise pedestrian and bicycle access, connectivity and safety in the Commercial Precinct. The Precinct urban form supports this access network and the Public Domain Plan integrates cycleways into the proposed street cross sections.

Public Car Parking Areas

The Public Car Parking Strategy is illustrated in Figure 77, and includes six Council owned sites that are set back from the Vincent Street 'Main Street', yet still within short walking distance of the retail spine - accessed from side or parallel streets and by pedestrians via laneways. These public car parking areas are well situated to support the 'Main Street' retail whilst not taking up prime retail frontage. The two Charlton Street public car parks also support the Woolworths and Coles shopping centres.

Any future upgrade of the Coles and Woolworths shopping centre developments would require a rethinking of car parking arrangements. Opportunities exist for shopping centre redevelopment to establish active street frontages, to better define CBD streets, and to support Council's objective to reinforce the Cooper Street axis and to establish a new Town Square. The square would provide a new meeting place, social focus and venue for events, enriching the public domain experience.

In the future, multi-level car parking structures could be constructed on these sites if demand warrants. Multi-level car parking structures can, if not designed well, become eyesores when viewed from

city streets. It is recommended that the ground floor level of such structures that face street frontages, are activated with retail or community uses, to bring activity and passive surveillance to the city streets, and that the rest of the structure is well designed and incorporates landscape treatments, such as planters, green walls or contemporary artwork.



Figure 25: Mountain View West along Hall Street



Figure 26: Mountain View West along Cooper Street (Source: Google)



Figure 27: View Corridor - Vincent Street North to Cessnock Hotel (Source: Google)



Figure 28: View Corridor Map (Source: Cessnock CBD Masterplan Background Report 2012)

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5.0 Urban Form Analysis

5.4 Urban Design Strategy

5.4.1 Existing Urban Form

The existing Precinct built form comprises a mix of building scales, setbacks and frontage treatments. The Precinct 'Main Street', Vincent Street is the most consistent and continuous urban streetscape in the Precinct, extending over four city blocks.

The urban definition of city streets and blocks diminishes as you progress down the city cross streets to the west and east of Vincent Street. And the built form is predominantly low scale detached villas in the residential streets in the north, south and western extents of the Precinct.

Whilst the predominant building height at the street frontage is one or two storeys, the overall height and scale of building varies significantly between the finer urban 'grain' of most Vincent Street buildings to the 'large box' architecture of the shopping centres located to the west and of the big box retail development e.g. Burnings Warehouse, located in the south of the Precinct.

The tallest buildings in the Precinct are the recently completed three storey mixed use development at the corner of Cooper Street and Darwin Street, the one storey shopping centres in the west of the Centre, the Burnings Warehouse development to the south and the higher, setback portion of the Cessnock Performing Arts Centre (CPAC) on the 'Main Street'.

Generally a low scale is maintained throughout the Precinct, allowing heritage buildings along Vincent Street to generally retain their prominence and integrity. With the exception of Vincent Street, there is a concern in regard to the poor definition of most city streets, particularly in the west of the Precinct, where many streets are exposed and poorly defined.

The 12m height limit has resulted in a reasonably uniform built expression and street scale for both heritage (generally 1-2 storey buildings of large floor-to-floor height) and more contemporary mixed use developments or large scale retail buildings (often 1 storey high with very large floor-to-floor heights). Along Vincent Street, and extending into some side streets, there is generally a consistent street wall height, often marked by older building parapet details.



Figure 30r: Recently Mixed Use Development



Figure 31r: Cessnock Performing Arts Centre

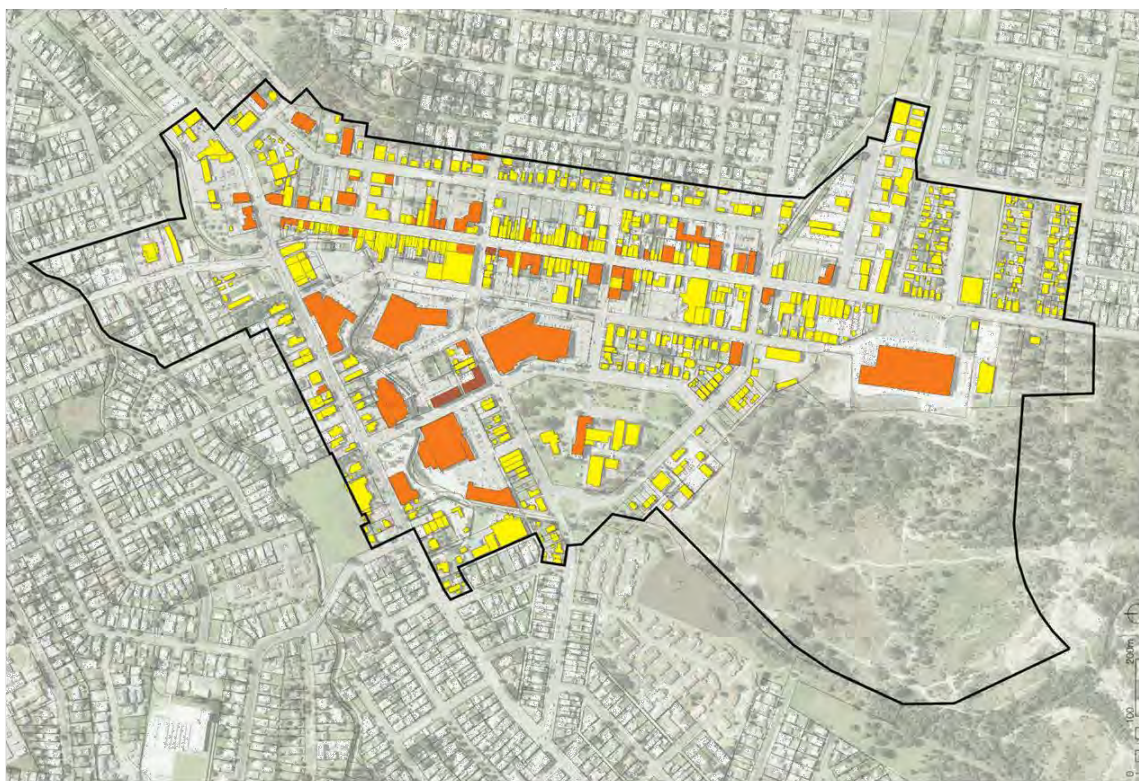


Figure 28r: Existing Building Heights

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5.0 Urban Form Analysis

5.4.2 Recommended Urban Form Strategy
The focus will be to reinforce the well-defined Vincent Street 'Main Street' urban 'grain' and to focus development and street activity in the northern blocks of Vincent Street – between Hall Street and Melville Road.

Cooper Street
Cooper Street is identified as a key street to link the two retail precincts – the Vincent Street 'Main Street' retail and the cluster of shopping centre developments to the west. New development should define Cooper Street west of Vincent Street, as a desirable and active CBD street, linking west.

Town Square
The proposed Town Square would establish a central meeting place within Cessnock, providing an attractive destination for the community and for visitors travelling to the area. When combined with a strengthening of the Cooper Street east-west link, the Town Square has the potential to draw together the most successful retail elements of the Centre (the Main Street and the shopping centres) into a new integrated whole.

The urban form opportunity is to define the Town Square space in the urban fabric of the CBD through the modification and redevelopment of the existing shopping centre redevelopment frontages and car parking arrangements. All development that fronts the square should be activated with retail or similar activity generating uses.

Building Height
The low growth projections generally also result in a lack of demand for additional height in the CBD. Some interest by the owner of a vacant site to redevelop (between Vincent Street and Cumberland Street) and Council's future plans to potentially expand their office accommodation on their current site has been accommodated by a proposed increase in allowable building height (to a maximum of six storeys) and in conjunction with an increased FSR for this northern city block.

Heritage Character
Vincent Street heritage buildings are key definers of the existing street character, are generally one or two storey in height, and feature articulated facades and ornate parapets. Developments should respect adjoining heritage buildings, maintain an appropriate street scale, and reinforce the predominant street wall height, and parapet and awning lines.



Figure 32: Existing Cooper Street



Figure 33: Existing Cessnock City Centre

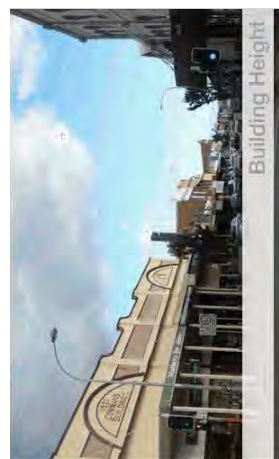


Figure 34: Existing Intersection of Vincent Street and Cooper Street



Figure 35: Active Street Frontages at House Hill Town Centre



Figure 36: House Hill Town Square



Figure 37: New Development along Main Street in Murrumbidgee

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5.0 Urban Form Analysis

Best Practice Urban Design - Incentives

Building height, development density or land use are not useful tools to incentivise best practice Urban Design outcomes in the Cessnock Commercial Centre, due to the generally weak market conditions present. However, there is the potential to incentivise desirable development outcomes through permitting lower car parking rates in new developments that achieve Design Excellence or provide Affordable Housing.

It is proposed that additional height is permitted, not as an incentive, but rather to:

- Reinforce the Vincent Street commercial 'spine' and the retail 'core' of the Precinct; and
- To build-in to the planning controls a degree of flexibility, and to not preclude higher development if particular site-specific economic factors support taller development in the Precinct.

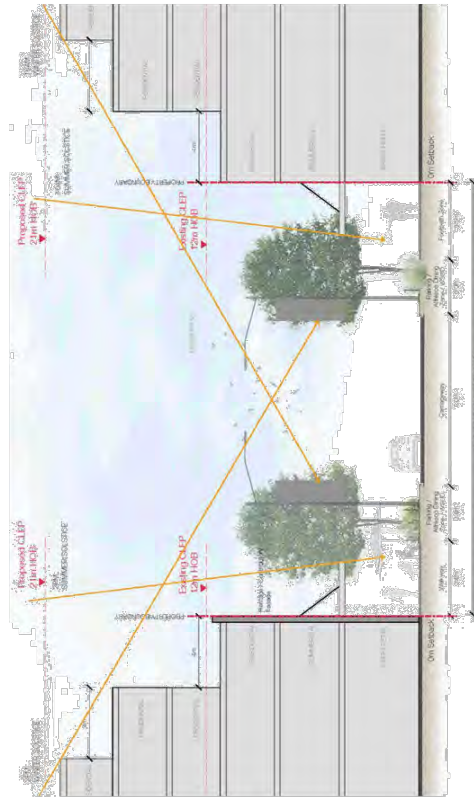


Figure 88: Main Street Section

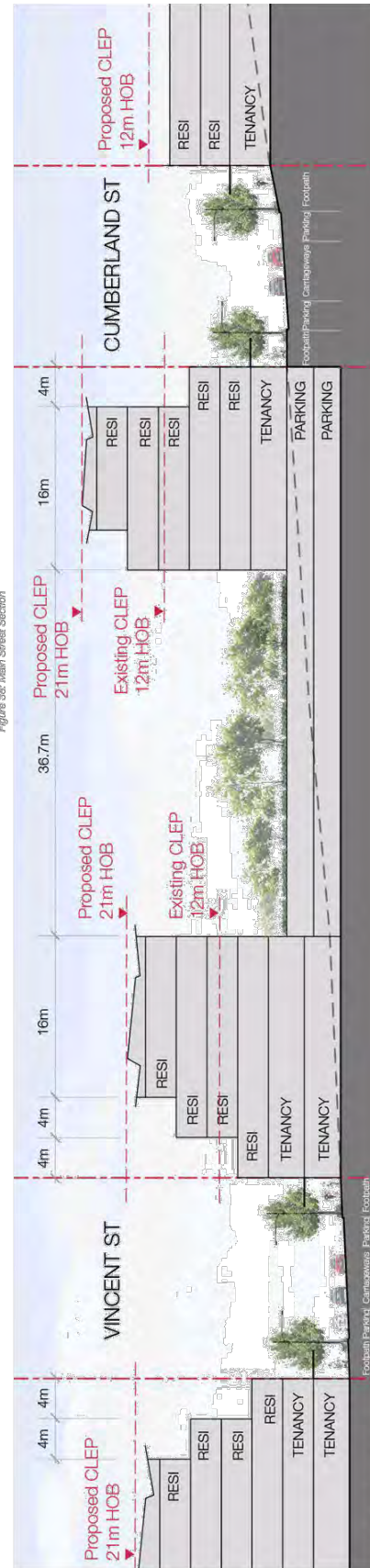


Figure 89: Vincent Street Section

5.0 Urban Form Analysis

5.5 Development Scenarios

5.5.1 Projected 5 Year Urban Form

Figure 40 illustrates a typical growth scenario for the Commercial Precinct in five years' time, based on the projected development floor space, established in the Supply and Demand study prepared by AEC Group.

Current vacant sites along Vincent Street are envisaged to be redeveloped first, whilst it is hoped that one of the shopping centres will also be redeveloped in order to start defining a new Town Square for the CBD.

It is envisaged that the TAFE facility will reopen for educational purposes, in conjunction with new civic uses on the site, such as a multi-purpose Cultural Centre, including a Mine and Wine Centre creating a new venue that would draw visitors from Sydney and become a regional attraction. A new Cessnock Library facility could also be part of the Cultural Centre facility. Other possible library sites include on Charlton Street adjacent to the proposed Cessnock Town Square with a new multi-level public carpark above, or as part of the existing CPAC cultural hub, incorporating Cessnock Art Gallery located on Council's currently vacant Vincent Street property.

Cessnock City Council administration building could be expanded on the existing site, incorporating a multi-level car parking facility with retail/community uses at the ground floor level street frontages.

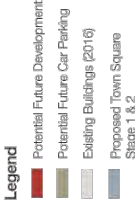
Projected Net Growth at 5 Years:

Retail	3,150 m ²
Office	300 m ²
Business	2,050 m ²
Tourist	300 m ²

Residential	
Medium density dwellings	25
Flats/units/shop top housing	6



Figure 40: Indicative 5 Year Growth Scenario



5.0 Urban Form Analysis

5.5.2 Projected 15 Year Urban Form

Figure 41 illustrates a typical growth scenario for the Commercial Precinct in fifteen years' time, based on the projected development floor space, established in the Supply and Demand study prepared by AEC Group.

It is anticipated that by the end of this stage the second shopping centre would have been redeveloped to complete the full extent of the new Town Square. And the first stage of Cessnock Library and Art Gallery would be completed. The former Cessnock Baths site would have been sold, and redeveloped as a mixed use precinct. Several apartment developments, consolidating several smaller properties along Vincent Street, and Cumberland Street, are envisaged over this timeline.

Projected Net Growth 6-15 Years:

Retail 6,100 m²
Office 1,600 m²
Business 3,850 m²
Tourist 600 m²

Residential

Medium density dwellings 50
Flats/units/shop top housing 10



Figure 41: Indicative 15 Year Growth Scenario

Legend

- Potential Future Development
- Potential Future Car Parking
- Existing Buildings (2016)
- Proposed Town Square Stage 1, 2 & 3

5.0 Urban Form Analysis

5.5.3 Projected 25 Year Urban Form

Figure 42 illustrates an estimated growth scenario for the Commercial Precinct in twenty five years' time, based on the projected development floor space, established in the Supply and Demand study prepared by AEC Group.

At this stage, it is envisaged that some further site consolidations along Vincent Street would allow some additional mixed use developments to be realised, with additional accommodation and commercial development to occur along the proposed South Bypass route and along Wollombi Road.

Projected Net Growth 15-25 Years:

Retail	4,250 m ²
Office	1,500 m ²
Business	1,950 m ²
Tourist	650 m ²

Residential	
Medium density dwellings	40
Flats/units/shop top housing	10



Figure 42: Indicative 25 Year Growth Scenario



6.0 Public Domain Infrastructure Audit

6.1 Introduction

This audit maps and assesses the condition of all public domain infrastructure located within the Cessnock Commercial Precinct study area, including pedestrian paving, street furniture, wayfinding and interpretive signage, public art and car parking infrastructure.

Vincent Street

Cessnock's retail 'Main Street' is the only street in the centre to have been upgraded and the improvements extend four city blocks from Maitland Road, in the north, to Aberdare Road, in the south. A 'family' of related streetscape elements has been implemented in stages over the past decade and in recent years the landscaping has been further refined and commemorative plaques installed highlighting local people of note in the community.

The upgraded streetscape includes honed concrete unit paving with coloured banding/frame, new street trees and roadside planter beds, lighting and street furniture, including seat benches, bollards, outdoor café seating areas, defined by low walls/fences and by trellises, litter bins, and a laneway artwork program has been implemented.

Assessment

The footpath paving is slightly 'dated' in design, however is in good condition, functional, robust, and generally low maintenance. Street crossings are provided at regular intervals along Vincent Street, are safe signalled crossings (some scramble crossing type) and incorporate pedestrian ramps.

Street trees and kerbside planter beds are generally effective in bringing 'green', shade and colour to the street experience. Ornamental grape vines on trellises, and out-out designs are an effective means of developing local identity and highlighting Cessnock's unique Hunter Valley location. No references to the local coal mining industry are evident.

Outdoor café seating areas on Vincent Street are at times utilised successfully, however at some locations are not used at all. In these situations the grey block walls are visually prominent and detract from the streetscape.

No Water Sensitive Urban Design (WSUD) measures, or ESD initiatives, such as litter recycling or reuse/recycling of materials in furniture - such as timber in seating, or the use of energy efficient luminaires, such as LED fittings are evident.

Generally, street lighting is provided by standard 'goose neck' tapering natural galvanised steel poles with traditional luminaires - a functional, low cost road lighting solution, however not ideally suited to a lower scale pedestrian environment, nor is it integrated with traffic signals or road/street signage, resulting in some footpath clutter. Banners are obvious add-ons, rather than integrated with the pole design. Some pedestrian scale pole top light fittings are provided in key cross streets such as Cooper Street.

Whilst the signature furniture colour reflects Council's corporate branding/identity, light colours such as this are often not ideally suited to street furniture items, such as steel framed items, that preferably utilise a dark colour palette.

Other Precinct Streets (Commercial + Residential)

Beyond the Vincent Street 'Main Street' spine, the furniture of the public domain is basic and functional - concrete footpaths, timber light poles, standard street and road signs, and basic street tree planting, and the limited provision of standard 'off the shelf' street furniture.

Infrastructure Audit

The audit undertaken in August 2016, covers every street in the study area, and was assessed on a street-by-street basis. The type, extent and number of infrastructure items have been summarised in three maps covering the north, middle and south of the precinct. Upgrade opportunities for each street where also identified, informing the subsequent development of the Public Domain Plan.



Figure 45: Bench Seating



Figure 44: Bollards

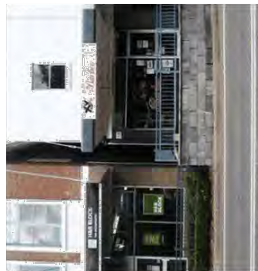


Figure 46: Cable Sealing Fences + Wall



Figure 46: Litter Bin



Figure 47: 'Main Street' paving



Figure 48: Pedestrian Fence



Figure 46: Street Light Poles

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6.0 Public Domain Infrastructure Audit

6.2 Audit of North of Precinct

Apart from Vincent Street, there is only limited existing public domain infrastructure, with some upgrade works completed for key cross streets, such as Edward Street. Generally a low level of amenity is afforded users, businesses and residents of major precinct streets, including Wollombi Road/Mallard Road and Cumberland Street. Charlton Street, Kerrie Street or Darwin Street.

To the west of Vincent Street adhoc open car parking areas dominate resulting in a confusing and uneasy public domain experiences. The laneway that leads west to Woolworths is brought to life by public art display, however, at its west end the laneway delivers pedestrians in a confused 'back of house' parking area.

Street tree planting within the study area (except for Vincent Street) is often non-existent, sparse or adhoc in nature – with some residential streets having impressive avenue planting, whilst others with almost no trees, are hot and exposed environments.

Streetscape	Infrastructure Type	Audit of Existing Infrastructure (15 August 2016)	Upgrade Opportunity
Vincent Street - North (40km/hr speed limit - designated HPA)	Paving	Special 'Main Street' paving - honed coloured concrete unit pavers with three colour banding and edge details, footpath widening (outdoor seating areas), plant verge at most crossing points, tactile indicator paving.	Carry special paving down laneways for continuity. Investigate resurfacing of VSD opportunities.
	Lighting	Underground power supply, standard tapering steel 'gooseneck' light poles along both sides, roundabouts at future crossings, vertical pole top light at the HJ Stenbeck Lane Park.	Investigate use of LED luminaires for efficiency, long life, low maintenance. Integrate existing/retained luminaires into new car parking areas behind 'Main Street' to improve visibility. Introduce feature lighting for key retail buildings.
	Street Furniture	Standardised bench seats and litter bins provided along both sides of street, continuous steel bollards along west side of Vincent Street, between Edward Street and Cooper Street, street tree planting and planters, 'Hill of Fame' plaques in planters, temporary private cafe chairs/seats, utility boxes, PO boxes, telephone booths, Roads + Maritime signage, picnic table/bench seats at the HJ Stenbeck Lane Park.	Introduce recycling composition. Provide bench/seats entry sign at north 'gateway' to Centre (i.e. at Mallard Road corner park). Introduce directional signage to supermarkets and parking areas.
	Outdoor Seating Areas	Honed aggregate concrete block walls with sandstone colour concrete capping unit and metal pedestrian fences near street crossings.	Review locations for footpath widening to ensure alignment with cafe/food tenancies.
	Signal Crossings	Signalised intersections at Mallard Road and Cooper Street ('remember' type crossing)	Roads + Maritime to review signal timing if Bypass proceeds.
	Zone Crossing	Edmond Street intersection and mid-block between Edward Street + Cooper Street	Investigate potential for raised pedestrian crossings.
	Drainage	Philip Lane - Public art on laneway walls	Support + extend this successful initiative to include laneway light installation networks.
	Street Trees	Standardised tree species (e.g. Grevillea, Eucalyptus, Casahuate, etc.) - Cessnock City Council (3 total). Located in small groups along 'Main Street' (11 total).	Review landscaping of HJ Stenbeck Lane Park and north 'gateway' park (at Mallard Road intersection).
Wade Street	Paving	No footpath.	Provide standard concrete footpath.
	Lighting	Standard timber light poles (2 total).	Review lighting levels with utility.
Sperry Street	Street Trees	Significant existing street trees.	Consolidate existing avenue.
	Paving	No footpath.	Provide standard concrete footpath.
	Lighting	Standard timber light poles (2 total).	Review lighting levels with utility.
Ferguson Street	Street Trees	Limited planting, no trees on south side.	Plant trees on both sides to create avenue.
	Paving	Standard concrete paving to position of north side.	Complete standard concrete footpath.
	Lighting	Standard timber light pole (1 total).	Review lighting levels with utility.
	Street Trees	Limited street trees.	Plant trees on both sides to create avenue.
Alaridale Road	Paving	Standard concrete paving along both sides.	Maintain existing concrete footpaths.
	Lighting	Standard timber light poles (7 total) - separating street 'gooseneck' light poles (1 total).	Review lighting levels with utility.
	Street Trees	Exposed landscape with limited street tree planting.	Plant trees on both sides to create avenue.
Westcott Street	Paving	Standard concrete paving along both sides.	Maintain existing concrete footpaths.
	Lighting	Standard timber light poles.	Review lighting levels with utility.
	Street Trees	Exposed streetcape with limited street tree planting.	Plant trees on both sides to create avenue.
Wollombi Road + Mallard Road	Paving	Generally standard concrete paving along both sides, special 'Main Street' paving at Vincent Street intersection.	Consolidate/repair existing concrete footpaths, special paving treatments based on 'Main Street' paving palette to highlight 'gateway' role.
	Lighting	Generally standard timber light poles (23 total) with tapering steel 'gooseneck' light poles (6 total) at Vincent Street intersection.	Provide lighting at Vincent Street 'gateway' park.
	Street Trees	Exposed roadway with limited street tree planting.	Plant trees on both sides to create avenue, landscape median + roundabout islands. Provide planting and signage at Vincent Street 'gateway' intersection.
Millfield Street	Paving	Generally standard concrete footpaths - short section of asphalt footpath.	Complete footpaths on northeast side.
Edward Street	Lighting	Standard timber light poles (3 total).	Review lighting levels with utility.
	Paving	Special 'Main Street' paving near Vincent Street intersection, standard concrete paving along the rest of Edward Street.	Extend 'Main Street' paving through to Cumberland Street in the long term.
	Lighting	Tapering steel 'gooseneck' light poles (1 total) + standard timber light pole (1 total).	Investigate use of LED luminaires for efficiency, long life, and low maintenance.
	Street Furniture	None.	Maintain seating.
Darwin Street - North	Street Trees	Standard timber light pole (1 total).	Plant trees on both sides to create avenue.
	Paving	Generally standard concrete footpaths on both sides, full width adjacent Leagues Club, special 'Main Street' paving at Vincent Street intersection.	Maintain existing footpaths, upgrade to full width as redevelopment takes place.
Charlton Street - North	Lighting	Very few street trees.	Investigate use of LED luminaires for efficiency, long life, and low maintenance.
	Street Trees	Short length of special 'Main Street' paving on east side at former cinema.	Provide full width footpaths for entire length of street.
	Paving	'Tapering steel' 'gooseneck' light pole (1 total).	Review street lighting levels and coverage to reinforce safety.
	Street Furniture	Bench seats located at roundabout (2 total).	Maintain seating, provide further seating as required.
	Street Trees	Generally exposed environment, four trees on east side.	Plant trees on both sides to create avenue (area permitting). Landscape roundabout island.
Cumberland Street - North	Paving	Generally standard concrete footpaths on both sides, special 'Main Street' paving at the Cooper Street intersection.	Provide full width footpaths for entire length of street.
	Lighting	Tapering steel 'gooseneck' light poles (2 total) - standard timber light pole (1 total).	Investigate use of LED luminaires for efficiency, long life, and low maintenance.
	Street Trees	Very few street trees.	Plant trees on both sides to create avenue.

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6.0 Public Domain Infrastructure Audit

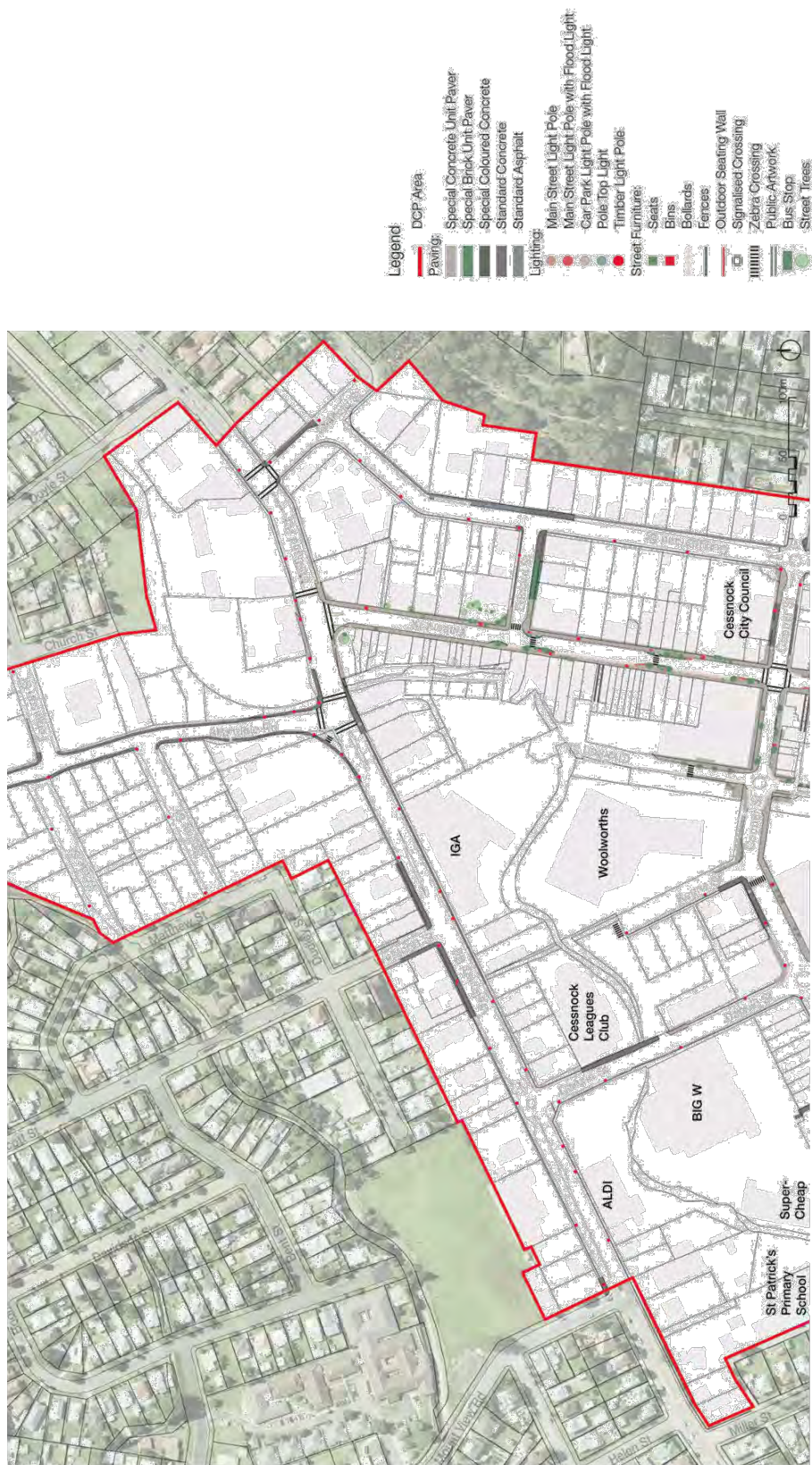


Figure 50: Public Domain Infrastructure Audit Plan - North

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6.0 Public Domain Infrastructure Audit

6.3 Audit of Middle of Precinct

Again, beyond Vincent Street, there is limited existing public domain infrastructure provided in the cross streets, with Cooper Street, and parts of Hall Street and Snape Street/Aberdare Road the exceptions. Many CBD streets to the west only have street lights, and often have no footpaths, trees or furniture. CPAC Plaza is well designed and performs well as a focus of activity pre and post-performance.

A low level of amenity is afforded users, businesses and residents of many streets, including Cooper Street, Hall Street and Snape Street/Aberdare Road, and Cumberland Street, Charlton Street, Darwin Street, and North and South Avenues. The TAFE site lacks a viable and connected footpath system.

To the west of Vincent Street adhoc open car parking areas and drainage canal infrastructure dominate, resulting in a confusing, undefined and unsafe public domain experience. Two laneways lead west to Coles supermarket via a confused 'back of house' public parking carpark. The public realm is poorly defined and car dominated. Orientation is confusing and safe passage for pedestrians not clear.

Street tree planting in the middle of the Precinct (except for Vincent Street) is often non-existent, sparse or adhoc in nature – most streets have very few trees, and are therefore exposed, glary and hot in summer.

Streetscape	Infrastructure Type	Audit of Existing Infrastructure (15 August 2016)	Upgrade Opportunity
Vincent Street - Middle of Vincent Street - Designated PPAN	Paving	Special 'Main Street' paving - tiered coloured concrete unit pavers with three colour banding and edge details, cobble paving (paving) and green grass pavers at most crossing points, tactile indicator pavers, special concrete paving at CPAC.	Carry special paving down laneway to continuity, investigate resurfacing of WILD opportunities.
	Lighting	Underground power supply, standard tapering steel 'greenneck' light poles along both sides, floodlights at extra crossings.	Investigate use of LED luminaires for efficiency, long life, low maintenance, integrate lighting with pedestrian barriers and light pole design. Integrate lighting of laneways and car parking areas behind 'Main Street' to improve safety. Introduce feature lighting for key heritage buildings.
	Street Furniture	Steel/fibre bench seats and litter bins provided along both sides of street, continuous steel bollards along west side of Vincent Street, between Cooper Street and Hall Street, street tree planting and planters, 'Hill of Fame' plaques in planters, temporary private car constraints, utility boxes, PO boxes, telephone booths, RMS signage.	Introduce recycling programme. Introduce directional signage to TAFE, CPAC, supermarkets and parking areas.
	Outdoor Seating Areas	Formal separate concrete block walls with stainless steel concrete cladding and metal pedestrian bollards near extra crossings.	Review locations for bollards (existing) to ensure alignment with safe foot crossings.
Cooper Street	Signal Crossings	Signalised intersections at Cooper Street/Vincent Street, Hall Street/Vincent Street, Vincent Street/Hall Street.	RMS to review signal timing if bypass processes.
	Extra Crossings	Mid-blocks between Cooper Street and Hall Street, Hall Street and Aberdare Road.	Investigate potential for raised pedestrian crossings.
	Public Artwork	Art Wall Lane and Sculpture Lane - public art on laneway walls.	Support a wider public art initiative to include laneway light installation artwork.
	Street Trees	Located in small groups along 'Main Street' (3 total).	Review landscaping and plant street trees in Vincent Street between Hall Street and Snape Street.
Cooper Street	Paving	Generally 'special' 'Main Street' paving along both sides, street section of standard concrete paving between Vincent Street and Cumberland Street.	Maintain existing footpaths, upgrade standard concrete paving to special 'Main Street' paving.
	Lighting	Underground power supply, standard tapering steel 'greenneck' light poles along both sides, feature pole top lights along south side of Cooper Street between Charlton Street and Vincent Street.	Investigate use of LED luminaires for efficiency, long life, low maintenance, integrate feature/light bollards into light pole design.
	Street Furniture	Steel/fibre bench seats and litter bins provided along both sides of street, continuous steel bollards along west side of Cooper Street, between Charlton Street and Vincent Street, street tree planting and planters, 'Hill of Fame' plaques in planters, temporary private car constraints, utility boxes, PO boxes, telephone booths, RMS signage.	Introduce recycling programme. Introduce directional signage to supermarkets and parking areas.
	Outdoor Seating Areas	Grape leaves patterned steel bollards located at south side of Cooper Street between Charlton Street and Vincent Street.	Review footpath widening at north side of Cooper Street between Charlton Street and Vincent Street to provide more outdoor seating areas.
North Avenue	Signal Crossings	Signalised intersections at Vincent Street/Vincent Street, Vincent Street/Hall Street.	RMS to review timing at intersection (see above) and if bypass processes.
	Extra Crossings	Mid-blocks between Vincent Street and Hall Street, Hall Street and Aberdare Road.	Investigate potential for raised pedestrian crossings at Charlton Street to replace roundabout.
	Paving	Generally standard concrete footpaths on both sides, special 'Main Street' paving at Darwin Street intersection and continuous south side to Cooper Street.	Maintain existing footpaths, upgrade street section of standard concrete paving close to Snape Street intersection to special 'Main Street' paving.
	Lighting	Standard timber light poles (6 total) - tapering steel 'greenneck' light poles (4 total).	Review lighting levels with utility.
Hall Street	Street Furniture	Steel/fibre bench seats and litter bins provided along south side of North Avenue between Darwin Street and Kenna Street. Steel bollards located at two corners of roundabout intersection.	Provides townships entry sign at west 'gateway' to Centre i.e. at South Avenue corner park. Introduce directional signage to supermarkets and parking areas.
	Extra Crossings	Kenna Street intersection.	Investigate potential for raised pedestrian crossings.
	Street Trees	Located in small groups along North Avenue (4 total).	Review landscaping of TAFE grounds and west 'gateway' park (South Avenue intersection), plant street trees on North Avenue.
	Paving	Generally standard concrete footpaths on both sides, special 'Main Street' paving at Vincent Street intersection.	Upgrade paving between Charlton Street and Cumberland Street to special 'Main Street' paving.
Snape Street + Aberdare Road	Lighting	Standard timber light pole (5 total) - tapering steel 'greenneck' light pole (1 total).	Investigate undergrounding of power supply and upgrade to steel light poles.
	Paving	Standard concrete paving along both sides of Aberdare Road, special 'Main Street' paving at Vincent Street.	Investigate undergrounding of power supply and upgrade to steel light poles.
	Street Trees	Standard timber light poles (2 total) - tapering steel 'greenneck' light poles (3 total).	Extend existing planting on both sides to reinforce avenue.
	Lighting	Standard timber light poles (4 total).	Provides standard concrete footpath.
South Avenue	Paving	No footpath.	Review lighting levels with utility.
	Street Trees	Standard timber light poles (4 total).	Provides standard concrete footpath.
	Lighting	Standard timber light poles (1 total).	Review lighting levels with utility.
	Paving	Standard concrete paving along both sides of South Avenue.	Provides standard concrete footpath.
Bridge Street	Lighting	Standard timber light poles (1 total).	Review lighting levels with utility.
	Street Trees	Standard timber light poles (1 total).	Provides standard concrete footpath.
	Paving	Standard concrete paving along both sides of Bridge Street.	Provides standard concrete footpath.
	Lighting	Standard timber light poles (1 total).	Review lighting levels with utility.
Darwin Street - South	Paving	Standard concrete paving along both sides of Darwin Street.	Provides standard concrete footpath.
	Street Trees	Standard timber light poles (1 total).	Review lighting levels with utility.
	Lighting	Standard timber light poles (1 total).	Provides standard concrete footpath.
	Paving	Standard concrete paving along both sides of Darwin Street.	Provides standard concrete footpath.
Charlton Street - Middle	Lighting	Standard timber light poles (1 total).	Review lighting levels with utility.
	Street Trees	Standard timber light poles (1 total).	Provides standard concrete footpath.
	Paving	Standard concrete paving along both sides of Charlton Street.	Provides standard concrete footpath.
	Lighting	Standard timber light poles (1 total).	Review lighting levels with utility.
Cumberland Street - South	Paving	Standard concrete paving along both sides of Cumberland Street.	Provides standard concrete footpath.
	Street Trees	Standard timber light poles (1 total).	Review lighting levels with utility.
	Lighting	Standard timber light poles (1 total).	Provides standard concrete footpath.
	Paving	Standard concrete paving along both sides of Cumberland Street.	Provides standard concrete footpath.

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6.0 Public Domain Infrastructure Audit



Figure 51: Public Domain Infrastructure Audit Plan - M2036

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6.0 Public Domain Infrastructure Audit

6.4 Audit of South of Precinct

The upgraded section of Vincent Street tailors off at the Shape Street/Abardere Road intersection. To the south and west of the 'Main Street' retail spine, there is limited public domain infrastructure. A low level of amenity is afforded users, businesses and residents of precinct streets, including Vincent Street (south), Charlton Street (south), Cessnock Street, Railway Street, Edgeworth Street and Gordon Avenue.

To the west of Vincent Street large open car parking areas associated with the Bunnings warehouse development, dominate, resulting in an undefined and exposed public domain experience. The public realm is poorly defined and car dominated. Orientation is confusing and safe passage for pedestrians not clear.

Street tree planting is often non-existent, sparse or ad hoc in nature – with some residential streets having impressive avenue planting, whilst others and large car parking areas with almost no trees, are hot and exposed environments.

Streetscape	Infrastructure Type	Audit of Existing Infrastructure (15 August 2016)	Upgrade Opportunity
Vincent Street - South	Paving	Generally standard concrete footpaths on both sides, special 'Main Street' paving at Abardere Road intersection.	Extend full width footpaths south to Edgeworth Street intersection.
	Lighting	Standard timber light poles along north side (3 total); lapwing steel 'house-neck' light pole (6 total).	Investigate use of LED luminaires for efficiency, long life, low maintenance, integrate feature/leaflet banners (no light pole design).
	Signage	Signage located at Shape Street/Abardere Road.	Plan to review signage timing if Bypass proceeds.
	Zone Crossing	Zone crossing for Vincent Street at Shape Street intersection.	Review signage and signage placement.
	Blue Sticks	Located close to Abardere Road intersection (2 total).	Provide improved details at bus stops where there are no awnings to encourage use of public transport.
	Street Trees	None.	Plant trees on both sides to create avenue. Provide perching for south gateway. (Assess/Frost Intersection)
			Provide standard concrete footpath.
Charlton Street - South	Paving	No footpath.	Review lighting levels with utility.
	Lighting	Standard timber light poles (2 total).	Plant trees on both sides.
	Street Trees	None.	Provide standard concrete footpath.
Cessnock Street	Paving	Short length of standard concrete paving at the north east of intersection.	Review lighting levels with utility.
	Lighting	Standard timber light poles (4 total); lapwing steel 'house-neck' light pole (2 total).	Plant trees on both sides.
	Street Trees	None.	Provide standard concrete footpath.
Railway Street	Paving	No footpath.	Review lighting levels with utility.
	Lighting	Standards timber light poles (1 total).	Plant trees on both sides to consolidate avenue.
	Street Trees	Street trees on the north side, no trees on the south side.	Provide standard concrete footpath.
Edgeworth Street	Paving	Short length of standard concrete paving at intersection.	Review lighting levels with utility.
	Lighting	Standard timber light poles (2 total).	Plant trees on both sides.
	Street Trees	Very low street trees.	Provide standard concrete footpath on both sides.
Gordon Avenue	Paving	Generally standard concrete footpaths along north side and short section of south side.	Review lighting levels with utility.
	Lighting	Standard timber light poles (2 total).	Plant additional trees on both sides.
	Street Trees	Good continuous tree planting in street median island. Groups of trees on the south side, limited trees on the north side.	

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6.0 Public Domain Infrastructure Audit



Figure 6.0: Public Domain Infrastructure Audit Plan - South



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7.0 Community Engagement Outcomes

7.1 Introduction

A high level of community engagement was undertaken in the initial phase of the project involving:

- On line surveys;
- Intercept surveys;
- Interactive online mapping;
- Interactive large format mapping in the library;
- Two community workshops; and
- Staff workshops.

7.2 Key Outcomes

Feedback to date includes:

- Facilities/places for young people
- Don't see need for town centre housing – 50%
- Increased and formalised parking
- Increased tourism opportunities & accommodation – 66%
- Like the new precinct (yoghurt land/snap n' treat)
- More clothes/fashion retail and retail in general – 50%
- Improved amenities (toilets, bins, bubblers)
- Improved pedestrian accessibility between shopping centres



Figure 53: Workshop - Issue Mapping



Figure 54: Staff Workshop



Figure 55: Interactive Large Format Mapping

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Figure 65: Vincent Street, Cessnock

31 March 2017 | Cessnock Commercial Precinct Project

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8.0 Vision and Objectives

8.1 Theme

Three themes have been investigated as means to realise the project vision:

Market / Town Square

A meeting place to bring locals and visitors together, celebrating local wines and organic agricultural production, enhancing local cultural experience and identity.

Al-fresco Dining

The street as an outdoor dining place to foster activity and a healthy, active lifestyle. Rejuvenate existing heritage buildings by adding a street dining experience. Design pedestrian friendly laneways with active frontages and art elements improving connectors and creating places linking destinations.

Urban Park

Celebrating nature and the regions mining heritage. A place to re-charge, to enjoy the sun, the natural environment, attend engaging events and activities.

The Cessnock Commercial Precinct is 'The Gateway to the Hunter Valley Wine Country' that celebrates the cultural landscape of the region and provides a quality environment where the community is actively encouraged to participate in community and civic life.



Figure 607: Town Market



Figure 608: Town Market



Figure 609: Active Frontage



Figure 607: Active Frontage



Figure 611: Laneway



Figure 602: Urban Park



Figure 605: Open Canal Landscape

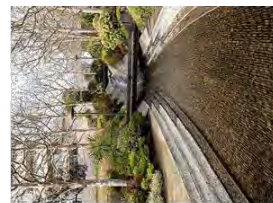


Figure 604: Open Canal Landscape

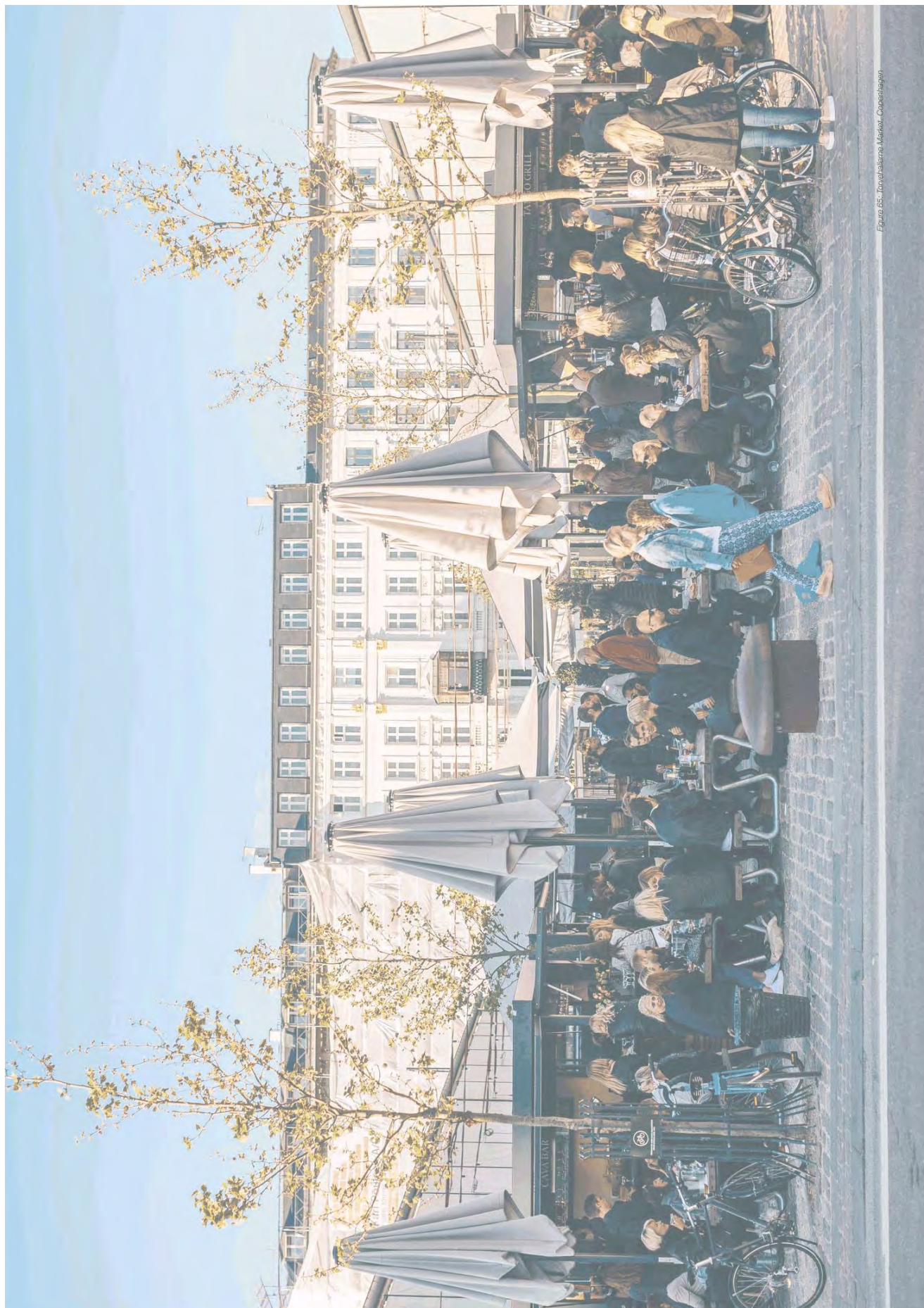


Figure 65: Townhallena Market, Copenhagen

9.0 Indicative Layout Plan

9.1 Introduction

The Indicative Layout Plan is informed by the CBD Masterplan and the Supply and Demand Analysis undertaken by the AEC Group, and is coordinated with the Urban Form Analysis that was developed in parallel.

The Plan illustrates a 2041 vision and 'master plan' for the future land use of the Commercial Precinct. It has informed the preparation of the Development Control Plan and established the city structure, which sets the framework for the Public Domain Plan.

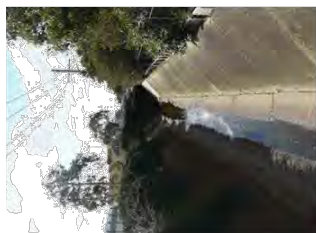


Figure 67: Existing Open Canal



Figure 68: Existing Blank Wall Street Frontages

9.2 Urban Design Issues

There are a number of challenging urban planning issues, mapped in Figure 66, that will need to be addressed, in order to realise the CBD Masterplan 'vision'; for an 'Attractive, Thriving and Welcoming' Cessnock Commercial Precinct.

The study identified the following key issues:

- Significant on-grade car parking areas, particularly in the west of the centre, undermine the provision of good urban amenity, and there is a lack of lively and attractive streets that feel safe to use.
- Significant lengths of blank walls and exposed 'back-of-house' functions dominate the experience of the west of the Precinct.
- Large 'super-blocks' in the west of the Precinct offer poor 'through-connectivity' and 'form barriers' in the urban fabric.
- The emerging business park in the south of the precinct lacks an identity, is difficult to access and is poorly integrated with the streets and life of the CBD.
- A network of large scale drainage canals form barriers, are often unsightly and undermine the forming of regular city streets.
- There is a lack of useable open space within a convenient walk of the CBD.
- There is no one public place within the CBD that could be called the 'heart' of the CBD.
- The 'Main Street' retail experience is disconnected from the shopping centre retail experience.



Figure 66: Existing 'TAFE' Park

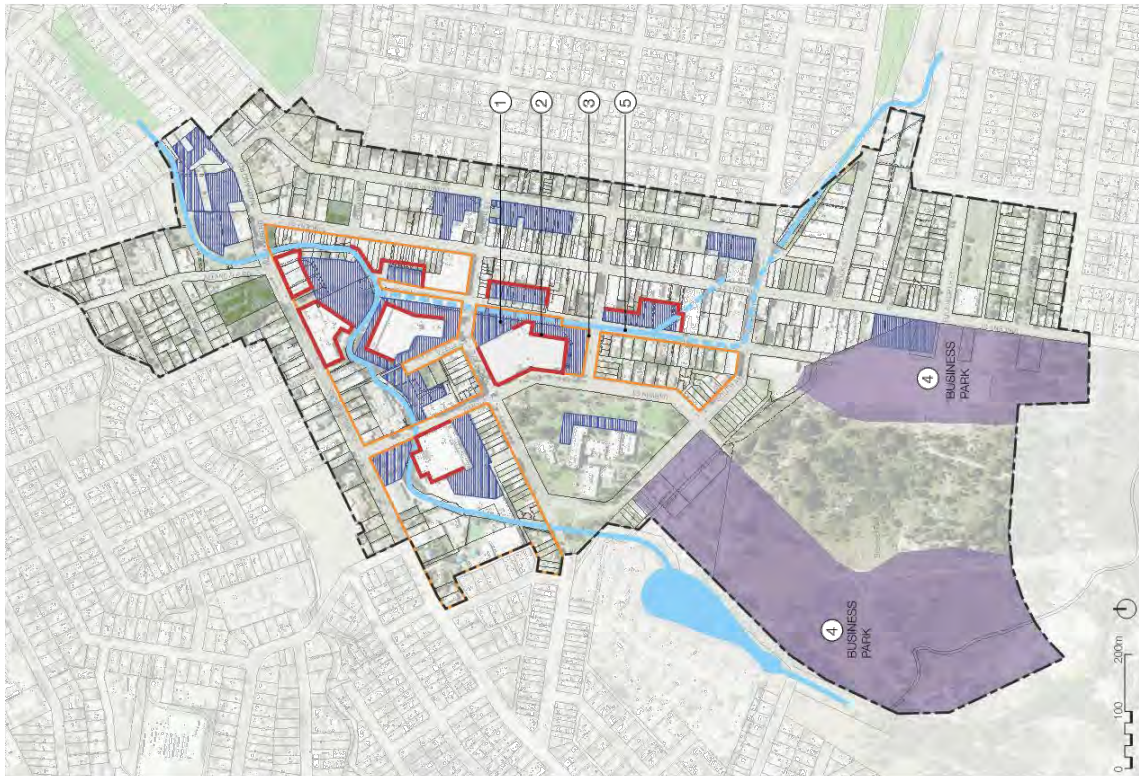


Figure 66: Urban Design Issues

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9.0 Indicative Layout Plan

9.3 Opportunities

The urban design study identified a series of inter-related urban planning opportunities which are illustrated in Figure 70. Some strategies have subsequently been modified, following more detailed analysis, development in the urban design strategy for the centre, and stakeholder consultation, however most of the fundamental urban planning strategies have been taken forward to inform subsequent stages of the project.

Urban planning opportunities identified for the Commercial Precinct, include:

- Extend the 'Main Street' urban character, west, to link up with the shopping centre developments and TAFE site.
- Create a central, urban, public place in the CBD, suitable for markets and as a social focus, a place for people to gather, a meeting place and destination for visitors.
- Complete the CBD urban street and block structure and improve overall amenity for people using the centre, improving the street level experience and developing new parks and plaza spaces.
- Provide new pedestrian links and 'green' corridors along waterways.
- Tailor land uses and urban form to address the proposed south CBD bypass.
- Reinforce the Precinct identity through featuring the three main CBD gateways.
- Improve signage and wayfinding at key locations to guide visitors to explore more of Cessnock.
- Provide a new draw-card visitor centre which is a landmark destination which attracts visitors from Sydney and regional NSW.



Figure 71: Open Canal Green Link



Figure 72: Food Market



Figure 73: Urban Park

- Retail Core
- Potential Urban City Blocks
- Existing Open Space
- Potential New Plaza
- Potential Market
- Potential New Green Space
- Gateway/City Landmarks
- Existing Main Street
- Potential New Retail Street
- Key Pedestrian Links
- Green Connections
- Waterway
- DCP Area

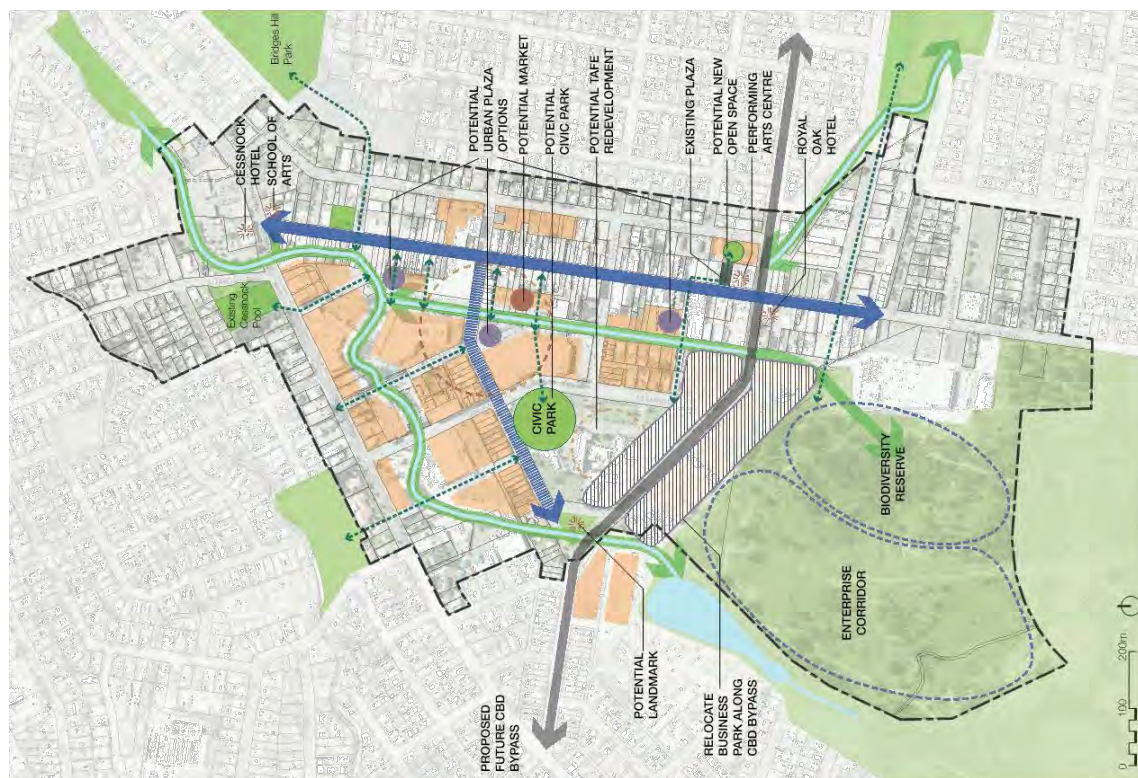


Figure 70: Urban Design Opportunity

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9.0 Indicative Layout Plan

9.4 Thematic Precincts

Cessnock Commercial Precinct will comprise a series of inter-related themed sub-precincts, each with their own distinct function and character. Collectively there will be synergies as sub-precincts overlap and complement each other.

The thematic precincts include:

- Mixed use zone
- Supermarket retail + club precinct
- Main road frontage businesses
- School precinct
- Medium density residential neighbourhood
- Light industrial/distribution
- Big box retail and business park
- Environmental preservation landscape

Within the Precinct there are four distinct activity 'hubs' that provide a focus for:

- Cultural / arts activities
- Civic functions
- Entertainment
- Potential 'Tourist' / cultural destination

Urban structure is provided by the 'Main Street' spine of Vincent Street, and by the three 'gateways' to the centre, marking the arrival and departure experience at the west, north and south periphery. The potential bypass route in the south of the Precinct will also potentially drive some land uses in the centre.

An outdoor dining strip is identified along Vincent Street, extending north from Hall Street, and then linking west along Cooper Street, culminating in the proposed Town Square that will become a new focus for socialising and for a program of activities. This sub-precinct is envisaged as pedestrian friendly, lively and with an urban form that is 'fine grained' and highly permeable in terms of pedestrian access.

A synergy is envisaged through integrating the Vincent Street, 'Main Street' retail experience with the two major shopping centres, and also through enrolling the entertainment and civic hubs situated along this important cross axis.



Figure 75: Shopping Centre Plaza in Ryde



Figure 76: Outdoor Dining

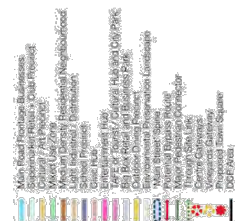


Figure 74: Thematic Precinct Plan

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9.0 Indicative Layout Plan

9.5 Indicative Layout Plan

The Plan provides an overall strategic level framework for the future urban planning of Cessnock Commercial Precinct. A holistic and integrated urban planning approach has been taken, drawing upon Council's expertise and detailed knowledge of Cessnock, a comprehensive process of stakeholder consultation and on the Consultant Team's analysis, and urban and landscape planning and design expertise.

The primary urban design 'moves' are illustrated, including the proposed land uses, location for public and community facilities (some with options), the proposed street hierarchy, major green spaces and pedestrian links, CBD gateways, overall drainage and canal infrastructure, public car parking sites and the major people places in the public domain are established.

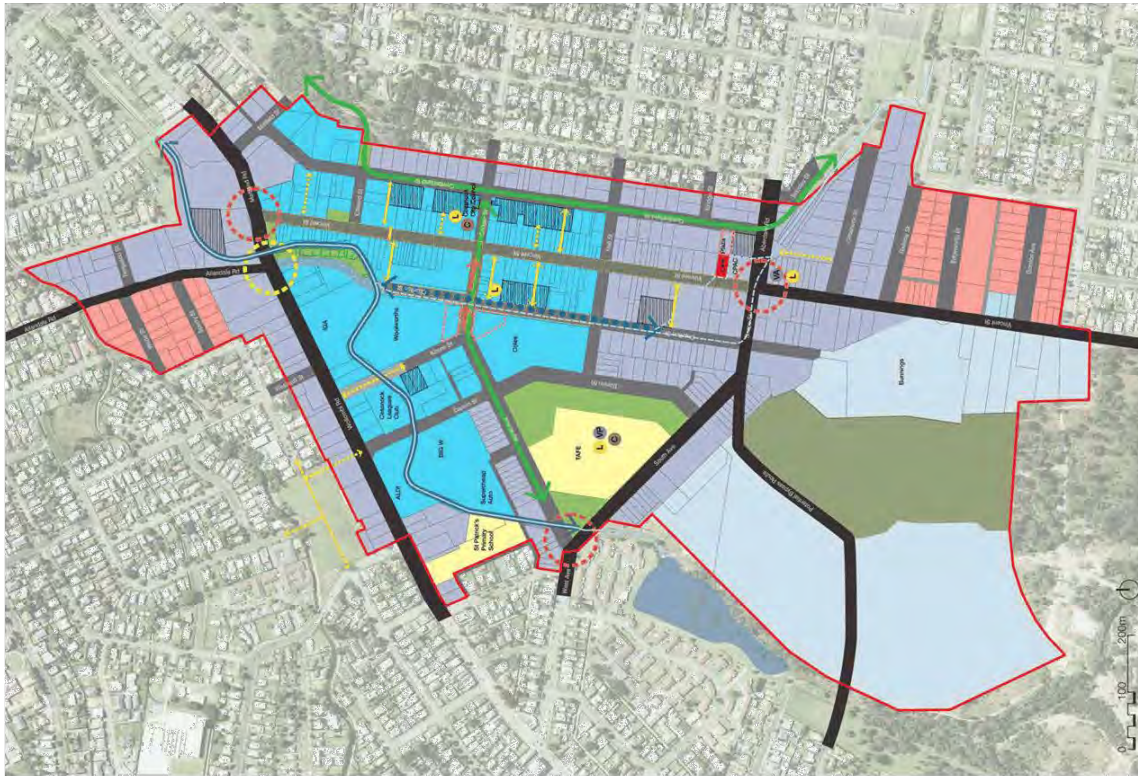
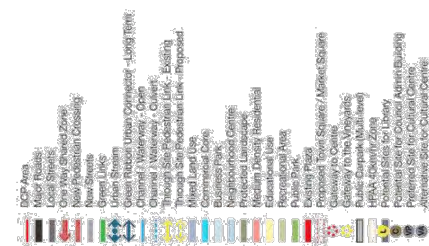


Figure 77: Indicative Layout Plan

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9.0 Indicative Layout Plan

9.6 Fostering Street Life + Activity

Well planned and designed urban centres have streets and public places that have good urban scale, are easy to access and move around, they feel safe, and most importantly are lively and interesting social destinations.

An important contributor to street life, are the activated frontages of businesses that line the CBD 'Main Street'. Requiring new businesses along the key streets of Cessnock to build up to the street frontage and to provide active uses at their ground floor level, will support a lively streetscape experience. This in turn will create an attractive destination for locals and visitors to the centre and support local business owners. The key principles include:

- Promote lively, active streets in the Commercial Centre;
- Ensure that development contributes to the quality, activity, safety and amenity of streets and the public domain;
- Provide for a comfortable pedestrian environment through the provision of awnings; and
- Contribute to the vitality of streets by maximising entries and display windows to retail uses and minimising blank walls.



Figure 78: Active Street Frontage in Paris



Figure 80: Active Street Frontage in Inner West Sydney

- Active Frontage - Required
 - Active Frontage - Preferred
 - Active Frontage - Encouraged
 - Square / Plaza
 - Existing Canal
 - DCP Area
- Notes: Awnings are required for all active frontages

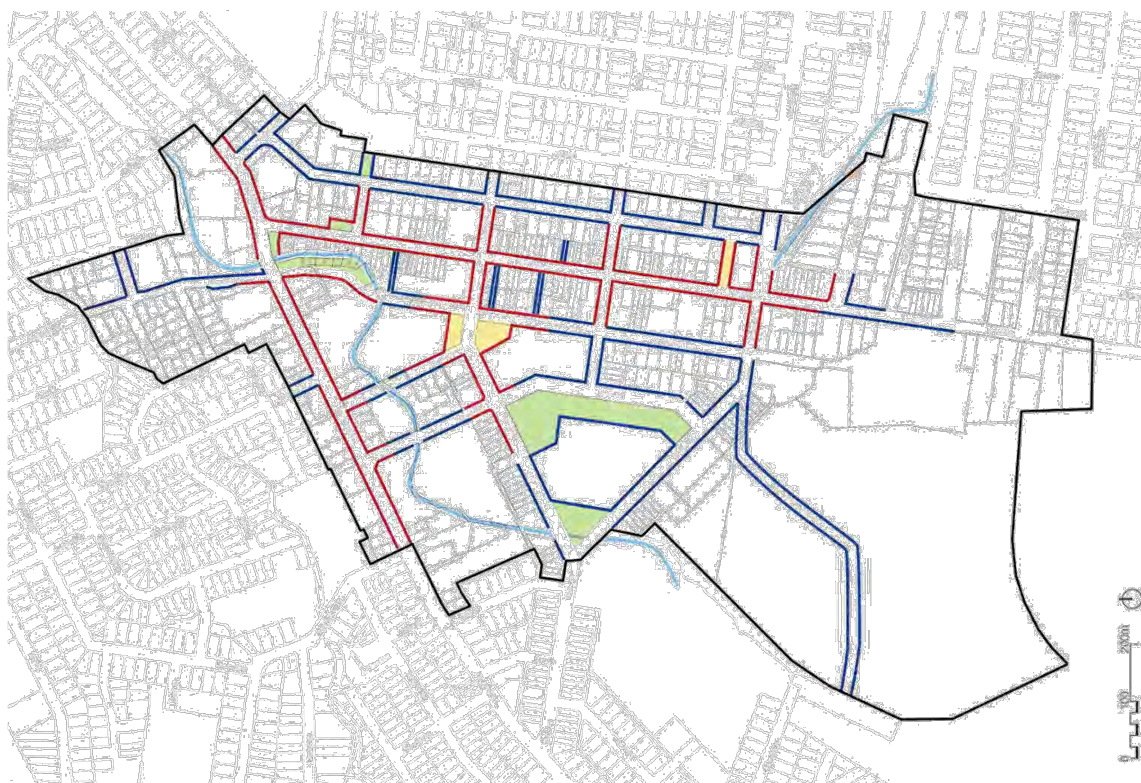


Figure 79: Active Frontage Plan

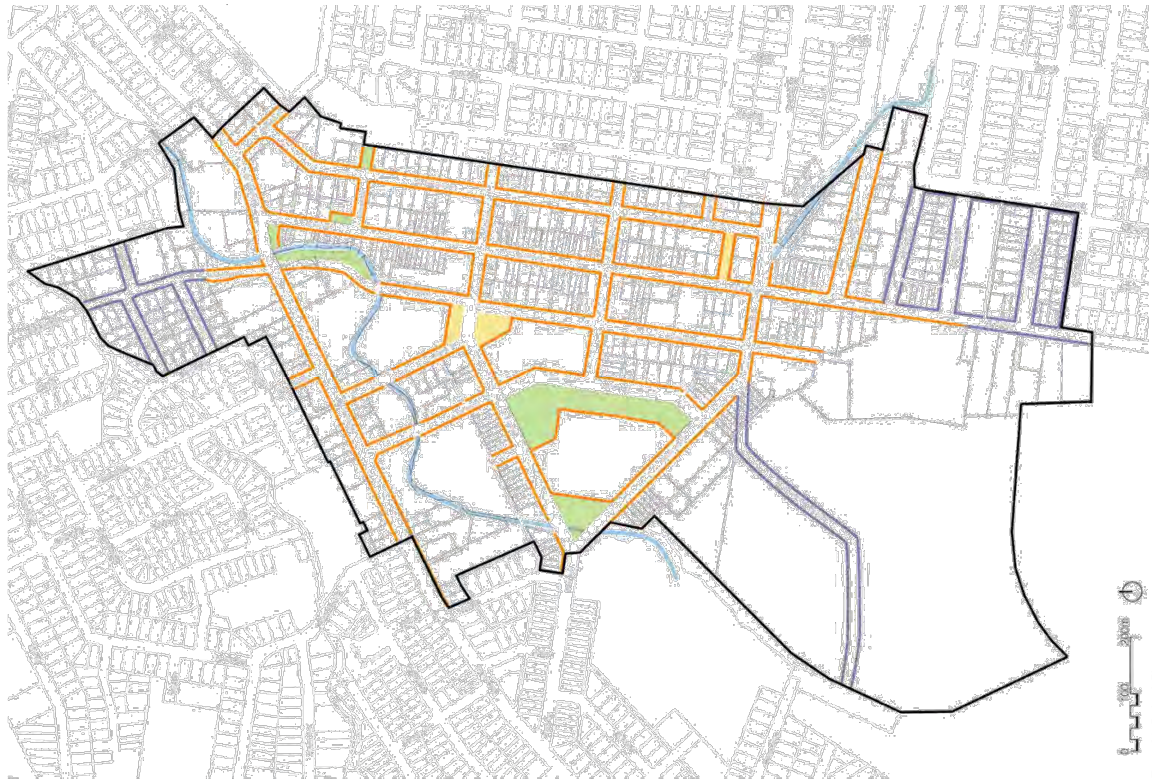


Figure 81: Setback Plan

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9.0 Indicative Layout Plan

9.7 Ensuring Good Human Scale

Good urban scale is a major factor in determining how we experience urban centres. A commercial precinct, such as Cessnock, has good urban scale when the streets, plazas and parks which comprise the public domain, are in a comfortable proportioned with regard to the built form. This can be tested by analysing the streets, parks and plazas in cross section.

Heritage buildings often set a precedent in this regard, establishing classic architectural details such as parapets, facade modulation, detailing and street awnings that both define the streetscape whilst also establishing good human scale.

A contemporary architectural approach is encouraged. New buildings should complement the existing heritage buildings in the 'Main Street' in scale by building to the street frontage and by consolidating the existing street wall height - setting back above this height, to respect the parapets of the traditional street frontage.

The key setback requirements are illustrated in the adjoining figures. In summary:

- To effectively define the streets and blocks of the CBD, generally a zero setback is required for the majority of the Commercial Precinct; and
- A 5.0m minimum front setback is required for the north and south residential areas, to allow for front gardens that enhance a low scale residential street; and
- In the Business Park in the southwest of the Precinct.



Figure 82: Mixed Use Development Integrating a Heritage Building Facade



Figure 83: Mixed Use Development Integrating a Existing Heritage Building Facade

- 0m Setback (at street wall)
- 5.0m Setback
- Square / Plaza
- Existing Canals
- DCP Area

CM⁺

9.0 Indicative Layout Plan

9.8 Height of Building

An increase in building height is proposed for Cessnock Commercial Precinct. Building heights in the CBD will:

- Reinforce the future urban form and character of the Precinct;
- Maintain good scale – in the street cross section;
- Respect the scale of existing heritage buildings;
- Avoid undue overshadowing of city streets and parks;
- Focus retail and street activity in the north of the Centre; and
- Build-in flexibility and reflect the projected future demand for floor space.

The recommended building height is illustrated in the adjoining Height of Building Map and can be summarised as:

- 21m (6 storeys) for the city block defined by Charlton Street, Cooper Street, Cumberland Street and Edward Street;
- 18m (5 storeys) for the former Cessnock Baths 'gateway' site;
- 15m (4 storeys) for the 'Commercial Core'; and
- 12m (3 storeys) for the remainder of the Precinct - for education, Business Park and residential areas.



Figure 85: Precinct - Mixed Use Development Along Main Street in Murrumbidgee



Figure 86: Precinct - Mixed Use Development Along Main Street in Murrumbidgee

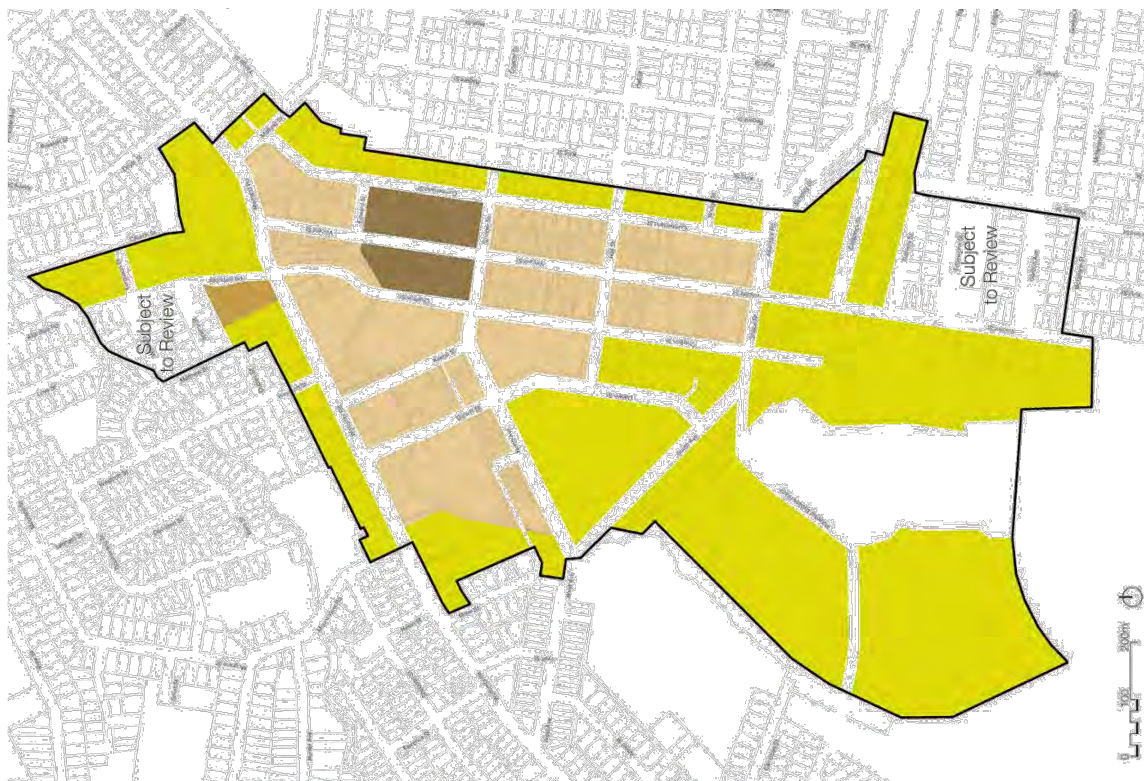


Figure 84: Height of Building Map

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9.0 Indicative Layout Plan

9.9 Active Transport Strategy

The Active Transport Strategy (ATS) prioritises pedestrian and cyclist access within the CBD and establishes a coordinated plan for the staged improvement and construction of new facilities as the CBD develops, and funding is available. The ATS is consistent with the Cessnock Bicycle Strategy (2016) and identifies specific opportunities for bicycle parking and for a repair 'hub' at the centre of the CBD.

The key principles of the ATS are to:

- Prioritise pedestrian and bicycle access, connectivity and safety;
- Ensure that any new public pedestrian and cycling links are located on nominated sites;
- Promote cycling through the mandatory provision of bike storage for nominated land uses, and
- Promote cycling through the strategic placement of bike storage facilities in the public domain.



Figure 98: On Road Cyclist



Figure 99: Off Road Cyclist

- Cycleways**
- Existing Regional On Road
 - Existing Regional Off Road
 - Existing District On Road
 - Existing District Off Road
 - Existing Local On Road
 - Existing Local Off Road
 - Proposed Regional On Road
 - Proposed Regional Off Road
 - Proposed District On Road
 - Proposed District Off Road
 - Proposed Local On Road
 - Proposed Local Off Road
 - Bicycle Parking
- Pedestrian Links**
- Major On Road
 - Through Site Link
 - DGP Area

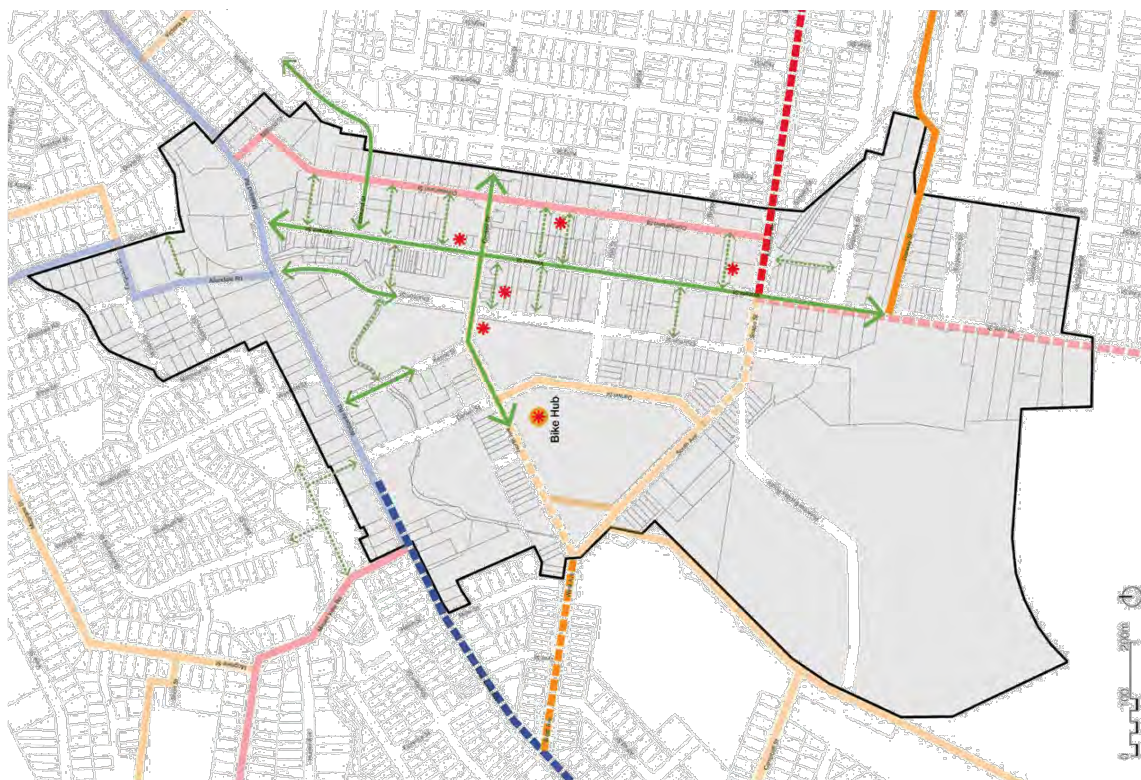


Figure 97: Active Transport Strategy

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9.0 Indicative Layout Plan

The Plan encourages the use of public transport (bus services) in the Commercial Precinct. A permeable urban form, including regular and safe through-site links, will enhance connectivity through the Commercial Precinct, so that residents, workers and visitors have access to safe and convenient public transport.

The following strategies are proposed to encourage public transport use:

- Encourage public transport (bus) use through lobbying State Government for improved frequency of services;
- Provide a high level of amenity by providing quality bus shelters that provide good weather protection (where no awnings);
- Provide seating that suits all age groups and levels of accessibility;
- Provide adequate lighting to enhance personal safety;
- Provide comprehensive transport information signage;
- Incorporate easy to read maps of Cessnock which illustrate key destinations; and
- Incorporate provisions for the sight impaired.

Roads and Maritime Services (RMS) will not approve a Shared Zone if on a bus route. To realise the proposed Shared Zone along Cooper Street and Charlton Street, re-routing of current bus services are proposed, as illustrated in Figure 90.

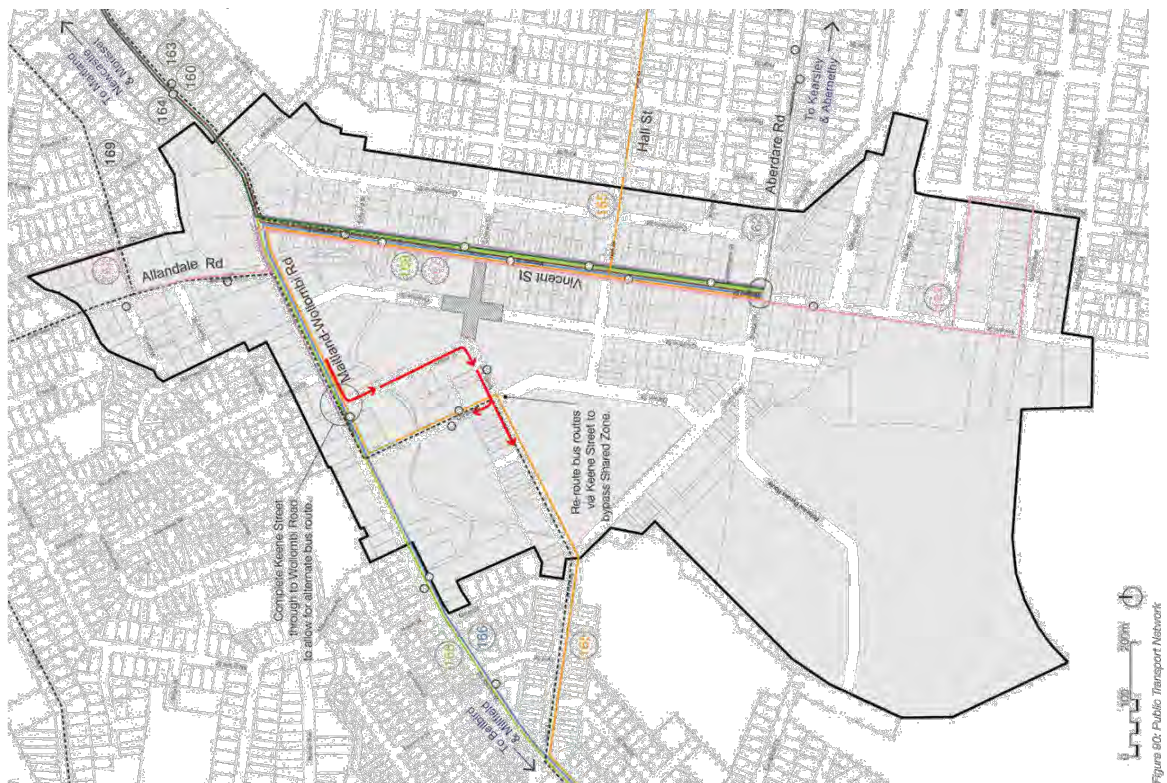


Figure 90: Public Transport Network

9.0 Indicative Layout Plan

9.12 Opportunity Sites

The study has identified a number of key sites within the Commercial Precinct which due to their location, size and function, have significant potential to positively shape the future of Cessnock. The adjoining table and map, identifies the location, site area and significance of each site within the Precinct. An assessment was then made as to the urban development opportunity that each site presents, and the Development Objective sought.

The strategies employed include:

- Provide development incentives to enable design excellence and sustainable design for new development;
- Identify redevelopment opportunities and development objectives for Catalyst Sites;
- Reduce the isolated nature of the major shopping centre areas; improve the appearance and reduce the environmental impacts of public car parking;
- Identify options for the redevelopment of the TAFE site as a regional cultural/educational centre and district-level park; and identify the long term renewal plan for the Commercial Precinct's major stormwater canal as an attractive landscaped linear green space and access way.

9.13. Catalyst Sites

Thirteen Catalyst Sites in Cessnock Commercial Precinct were identified:

1. South Avenue Properties
2. TAFE
3. Coles Supermarket
4. Cessnock Plaza (Woolworths)
5. Cessnock Baths Site
6. Former Cinema + Carpark
7. Vincent Street Carpark Site
8. Cessnock City Council
9. Public Carpark
10. Mixed Use Site
11. Public Carpark
12. Vincent Street Site
13. Hall Street Site



Figure 62: Site 3 - Coles Supermarket Site



Figure 63: Site 4 - Cessnock Plaza Site

DCP Area
Development Sites
Open Space Opportunities



Figure 91: Catalyst Sites Plan

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9.0 Indicative Layout Plan

Ref.	Catalyst Development Site	Ownership	Area (Ha)	Significance	Development Objectives
1	South Avenue Properties	Private	1.31	Properties along South Avenue have important frontages (visually prominent) to proposed future Bypass route.	<ul style="list-style-type: none"> To encourage future development promotes commercial, retail and business activity.
2	TAFE	Department of Technical - Further Education	3.16	A major site in the heart of the Centre - if educational use is to continue, a facility of this scale brings employment, retail demand and a vibrant student demographic to the Centre.	<ul style="list-style-type: none"> To encourage the use of the TAFE site as an education hub, potentially in conjunction with a compatible civic use, such as Library and Multi-purpose Cultural Centre (museum spaces, wine and mine centre, etc.)
3	Coles Supermarket	Private + Council (Operational)	2.00	A large, and key site in the heart of the Centre and a major retail destination. A key site on the Cooper Street cross axis link to 'Main Street' and with an important heritage to TAFE Park.	<ul style="list-style-type: none"> To aid in the development of an integrated commercial centre. To promote the development of a town square for the neighbourhood, accommodating areas for gathering, seating and children's play.
4	Cessnock Plaza (Woolworths)	Private + Hunter Water Corporation	1.82	Also a large, and key site in the heart of the Centre and a major retail destination. A key site on the Cooper Street cross axis link to 'Main Street' and with important street frontages.	<ul style="list-style-type: none"> To aid in the development of an integrated commercial centre To promote the development of a town square for the neighbourhood, accommodating areas for gathering, seating and children's play.
5	Cessnock Baths Site	Council (Community)	0.69	The long term plan is for Cessnock Baths to be relocated to a new site outside the Centre, and, as a result, this prominent corner site, at the northern 'gateway' to Cessnock Commercial Centre, would become available for other uses.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design. To encourage future development that promotes a suitable mix of land uses.
6	Former Cinema + Carpark	Private	0.60	A prominent 'Main Street' corner property with significant frontage to Cooper Street - an important west-east connector street. The former cinema provided one of the few high-quality entertainment venues in the Commercial Centre.	<ul style="list-style-type: none"> To encourage the ongoing use of the site for entertainment purposes.
7	Vincent Street Car Park Site	Council (Operational) + Private	0.39	Consolidated, current carpark site in the heart of the CBD, with both Vincent Street and Cumberland Street frontages. Elevated with views west over the valley.	<ul style="list-style-type: none"> Future development of the site is to achieve high quality architectural design that promotes retail, commercial and residential uses.
8	Cessnock City Council	Council (Operational)	0.52	Current Cessnock City Council Council Offices, public address and associated car parking. A key consolidated site within the CBD with Vincent Street, Cooper Street and Cumberland Street frontages.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design.
9	Public Carpark	Council (Operational) + Private	0.58	Consolidated Commercial Centre site, currently used for public car park. Significant CBD corner/street frontages.	<ul style="list-style-type: none"> Ensure future development promotes activity on the street and within the public domain.
10	Mixed Use and Public Carpark Site	Cessnock Leagues Club	0.54	Large consolidated site, including on site parking and green space.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design and public domain outcome.
11	Public Carpark	Council (Operational)	0.16	Consolidated Commercial Centre site currently used for public car park. Significant CBD corner/street frontages.	<ul style="list-style-type: none"> Future development is to encourage active street frontages.
12	Vincent Street Site	Council (Community)	0.41	Consolidated, prominent 'Main Street' corner site, currently three-quarters vacant.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design and public domain outcome.
13	Hall Street Site	Council (Operational)	0.07	Prominent Centre corner site, currently vacant.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design and public domain outcome. To provide for active street frontages to promote activity on the street and within the public domain.
14	Potential Library Site and Public Carpark	Council (Operational)	0.40	Consolidated Commercial Centre site currently used for public car park.	<ul style="list-style-type: none"> Future development is to encourage active street and laneway frontages. High quality architectural design and public domain outcome.

Table 4: Catalyst Sites



Figure 9.4: Open Space Opportunity Sites Plan

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9.0 Indicative Layout Plan

9.14 Open Space Opportunity Sites

The study also identified Open Space Opportunity Sites in the Commercial Precinct. The adjoining table and map, identifies the location, site area and significance of each, within the Precinct. An assessment was then made as to the open space/public realm opportunity that each site presents and the Development Objective sought.

Seven Open Space Opportunity Sites in Cessnock Commercial Precinct were identified:

- A. TAFE Park Site
- B. Stormwater Canal (north)
- C. Vincent Street 'North Gateway' Park
- D. HJ Sternback Lions Park
- E. Cooper Street and Shopping Centre Carparks
- F. CPAC Carpark (Plaza extension)
- G. Western Gateway Triangle



Figure 9.5: Site B - TAFE Site



Figure 9.6: Site G - Stormwater Canal (north)

DCP Area
Development Sites
Open Space Opportunities

CM⁺

9.0 Indicative Layout Plan

Ref.	Open Space Opportunity	Ownership	Area (Ha) / Length (m)	Significance	Development Objectives
A	TAPE Site	Department of Technical and Further Education (TAFE)	1.51	A large green space at the 'heart' of the CBD that is currently underutilised, poorly defined, and lacks usable spaces and places to enjoy on a day to day basis (e.g. currently consists of a war memorial, grassed, exposed earth areas, and a large concrete stormwater canal).	<ul style="list-style-type: none"> To provide a distinct recreational park for the community, accommodating areas for gathering, picnicking, children's play and relaxation.
B	Stormwater Canal (North)	(Hunter Water Corporation)	833m (length)	A major concrete stormwater canal, of significant width, depth and length, weaves through the urban fabric of five city blocks.	<ul style="list-style-type: none"> Upgrade and incorporate WSUD of the existing stormwater canal.
C	Vincent Street North Gateway Park	Council (Community)	0.07	Prominent corner site at the north gateway to the Commercial Centre.	<ul style="list-style-type: none"> Enhance the existing park signifying its gateway location.
D	HJ Sternback Lions park	Private	0.06	Publicly accessible, medium sized park located on a key corner of the 'Main Street'.	<ul style="list-style-type: none"> Upgrade the existing park and open to surrounding public domain.
E	Cooper Street and Shopping Centre Canals	Private	0.48	Underdeveloped space at the 'heart' of the CBD, with connections from key shopping centres to the 'Main Street', currently only used for carparking.	<ul style="list-style-type: none"> To promote the development of a town square for the neighbourhood, accommodating areas for gathering, strolling and children's play. Develop Cessnock Market Strategy.
F	CPAC Carpark	TBC	0.06	Important through-block public domain, pedestrian and visual connection from Vincent Street (existing plaza) through to Cumberland Street, adjoining CPAC.	<ul style="list-style-type: none"> Encourage the expansion of the existing CPAC plaza through to Cumberland Street.
G	Western Gateway Triangle	TBC	0.37	Prominent corner, triangular site at this west gateway to the Commercial Centre.	<ul style="list-style-type: none"> Deliver a gateway park which enhancing streetscape.

Table 5: Open Space Opportunity Sites



Figure 97: Town Square, Bussell Hill

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10.0 Public Domain Plan

*'A gateway to the Hunter Valley
Wine Country that celebrates
the cultural landscape of
the region and provides a
quality environment where the
community is actively encouraged
to participate in community and
civic life.'*

of the town's brand and provide an open space framework that enhances the Cessnock Commercial Precinct.

10.2 Landscape Objectives

The landscape vision seeks to create a model 21st century regional community. The overall landscape vision will establish the essential character of the Cessnock Commercial Precinct, exploiting the viticultural setting, and providing visual and environmental amenity.

A series of key open space and landscape planning objectives have been identified:

Develop A Distinct Identity

The public domain should celebrate and interpret the viticultural and mining heritage of the town and the surrounding region to distinguish itself as the 'gateway' to the Hunter Valley Wine Country. This theme is to be reinforced and extended through the public domain by using specific planting, materials and furniture palettes which reinforce the theme and history of Cessnock as the Hunter Valley's 'Wine Country'. Other opportunities for strengthening the theme include the integration of interpretative art elements throughout the public domain (installations / streetscape details + figures / sculpture), way-finding + signage, and informal / formal play spaces.

Develop A Green Network To Connect Character Zones

Existing open spaces and pedestrian links will be upgraded to strengthen accessibility and permeability between the components of the open space framework whilst enhancing their notion as 'green links + connectors'. The landscape will enforce way-finding throughout the precinct and provide ease of circulation and environmental amenity.

Creation Of A Legible Street Hierarchy

Develop a street network which integrates strong pedestrian and cycle links alongside the existing functional needs of public transport and vehicle movements. The proposed tree species and materials palette will create variety and character throughout the centre, supporting the overall 'Wine Country Gateway' and 'Mining Heritage' themes established.

Engage The Water Landscape

Allow connections to, and experience of, the canal system through the upgrade of existing channelled zones and above-ground

10.1 Vision

This Public Domain Plan (PDP) was prepared by Context Landscape Design in consultation with the Cessnock Council and the community.

The 'Gateway to the Hunter' vision builds upon the notion of the Lower Hunter Region as a notable productive landscape - a 'Valley Wine Country' serving its residents and surrounding cities and towns.

A study into the Lower Hunter region's Agricultural Lands, commissioned by the Australian Government's Sustainable Regional Development organisation (SRD), revealed that the region "has a range of natural resources and climatic conditions which support a wide variety of agricultural enterprises." "Not only does the region boast a complex rural economy driven largely by livestock grazing, poultry farming, and protected, broad acre and cultivated cropping but the region has established a sound international reputation for wine making and viticulture - a reputation which has propelled a thriving tourism industry alongside it."

The City of Cessnock is a key location within the region as the first point of contact for visitors, the majority of whom travel to the region from the south. It is positioned to harness the influx of visitors making their way to the wineries further north, building upon its current identity as a regional centre. Cessnock has the opportunity to capitalise on the agricultural and viticulture identity of the region by incorporating the idea of the 'productive landscape' into its branding and open-space strategy.

Key existing open-space sites have been identified as potential opportunities for Cessnock to incorporate the landscape as part

interpretation of the covered channel zones via the implementation of swales and Water Sensitive Urban Design Principles (WSUD). Facilitate active engagement with the water by reverting the channels back to their original 'open stream' condition (through emphyseal in nature) through removal of the concrete confines and re-design of natural embankments with native planting. Improve links between the town centre and the water, visually and physically, especially through the upgrade / replacement of new pedestrian bridges. As a long-term vision, a flood study would need to be undertaken in the first instance to model the changes in flood water levels. Additionally a dam may need to be constructed to achieve a constant stream of water.

Create A Usable Landscape

Create a landscape setting that can accommodate a range of community and cultural activities for both day and night occasions. Public spaces that offer possibilities and activities for pausing and relaxing - inviting both residents and visitors to linger in the precinct for longer periods of time. Improving conditions and entertainment for children, such as playgrounds and adventure parks, will also entice visitors (especially families) to spend more time in the town centre. Currently under-utilized spaces in laneways and gaps in the built fabric have great potential to serve the town and its people with new 'green lungs' such as pocket parks and green facades / walls.

Ensure Safety and Security

Ensure that residential developments have defined private open spaces that overlook the public domain to create a sense of activity, safety and security. Ensure more active and transparent street frontages along key retail strips. Encourage terraces to extend operating hours into the evening in order to maintain an active public environment beyond dusk. The implementation of a lighting strategy for the whole Commercial Precinct will facilitate and encourage evening activity.

Integrate the Landscape and Public Domain

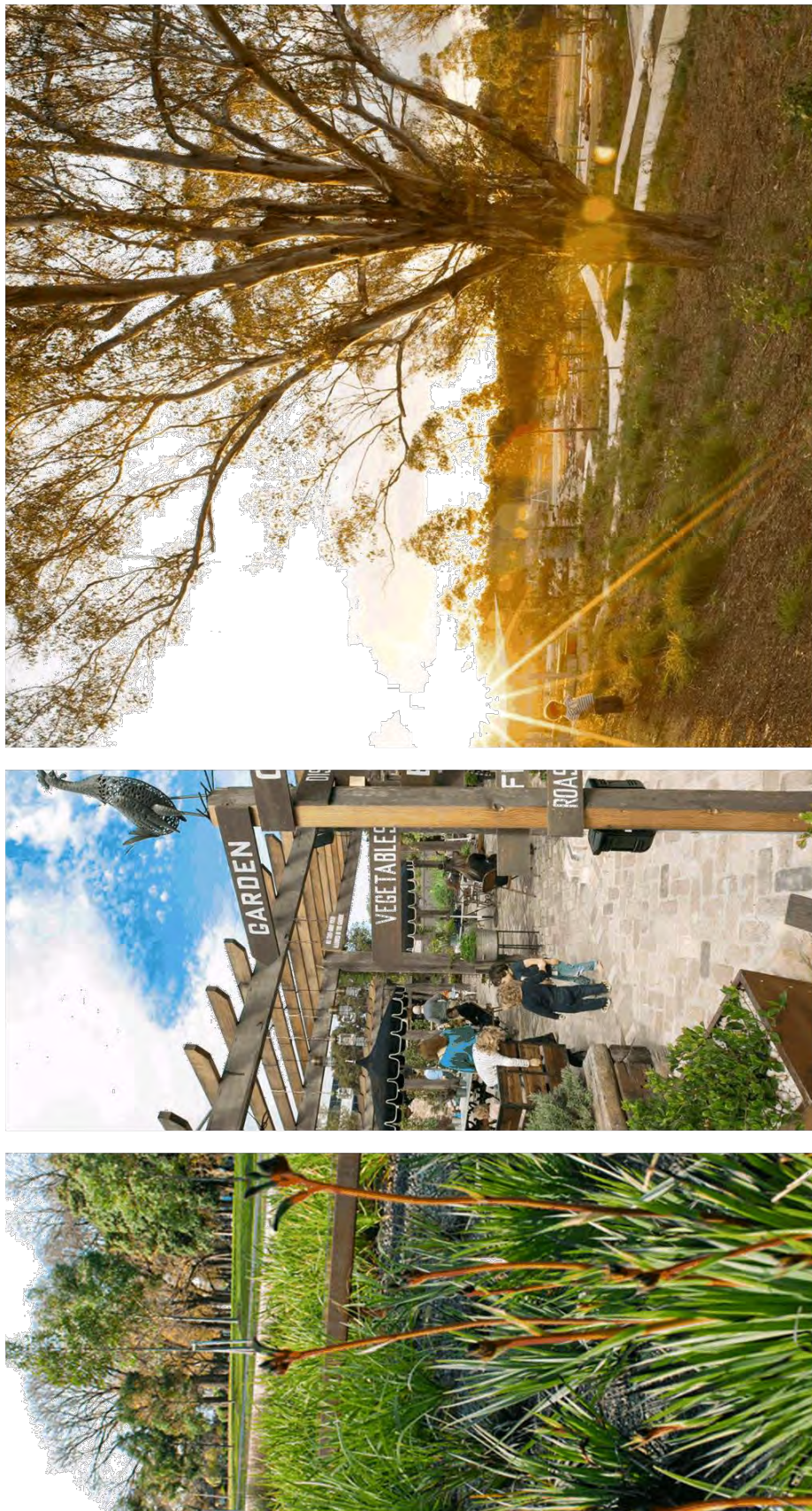
The visual continuity and thematic vision - 'Mines and Wines' - for the Commercial Precinct can be enhanced by establishing a unified suite of public domain furnishings and materials, that includes lighting. Materials must be robust and enduring whilst reflecting the culture of the town.

¹ Hunter Councils (June 2013). *Mining Important Agricultural Lands in The Lower Hunter Region* of NSW p13. Available: www.environment.gov.au [August 2016]
p. 164



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'Open spaces are significant public assets that contribute to the development of engaging, liveable and sustainable centres and communities.'

10.3 Open Space

The Open Space Framework Strategy identifies the key existing open spaces within the study zone and suggests principle linkages and connections between these sites through the implementation of 'green corridors'.

The existing public open spaces of Cessnock are currently under utilised. Cessnock has the opportunity to tap into its existing recreational amenity through the provision of better linkages and accessibility.

Key parks and open spaces within the study area have been identified as opportunities to become local and regional attractions. The proposed upgrade and re-design of these existing open spaces will incorporate a range of activities and amenity to attract visitors and entice frequent use from residents.

These spaces reflect a hierarchy of uses based on their scale and location - from large open spaces for recreation to smaller, intimate spaces for gathering. Additionally, two locations along Vincent Street at the corners of Aberdare and Malliland Roads have been identified as key nodes where the notion of a 'Gateway' to the Main Street can be enhanced through numerous interventions such as public art installations / sculptural icons, alongside way-finding and interpretative signage.

The anchor parks and civic spaces consists of the following typologies:

1. Enterprise Corridor
2. Civic Park
3. Town Square
4. Gateways
5. Pocket Park

The 'Green Corridors' provide opportunity for a series of key connections between the open space network. The 'Green Corridors' consist of the following typologies:

6. Urban Waterway
7. Green Ribbon - Urban Connector (long term vision)
8. Urban Spine (Main Street)
9. Primary Green Street Connector
10. Green Linkages

Finally, specific sites within the Centre have been recognised as key 'Catalyst Sites' which will celebrate the notion of the 'Hunter Valley Wine Country' alongside the viticultural character of the region. These anchor sites embody the overall landscape vision:

- A. Cessnock City Market / Event Site
- B. TAFE and Cultural Centre (Preferred Location)
- C. Cessnock Cultural Centre - Alternative Locations



Figure 98: Open Spaces + Landscape Framework Plan

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10.4 Catalyst Sites

Cessnock City Market

The Proposed Town Square is identified as a site to accommodate local and regional events and is identified as the City's commercial 'heart' - between the main street and a series of supermarkets. A yearly and seasonal calendar of events ranging from weekly wine and produce markets to festivals and music performances would cater for a wide range of users and age groups and attract visitors from elsewhere in the region.

Events create gatherings of people which in turn spills out into the peripheral streets and surrounding public domain, bringing life and bustle to a much larger radius. The surrounding retail and food and beverage offerings will benefit largely from events which generate a higher than average population of residents and visitors.



Cessnock Cultural Centre

TAFE will expand their educational services on the current site, bringing new activity and energy to the Commercial Precinct. A vibrant TAFE hub is envisaged with complimentary civic uses - a multi-purpose Cultural Centre which would include a new Cessnock Library, a High Tech Museum, and 'Mine and Wine' theme Visitors Centre.

The Cultural Centre is envisaged as a welcoming facility which is an architectural attraction in itself and offers visitors a unique experience. A new expanded Cessnock Library facility would be incorporated in the Centre.

The high tech Museum and Visitors Centre would be devoted to the regional wine culture and Cessnock's mining heritage. It would provide visitors with an understanding of the history and future of the Hunter Valley as a centre of wine production and offer information into the mining history of Cessnock, providing a narrative into the township's growth and societal changes as a result of the developing coal mining industry.



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10.5 Open Space Typologies

Enterprise Corridor

The opportunity exists to rethink the open space area to the south of the Cessnock Commercial Precinct within the zoned industrial/business park area and remnant forest. The proposed 'Enterprise Corridor' accommodates an alternative bypass route that connects Aberdare Road with James Street and offers an alternative route around the Commercial Precinct for through traffic.

Adjacent to the potential bypass is zoned parkland, offering a number of active and passive recreational activities that will attract people from the region and local area alike, providing recreational activities for all abilities. Activities such as BMX / mountain bike trails, walking tracks and waterways all within an enhanced endemic forest system.

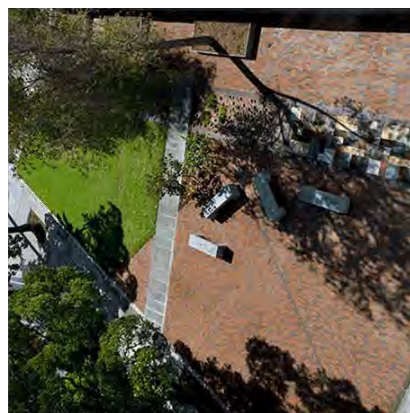
The proposed attractions will be integrated with appropriate facilities that complement the expected use such as toilets, picnic shelters, bins, lighting, wayfinding/signage and sensitively placed parking that will encourage use and provide the infrastructure for a range of potential future events.



Civic Park

Located in the central area of the commercial precinct the civic park will act as a 'green heart', central to the open space network. This park will realise the following objectives:

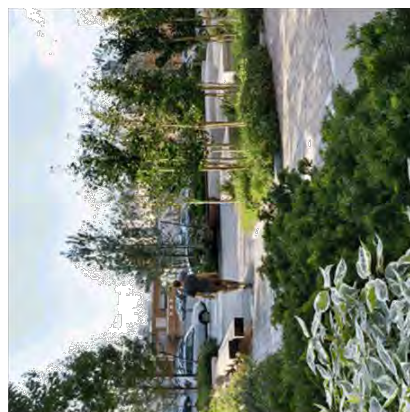
- Provide a distinct identity that enhances the 'Cessnock' brand.
- A safe place for residents and families.
- Enhance opportunities for community activities.
- Provide a cultural meeting place.
- Prioritise pedestrian connectivity.
- Promote public art opportunities.



Town Square

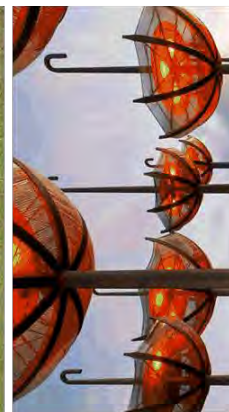
An open public space, within the heart of the town, with a distinct urban identity, used as a community meeting place, a place to sit and have lunch or merely as a place to stop, rest and enjoy the town surrounds. A mixture of hardscape and formally arranged mature trees will provide shade and create a space that is usable in all weather conditions. Public art or water features would add to the sense of a special community space. The Town Square also has the potential to be utilised as an event space for markets. The design for the Town Square will realise the following objectives:

- Activation - day / night and year round.
- Adaptable / multi-purpose space.
- Strong connection to surrounding streets.
- Maximise solar access.
- Safe and accessible.
- Robust materiality.
- Promote public art opportunities.



Gateways

Four locations have been nominated as key sites in which to promote the 'Gateway' experience for visitors. A feature statement in the form of a sculptural piece or art installation will signal the entrance to Cessnock and to the Hunter Valley Wine Country when travelling north. This icon will become identifiable with visitors and has the potential to be part of the Centre's branding.



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10.0 Public Domain Plan

Pocket Park

This is a site within the city's commercial 'heart' which has the potential to be transformed into an important green space. This small park is envisaged as a space that will improve the staying opportunities within the Commercial Precinct. The creation of interesting and fun spaces with pleasant micro-climates, will offer a sense of repose away from the more populated areas of the Centre. Located on the corner of Vincent and Edward Streets, the existing park is proposed to be upgraded to create a strong, unique character. Its upgrade would provide the following amenity:

- Deciduous trees for shade in summer and to allow sun exposure in winter;
- Diverse feature planting bringing colour and texture;
- Seating to suit both groups and individuals;
- Creation of 'smaller' rooms' for group gatherings; and
- Other furniture elements such as bins and water bubblers.

Urban Waterway

The existing creek/canal system along Charlton Street is a mixture of covered and open concrete channel. The opportunity exists to interpret the water course above the ground in a more urban treatment such as a water feature incorporating WSUD; for example as a planted swale that improves storm water quality. This interpretation will provide an attractive walkway along the west edge of Charlton Street and a possible termination/feature within the proposed Town Square.

Open areas of the channel can be rehabilitated and incorporate a hard urban edge such as water steps that encourage interaction with the water and a place to rest and play. Interpretation of this water story will be educational and provide a strong and important sustainable message to the community as well as raising an awareness of the natural environment that runs through and below the Centre's streets.

Green Ribbon - Urban Connector

As part of a long-term vision for the centre, the 'Green Ribbon' becomes a second connector which capitalises on the existing water canal system as an opportunity to create a unique pedestrian journey. The long-term vision for the existing canal is for the transformation of an extremely unpleasant concrete channel into a ribbon of massed riparian planting, which reinstates its historic condition of a thriving natural waterway. This will in-turn generate the following outcomes:

- A new recreational trail which transitions from a walkway alongside the water, to boardwalks hovering above the water.
- Pockets of grass on top of the embankment providing spaces for picnicking and social gatherings.
- Bench seating along the ribbon offering amenity for rest and relaxation.
- Creation of an ecologically valuable habitat.

This vision is a long-term one, and in the first instance would rely on a flood study to model the changes in water levels, an assessment of the viability of relocating utilities and a construction feasibility of now to introduce small dams to the canal to achieve a constant stream of water.



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10.0 Public Domain Plan

Urban Spine (Main Street)

The existing 'Main Street' forms the central spine of the Cessnock Commercial Precinct. It provides an important retail focus and a clear identity. The 'Main Street' is a place for people to meet, socialise and gather.

Key design moves which can assist with this include:

- Street tree planting to define the character and provide shade;
- Generous footpath widths;
- Active and open building facades to encourage and invite users;
- Appropriate lighting;
- Provide a human scale appropriate to existing density;
- Public artworks and cultural markers; and
- A street furniture palette that enhances Cessnock's character.

Primary Green Street Connector

Cooper Street / North Avenue is identified as a strong pedestrian link between the retail 'Main Street' and the series of supermarkets to the west. As a key east-west connector, this linkage should become one of high pedestrian and bicycle priority incorporating traffic calming measures, such as a Shared Zone approach, in combination with well-designed pedestrian crossings and integrated bicycle routes.

Its character will be one of a green corridor:

- Introduce a Shared Zone approach at the Town Square;
- Leafy, tree-lined streets framing the pedestrian zone;
- WSUD integrated principles and street verge planting;
- Moveable planter and pots along the Cooper Street retail / dining end; and
- Layers of lighting at a human scale to facilitate evening activities.

Green Linkages

Existing tertiary streets and laneways are identified as 'Green Linkages' - these forgotten (under-utilised) spaces will become pleasant walking routes, where people can step away from the main traffic corridors and enjoy a more intimate green walk at a human scale.

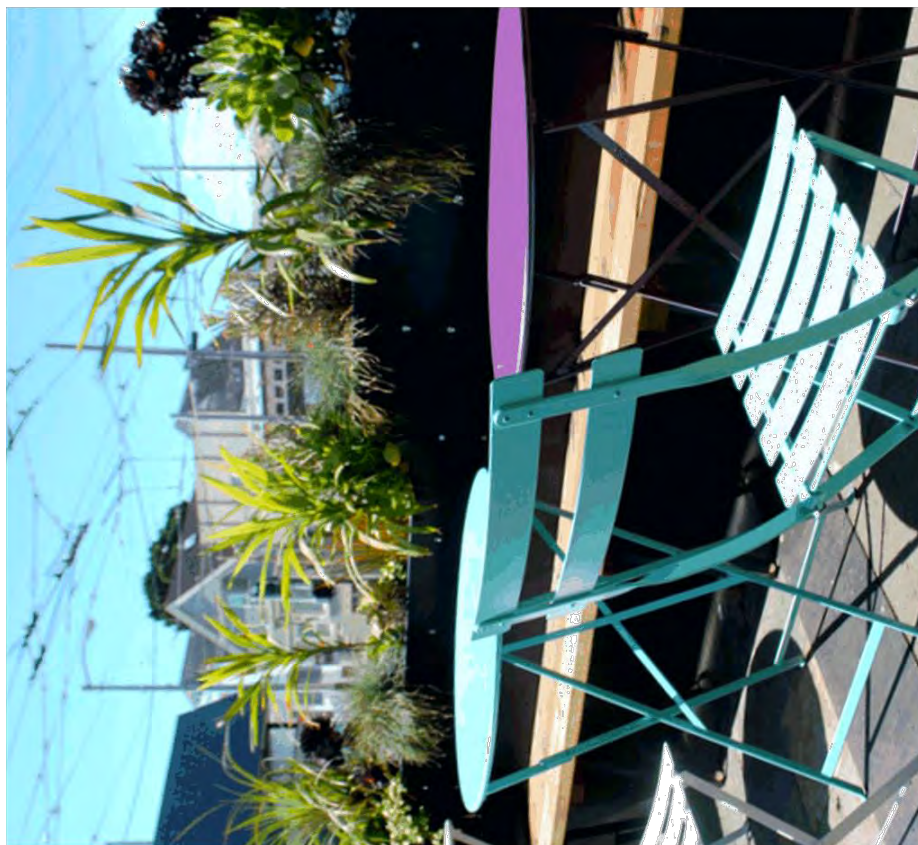
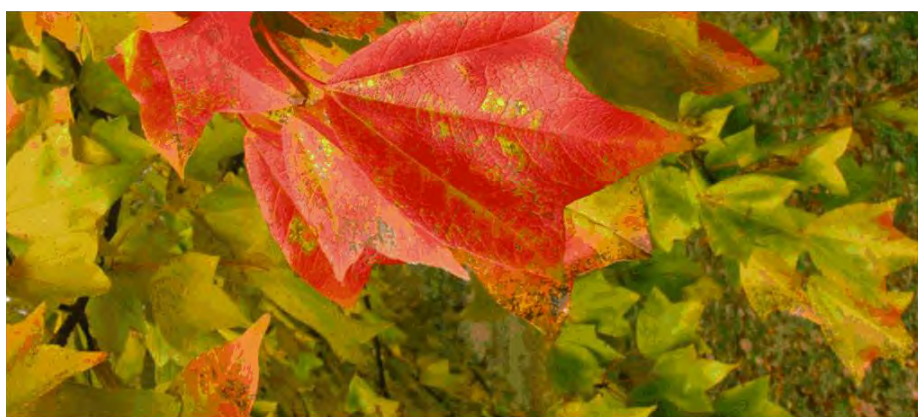
Key features include:

- Street tree planting in tertiary streets to define the character;
- Low-level planting in blisters to give the street a 'garden' feel; and
- Sustainability initiatives such as sloped kerbs, WSUD tree pit design and green walls (laneways).



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10.0 Public Domain Plan

*'Beyond their use for moving people and goods, streets comprise an extensive network of public open spaces that can facilitate social, civic, and economic interactions.'*¹

10.6 Streetscapes

Street Typologies

Not only corridors for movement, streets take on a crucial role as the scene for activities and social gatherings of the public realm. Through striving to achieve better balances between all street users (pedestrians, cyclists and vehicles) the quality of the street environment is already increased. Then, through various levels of upgrades and implementations aimed at both pedestrian experience and ecological value, we can create streetscape environments which are perceived as destinations in themselves. There are six street character types within the Centre, this reflects the use and function of streets within the Commercial Precinct. The character of each street is a result of the relationship between its built form / open space and the varying intensity of pedestrian and vehicular movement.

The study area street network consists of the following typologies:

1. City Bypass Road

This road typology consists of high traffic volumes and this is reflected in the wider street dimension. Medians or WSUD planting and large street trees help to mitigate the predominantly vehicular character.

2. Main Street

Vincent Street forms the 'Main Street' of Cessnock, coined the 'urban spine' of the centre. Its existing condition reflects that of a traditional retail shopping strip with wider walkway zones and on-street parking. Material upgrades and street planting will advance the pedestrian experience and amenity along this corridor.

3. Cross Street Connections

The streets are envisioned as a series of green linkages between the Centre's network of parks and open spaces. Due to the east-west orientation of these streets, they serve as key connectors, moving pedestrians across the Commercial Precinct.

Cooper Street is a key east-west connector, and, in conjunction with the proposed Town Square, is proposed in part as a Shared Zone (that would also extend part way along Charlton Street). A Shared Zone prioritises pedestrian access and has a strict speed limit of 10km/hr. These zones are paved from drop front to shop front, with vehicular passage restricted to one or two way routes, defined by bollards and other street furniture.

4. Secondary Streets

The Secondary Streets run parallel to the 'Main Street' (Vincent Street) to the east and the west. These streets are of a smaller, human scale and will provide high amenity for both pedestrians and cyclists.

5. Residential Streets

The Residential Streets occur on the periphery of the study area and are composed of largely single-dwelling plots. The existing streetscape character varies widely and the most crucial intervention is to introduce street tree planting along with roadside low-level planting and / or WSUD initiatives such as rain gardens.

6. Laneways

The Laneway typology varies in dimensions and predominantly exists between Vincent Street and Charlton Street to the west. They are pedestrian only in character and have the potential to foster a unique finer-grained experience that is currently lacking in the Commercial Precinct.

The following pages explore the landscape vision for each of the street typologies in more detail.

¹ New York City Department of Transport (2009), Street Design Manual, United States: Vanguard Direct, p20.

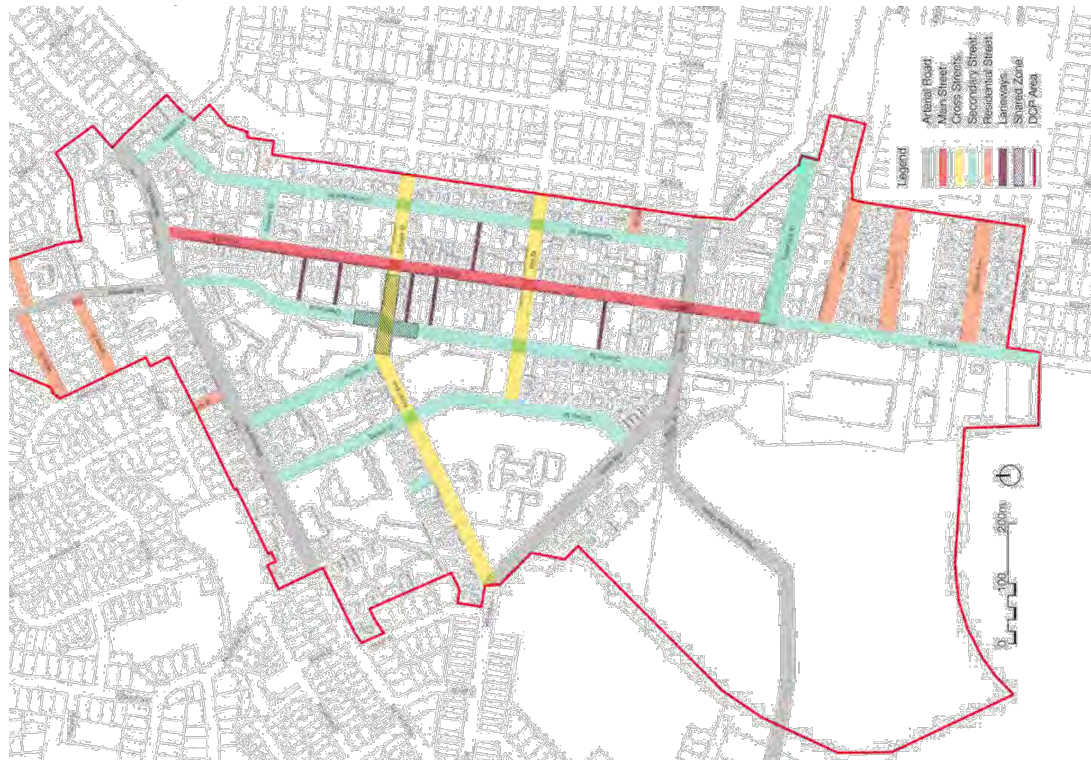


Figure 68: Street Typologies + Hierarchy

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10.0 Public Domain Plan

10.7 Street Type: City Bypass Road

Character + Design Principles

- Mitigate large scale road environment with large tree planting to form boulevards / avenues.
- Integrate WSUD measures, dedicated cycle path (both sides) and ensure pedestrian provision.
- Incorporate median swale wherever possible between major intersections.

Street Trees + Planting

- *Eucalyptus purpurea* to either side of street, spacing: 20m.
- *Waterhousea floribunda* to median swale strip, 20 m spacing of opposite arrangement.
- Native grasses and Macrophyte species for swales, e.g. *Juncus usitatus*, *Carex appressa*, *Lomandra longifolia*, *Isolopais nodosa*.
- Roadside planting: *Dietes grandiflora*, *Trachalasparrum jasminoides*, *Lomandra* spp., *Dianella* spp.

Materiality

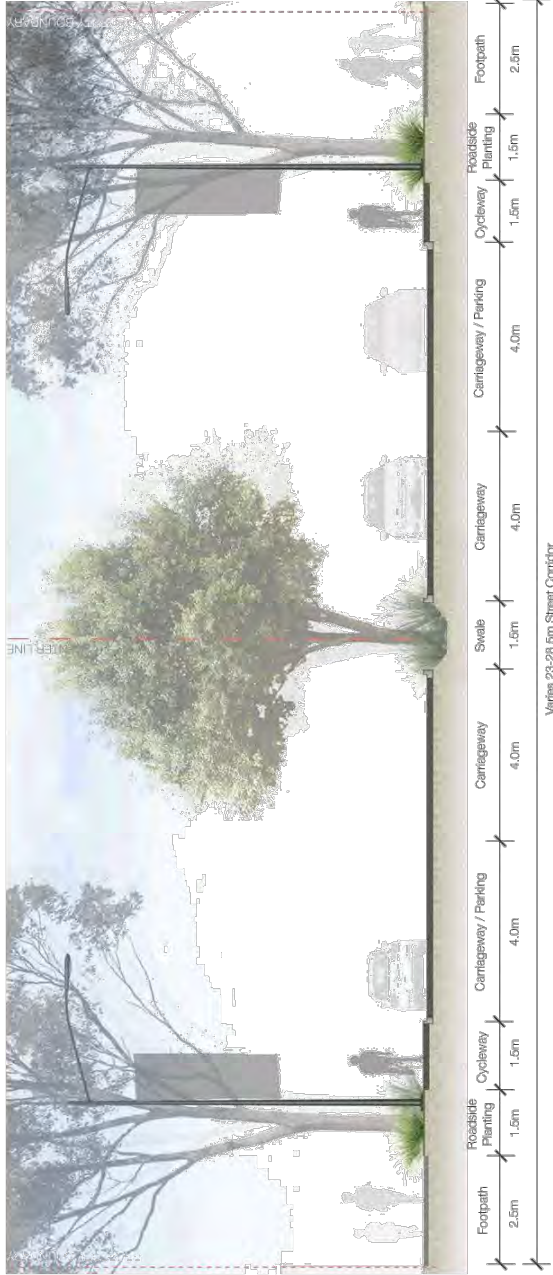
Asphalt surfaces for off-road cycleways - line marking in accordance with NSW Bicycle Guidelines. Concrete footpath to match existing.

Lighting

Existing light poles to be retained in the short term. In the long term, investigate more efficient, low maintenance LED lighting and upgrade poles to incorporate banners. Design light fittings to light both cycleway and road carriageways. Incorporate feature lighting at major gateways to Cessnock Commercial Precinct.



Figure 101: Typical Arterial Road Plan + Section



Veris 23-28.5m Street Corridor



Figure 103: Location Plan

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10.0 Public Domain Plan

Character + Design Principles

- Main retail strip - urban 'spine' of the Centre.
- High pedestrian amenity and high quality of finish is essential.

Series of recommendations:

- Removal of wall barrier and replace with planted garden bed as edge to pedestrian zone;
- Retain existing paving in the short term;
- Replacement / upgrade of existing furniture suite;
- Encourage the installations of Parklets, Pop-Up stores and Public Art to bring another layer of intrigue, interest and activation to the public realm; and
- Interpretative art elements have the potential to be integrated into the street furniture suite;
- Introduce bicycle parking every 100m (in pairs) alternating on either side of the road between Leiland Road & Abchurch Street, and located on both sides of the road between Edward & Hall Streets. Bicycle Parking between Edward & Hall Streets should be sculptural in nature, reflecting the 'wine or mine' theme.

Street Trees + Planting

Retain existing street trees - *Pyrus* spp. Infill with trees where gaps are present along street to a frequency of 20-25m staggered spacing where possible. Understory planting currently consists of *Nandina* Gulf Stream + *Buxus sempervirens*. Infill to greater densities.

Materiality + Furniture

increase the level of amenity along Vincent Street through the addition of simple, robust and aesthetically pleasing street furniture, in particular, encourage outdoor dining with moveable seating to alfresco dining zones by tenants. Potential integration of bus shelters.

Lighting

introduce post-top street lighting as a secondary layer where there are no awnings to establish a pedestrian-oriented street character. Change existing light poles over to efficient LED lighting and incorporate festival banners. A hierarchy of lighting levels should also be introduced to encourage activation after dark: feature uplighting to street trees, additional illumination in alfresco dining areas, etc.

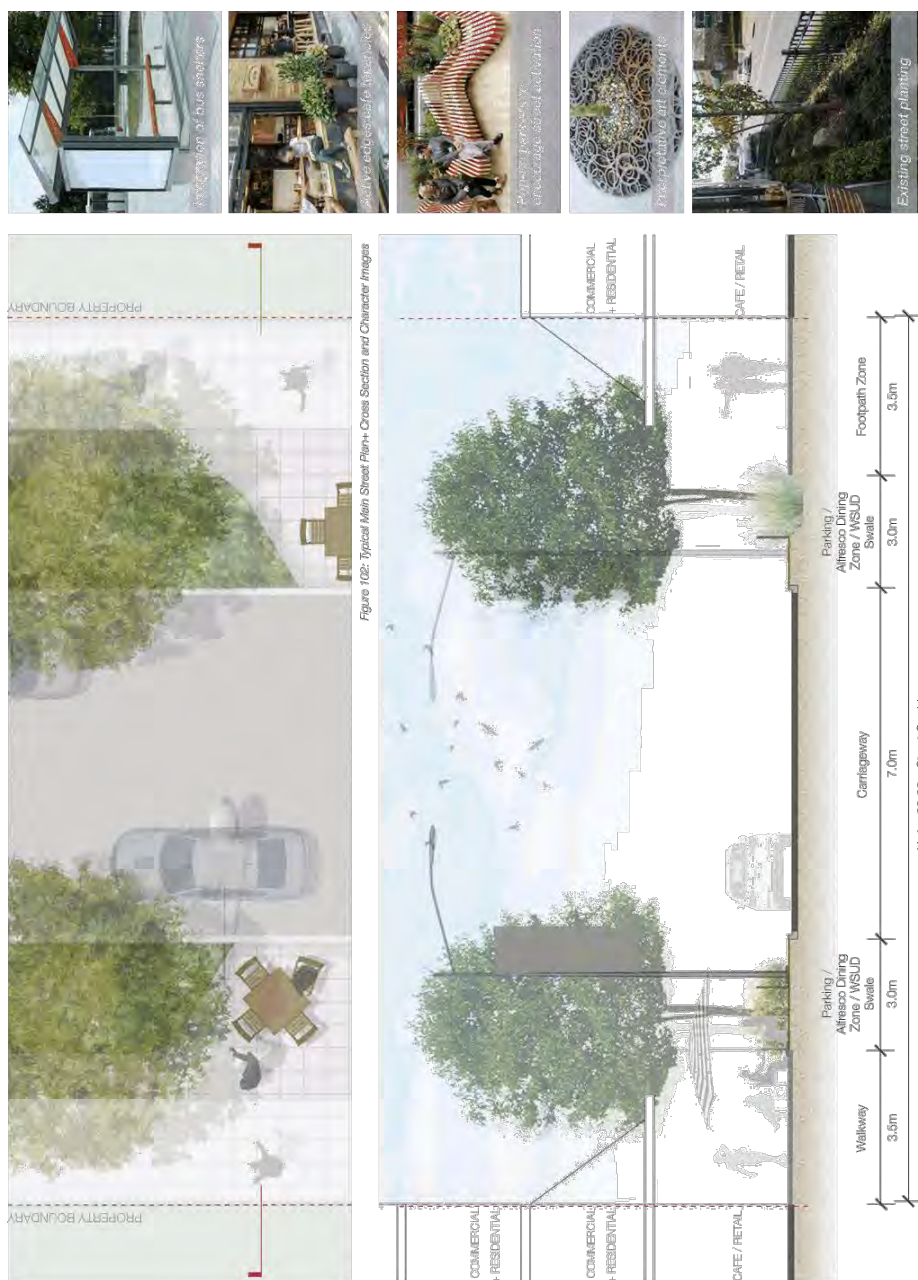


Figure 102: Typical Main Street Plan+ Cross Section and Character Images

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10.0 Public Domain Plan

10.9 Street Type: Cross Street Connection

Character + Design Principles

Envisioned as key green linkages between the Centre's parks and open spaces. The Cross Street Connections will incorporate higher levels of planting in the form of street trees, swales and rain gardens, roadside planting and tenancy planting e.g.

pots / moveable planters along Cooper Street. A section of Cooper Street will be a Shared Zone, with large format pavers across the full width of the street. Vehicles are limited in speed and to a one way route, to facilitate pedestrian movement as well as accommodating for tenancy seating / spill-out zones.

Street Trees + Planting

- Cooper Street: Existing street trees to be retained to west of Cumberland Street. *Waterhousea floribunda*, spacing of staggered arrangement at one per lot or 20m (whichever is lesser), to be planted to east of Cumberland Street. Place at 15m spacings where possible.
- North Avenue + Hall Street: *Waterhousea floribunda*, 8m spacing.

Materiality + Furniture

Upgrade paving lighting, and all street furniture to suit proposed Shared Zone. Integrate finishes palette with proposed Town Square.

Lighting

Upgrade lighting fixtures to increase the ambience after dark and encourage tenancies to capitalise on outdoor dining.



Figure 103: Location Plan

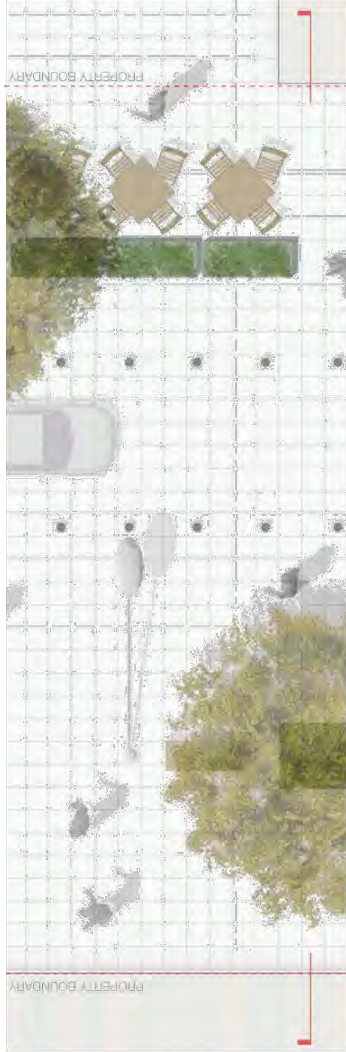
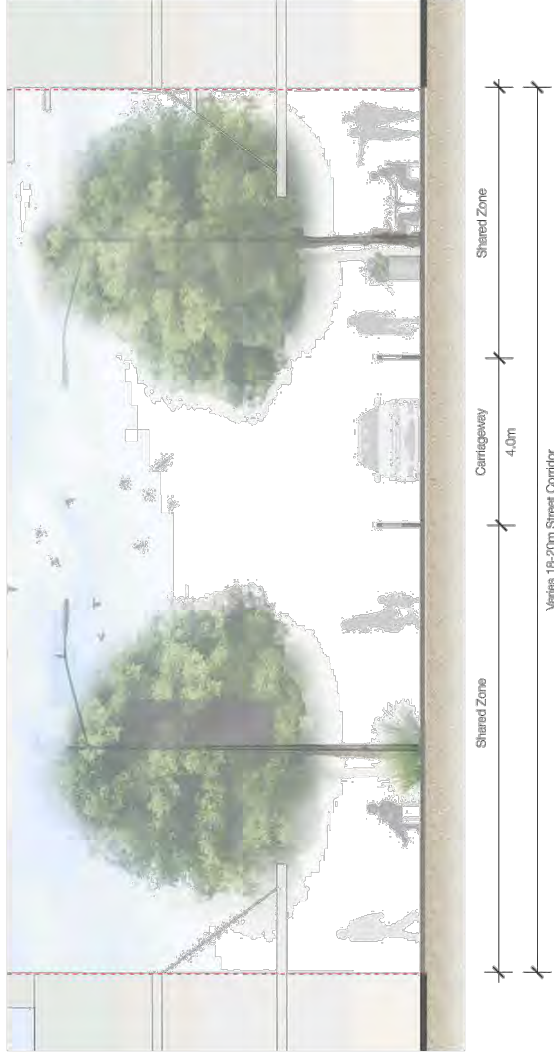


Figure 104: Typical Cross-Street Connection Section and Character Imagery



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10.0 Public Domain Plan

10.10 Street Type: Secondary Street

Character + Design Principles

Integration of a shared path / cycleway, along with the introduction of a series of raised pedestrian crossings will result in a passive, pedestrian friendly street. In Charlton Street - the street corridor will complement the 'catalyst site' of the existing car-park, which is earmarked as a temporary event space. The covered creek channel which runs underneath the street is re-interpreted above ground in the form of a linear swale.

Street Trees + Planting

- Charlton Street: *Elaeagnus reticulatus* within the swale, spacing 10m, opposite arrangement. Native grasses and Macrophyte species for swales, e.g. *Juncus usitatus*, *Carex appressa*, *Lomandra longifolia*, *Isalepis nodosa*.
- Darwin Street: *Lophosieton confertus*, spacing 15m, on the western side of the street.
- Cumberland Street: *Fraxinus angustifolia* 'Raywood', spacing at one per lot or 20m (whichever is lesser), opposite arrangement. Place at 15m spacings where possible.

Materiality + Furniture

Asphalt surface for shared path and raised pedestrian crossings - line marking in accordance with NSW Bicycle Guidelines. Timber boardwalk crossing above swale.

Lighting

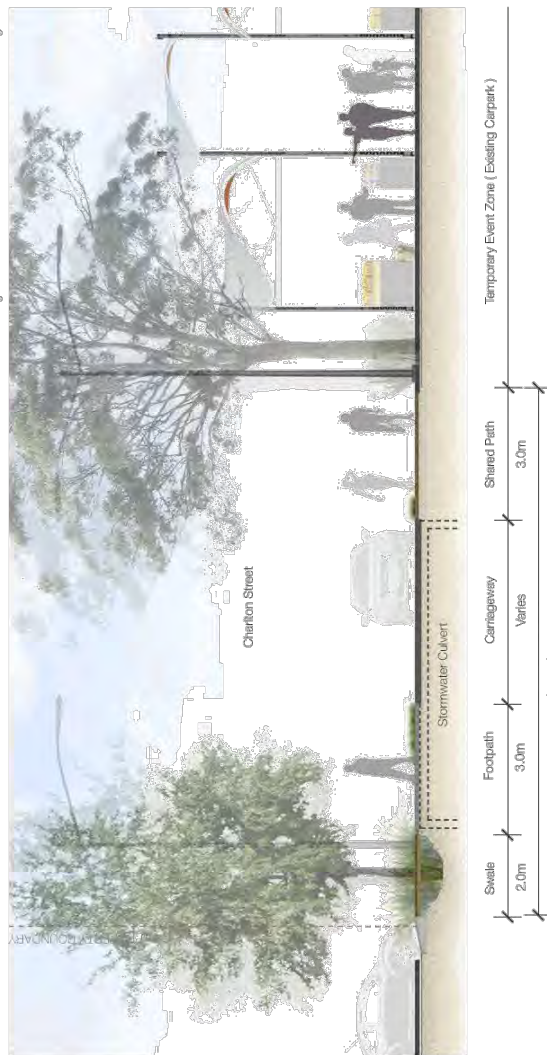
Review standard street light levels to reinforce safety and incorporate special lighting within car-park for 'carpark mode' and 'event mode'.



Figure 105: Location Plan



Figure 106: Charlton Street Section and Character Images



Cycleway / Shared path



Pedestrian crossing



Pedestrian crossing



Swale planting

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10.0 Public Domain Plan

10.11 Street Type: Residential Street

Character + Design Principles

Enhance the residential street environment through the establishment of street trees at regular intervals and the implementation of WSUD principles such as rain gardens and swales where appropriate. The primary aim is to establish green canopies and the presence of low-level planting throughout the residential street zones.

Street Trees + Planting

Where appropriate retain the existing street tree planting. Vary the street tree species among residential streets to avoid creating a monoculture. Select from the following palette:

- Evergreen: *Tieriaopsis laurina* 'Luscibus', *Elaeocarpus reticulatus*, spacing at one per lot or 20m (whichever is lesser), opposite arrangement. Place at 15m spacings where possible.
- Deciduous: *Parus cerasifera*, *Acer fraxgeranum*, spacing at one per lot or 20m (whichever is lesser), opposite arrangement. Place at 15m spacings where possible.
- Rain Garden Planting: e.g. *Juncus usulatus*, *Carex appressa*, *Lomandra longifolia*, *Isolepis nodosa*.

Materiality

Concrete footpaths to be introduced on both sides of the street.

Lighting

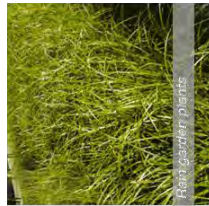
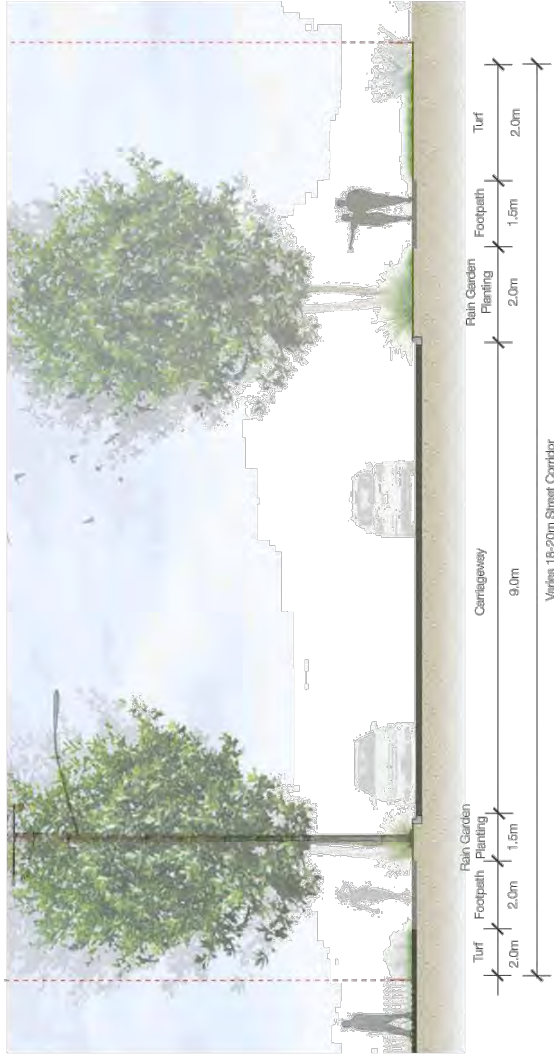
Standard street lighting on both sides of the street, at intervals appropriate for low-traffic, residential streets. Staggered setout.



Figure 107: Location Plan



Figure 108: Typical Residential Street Section and Character Images



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10.0 Public Domain Plan

10.12 Street Type: Laneway

Character + Design Principles

Inspired by the 'laneway revitalisation' revolution, first initiated in Melbourne, and now adopted in many cities and town centers - Cessnock laneways have the potential to create a finer-grained experience that is creative and made safe by introducing activities, passive surveillance, lighting installations and artworks.

Street Trees + Planting

- Capacity to incorporate climbers on wire trellis along laneway walls - green walls. Artistic arrangement complementary to the theme of 'wine country' or Cessnock's 'mining heritage'.
- Large planters housing shade-tolerant trees and accent planting.

Materiality + Furniture

Unit paving to compliment the treatment along Vincent Street. Moveable furniture items to encourage small groups to stop and stay - bringing activity to these spaces which are currently thoroughfares and largely void of people.

Lighting

Feature catenary lighting spanning across the laneway. Potential for light installations along with public sculptures, and temporary exhibits within the laneway.

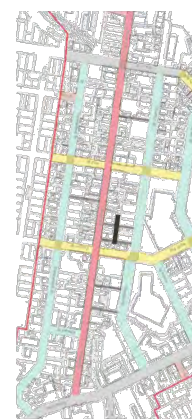
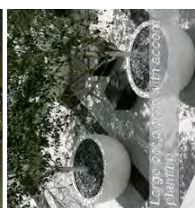
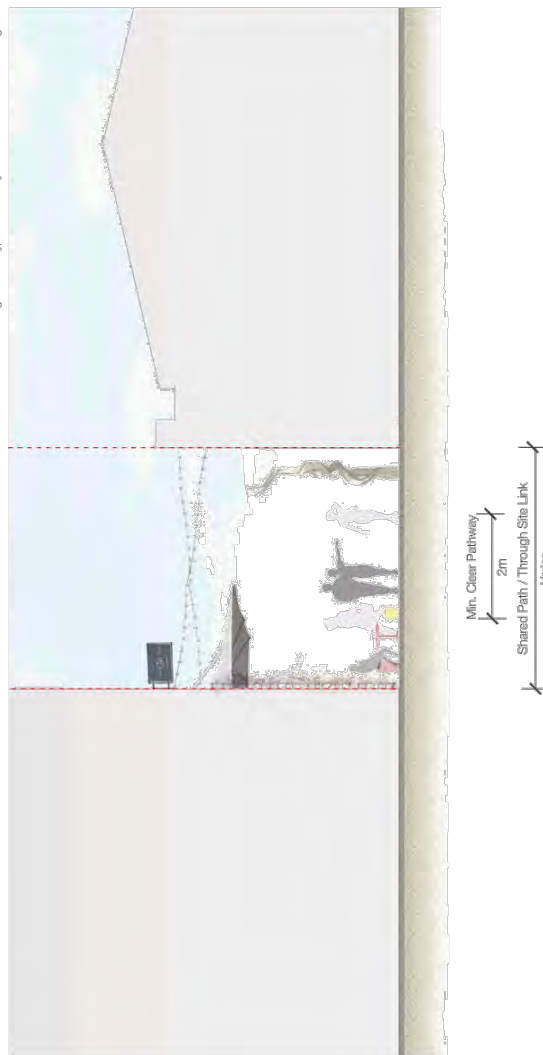


Figure 108: Location Plan



Figure 110: Typical Laneway Section and Character Images



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10.0 Public Domain Plan

‘Green Corridors’ and ‘Urban Waterways’ form a connective network of revitalised open spaces; from Pocket Parks spaces; from Pocket Parks and a Town Square to a Regional Parkland and an iconic Cultural Center.

10.1 Public Domain Plan

The Cessnock Commercial Precinct is currently in need of a strong network of high quality green open spaces and recreational areas. Elements which are crucial in contributing to vibrancy and activation within the Public Realm.

The purpose of this Public Domain Plan is to provide a vision and cohesive plan to guide future development under the provision of a considered and unified network of open spaces, making up the public domain. This plan provides an overall landscape and urban design vision, one of which is driven by the desire to create strong green linkages between new and revitalised open spaces within the existing framework of Cessnock's Commercial Precinct.

The conceptual designs are a generator for directing new development. Indicative in nature, they present ideas and opportunities for creating interesting and engaging public spaces with community amenity as the driver. Street connections, parks, open spaces and urban squares present an opportunity to define the character of the overall Precinct and to provide a high quality, integrated public domain of significance to both locals and to visitors.

The conceptual designs also build upon the visionary theme of Cessnock as a regional ‘Food Bowl’. Two of the largest open-space sites within the precinct present exciting opportunities to integrate attractions at a regional scale: The Cessnock Farmers Market site and the Cultural Centre.

The Public Domain plans bring together the hierarchy of open spaces and street corridors which have presented as part of the Open-Space and Streetscape Typologies chapter. It is here that the individual characteristics envisioned for each site and street corridor have been exhibited in more depth and detail. All conforming to the overall vision and urban and landscape objectives for transforming Cessnock's Centre.

Legend

- A. TAFE and Cultural Centre
- B. Market / Event Site
- 1. Enterprise Corridor
- 2. Civic Park
- 3. Town Square
- 4. City Gateway
- 4A. Gateway to Vineyards
- 6. Pocket Park
- 6. Urban Waterway
- 7. Green Ribbon - Urban Connector (long term vision)
- 8. Urban Spine (Main Street)
- 9. Primary Green Street Connector (including Shared Zone)
- 10. Green Linkages
- 11. Existing Open Space
- 12. Potential Multi-Lane Carpark Stiles
- 13. Potential Cessnock Library Site
- 14. Information Kiosks



Figure 111: Public Domain Plan

CM⁺

10.0 Public Domain Plan



Legend

- A. TAFE and Cultural Centre
- B. Market / Event Site
- 1. Enterprise Corridor
- 2. Civic Park
- 3. Town Square
- 4. City Gateway
- 4A. Gateway to Vineyards
- 5. Pocket Park
- 6. Urban Waterway
- 7. Green Ribbon - Urban Connector (long term vision)
- 8. Urban Spine (Main Street)
- 9. Primary Green Street Connector (including Shared Zone)
- 10. Green Linkages
- 11. Existing Open Space
- 12. Potential Multi-Level Carpark Sites
- 13. Potential Cessnock Library Site
- 14. Information Kiosks

Figure 112: Public Domain Plan - North

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10.0 Public Domain Plan



Figure 11a: Public Domain Plan - Middle

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10.0 Public Domain Plan



Figure 114: Public Domain Infrastructure Plan - South

11.0 Cessnock Town Square

11.1 Introduction

A Town Square is proposed which would provide a social focus and a 'heart' for Cessnock Commercial Precinct. It would be the place to meet - a destination in its own right, and a focus for the community, for socialising, entertainment and relaxation.

The proposed Cessnock Town Square and Cooper Street/Charlton Street Shared Zone will, crucially, link and tie together the precinct's two retail areas: the Vincent Street retail 'spine' and the cluster of major shopping centres situated in the west of the centre.

11.2 Existing Site Issues

An assessment of the proposed site for the Town Square identified a series of urban design issues that impact on the current amenity for users of the centre. The key issues can be summarised as:

- Poor access and legibility for pedestrians and drivers;
- Car dominated - unsafe pedestrian access (many car-pedestrian conflict points);
- Unsafe at night for pedestrians;
- Road infrastructure e.g. roundabouts; makes crossing for pedestrians difficult;
- An attractive group of eucalypt trees at Coles corner;
- Good laneway connections from parking areas, to Vincent Street;
- Long, blank walls along key street frontages;
- Un-used buildings at key locations e.g. former cinema that undermine street life;
- Activity is internalised, or significantly set back from the street;
- Loading dock/messy back-of-house functions are exposed to view; and
- Open drainage canals are unsightly and interrupt regular streetscapes.



Figure 115: Poor access / conflicts



Figure 116: Roundabout / hard parking dominates / attractive Eucalyptus



Figure 117: Blank walls to streets



Figure 118: Activity removed from the street



Figure 119: Exposed 'back of house'



Figure 120: Car dominated

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11.0 Cessnock Town Square



Figure 121: Parking areas / have few trees and pedestrian access is confusing.



Figure 122: Exposed canal is unsightly.



Figure 123: Unused former Cinema building creates a 'dead' frontage.



Figure 124: Roundabout impacts upon pedestrian access along Cooper Street.

11.0 Cessnock Town Square

11.3 Proposed Character

A new 'place' is needed where people can naturally meet and celebrate the unique lifestyle which Cessnock has to offer - a place that has the services infrastructure in place to host regular produce markets, seasonal festivals, where yearly celebrations such as Christmas, Easter, Melbourne Cup and Australia Day can be held.

Building-in infrastructure in the Square, to support, for example, the temporary installation of a video screen, would allow twilight movie nights, real-time broadcast of Olympic or Commonwealth Games events, or carols by candle-light. Providing areas suited for small music events, such as 'Jazz in the Square' or for Winter Lights festivals would enhance the experience of the public realm. Existing events such as the Posty Bike Race would have an appropriate venue for spectators to gather and for the winner to be announced.

The proposed Town Square is seen as a lively meeting place and a new social hub for the centre. The potential program of activities could include:

- Paved areas for weekly markets;
- Shaded seating areas, arbour/pergolas;
- Engaging children's water feature;
- Children's play experience;
- Raised grass areas for sitting/resting/reading;
- Quiet seating areas with movable chairs;
- Small areas for buskers/ live music;
- Interpretive public artwork (feature lighting, paving inserts, sculpture, etc.);
- Outdoor café dining areas;
- Well-lit night-time zone;
- Wayfinding signage and visitor information totems;
- Safe bicycle parking; and
- WSUD interpretation.

The proposed Town Square will need to be a collaborative effort, with Council working with key Stakeholders, including Woolworths, Coles, Cessnock Leagues Club, various property owners, Hunter Water and Roads and Maritime Services (RMS) to deliver an important new community and social 'asset' for Cessnock.



Figure 125: A mix of 'green' and 'beaved' spaces, including comfortable seating and public art.



Figure 126: Live music, e.g., busking.



Figure 127: Weekly produce market.

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11.0 Cessnock Town Square



Figure 125: Engaging children's water experiences.



Figure 126: Simple shaded areas.

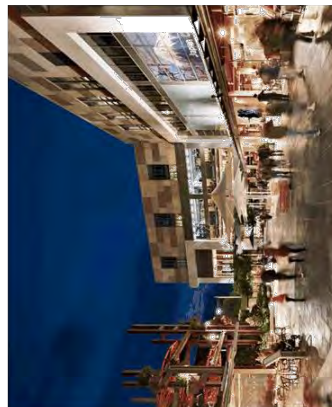


Figure 130: A focus for night-time activity.

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11.0 Cessnock Town Square



Figure 131: Engaging children's play experience.



Figure 132: Movies in the 'square'.



Figure 133: Quality paving, landscape and public art.

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11.0 Cessnock Town Square

11.4 Design Concept - Stage 1

The Town Square project would be rolled-out in three stages. This will dovetail with available funding, the reorganising of car parking arrangements, the re-routing of existing bus routes (to facilitate the Shared Zone concept) and the readiness for key stakeholders to partner with Council on the delivery of the Square.

Stage 1 Works

The upgrade works include:

- Converting Cooper Street into a one way east, Shared Zone, from Vincent Street to the Keene Street-North Avenue intersection;
- Converting Charlton Street into a two way Shared Zone north of Cooper Street, and a one way north Shared Zone, south of Cooper Street;
- Upgrading all paving to a large format charcoal colour honed concrete "flagstone paver" for the full width of the street, removing kerbs and gutters, defining the passage for vehicles with bollards and other new street furniture;
- Upgrading all street lighting to new smart pole standard (underground power supply);
- Including new street furniture, seating areas and outdoor dining areas for café/restaurants (Cooper Street);
- Wherever possible retaining the existing group of eucalypt trees and the street trees in Cooper Street, and consolidating with new street trees;
- Incorporating four short stay parallel parking spaces (Cooper Street);
- Including paving and pedestrian lighting upgrades and public artwork for the laneway between Vincent Street and Charlton Street; and
- Incorporating new wayfinding signage, supermarket sign and visitor information totems.

Funding

Stage 1 is primarily funded from existing Council budget provisions and is on Council owned land – this allows an early start on these upgrade works. All services, paving levels and the Upgrade finishes palette will be designed to allow seamless expansion of the square in subsequent project stages.

Facilitating Infrastructure

The re-routing of existing Cooper Street bus services will be required, refer Figure 80: Public Transport Network, for the Shared Zone to be approved by the FMS. The proposal is to re-route bus services from Wollombi Road via Keene Street, which will require the completion of the north section of Keene Street.



Figure 194: Town Square Concept - Stage 1

11.0 Cessnock Town Square

that Council build a multi-level public car parking facility on Keene Street that would have a proportion of the car spaces dedicated for Woolworths customers, equivalent to the number of spaces lost.

This phase of the Town Square project relies on the owners of the Woolworths Casenbrook Plaza Shopping Centre site to partner with Council on the delivery of the square - initial meetings with the owner have indicated a willingness to work with Council on the project.

The current privately owned car parking areas which front Cooper Street would be dedicated to Council for public use - to construct the north of the Town Square. The owner would be compensated for the loss of parking spaces, by Council constructing a multi-level public carpark on Keene Street, adjacent to the Shopping Centre.

The upgrade works include:

- Converting the current Woolworths carpark to a paved Town Square (north part);
- Upgrading paving to a large format charcoal colour honed concrete 'flagstone paver';
- Reconfigure Keene Street entry/exit to shopping centre carpark;
- Upgrading all street lighting to new smart pole standard (underground power supply);
- New pedestrian scale 'catenary' and feature lighting for the Square;
- New street furniture, seating areas and outdoor dining areas for cafés/restaurants;
- Interactive children's play experience;
- Wherever possible retaining existing street trees, and consolidate surrounding streetscapes with new trees;
- Interpretation of Coal Mining heritage (sculpture/artwork); and
- Incorporating new walking and visitor information signs.

Stage 2 would be joint funded – from private developer contributions and the value of land dedication, supplemented by Council from the annual works budget.

The proposed agreement between the land owner and Council involves the existing privately held land, currently used for car parking, to be dedicated to Council (public use). It is proposed



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11.0 Cessnock Town Square

11.6 Design Concept - Stage 3

This phase of the Town Square project relies on the owners of the Coles Supermarket site to partner with Council on the delivery of the Square - initial meetings with the owner have indicated a limited interest at this time to work with Council on the project.

It is proposed in the long term that the current privately owned car parking area at the corner of Cooper Street and Charlton Street would be dedicated to Council for public use to construct the south of the Town Square.

The owner would be compensated for the loss of parking spaces, by Council constructing a multi-level public carpark on the current public car park site it owns on Charlton Street. There is an opportunity to incorporate at the ground floor level an expanded Cessnock Library - if approved by Council, this location would bring activity and passive surveillance to Charlton Street, the Vincent Street laneway and to the Town Square.

Stage 3 Works

The upgrade works include:

- Converting the current Coles carpark to a paved Town Square (south part);
- Upgrading paving to a large format charcoal colour honed concrete "flagstone paver";
- Upgrading all street lighting to new smart pole standard (underground power supply);
- New pedestrian scale 'catenary' and feature lighting for the square;
- New street furniture, seating areas and outdoor dining areas for café / restaurants;
- Interactive water feature;
- Wherever possible retaining existing street trees, group of eucalypts and consolidating surrounding streetscapes with new trees;
- Shaded, raised grass areas with seating edges;
- Interpretive public sculpture/artwork; and
- Incorporating new wayfinding and visitor information signs.

Funding

Stage 3 would be joint funded - from private developer contributions and the value of land dedication, supplemented by Council from the annual works budget.

Facilitating Infrastructure

The proposed agreement between the land owner and Council involves the existing privately held land, currently used for car parking, to be dedicated to Council (public use). It is proposed that Council build a multi-level public car parking facility on Charlton Street that would have a proportion of the car spaces dedicated for Coles customers, equivalent to the number of spaces lost.



Figure 196: Town Square Concept - Stage 3

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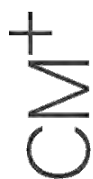
11.0 Cessnock Town Square

11.7 Car Parking Strategy

The adjoining diagram illustrates the proposed relocation of car parking spaces from existing at-grade parking areas, currently owned by the shopping centres, to new multi-level public car parking facilities built by Council. This reconfiguring of car parking within the centre provides an opportunity for additional public domain area and to realise a major new public place for the community – Cessnock Town Square.

Further, relocated parking spaces would potentially only take up one third of the new car parking facilities, (assuming they are three stories high), allowing additional car parking spaces to be provided in support of both Vincent Street and shopping centre businesses.





12.0 Implementation Plan

12.1 Introduction

The Implementation Plan describes the strategy to implement and deliver the urban planning and urban design initiatives developed in the preparation of the Development Control Plan and the Public Domain Plan (Chapter 10).

12.2 CBD Masterplan - Public Domain Implementation Plan 2012

The Cessnock CBD Masterplan Study was completed in 2012. The associated Implementation Plan included ten Public Domain Initiatives. In summary, they were:

- Develop TAFE central parkland and community gardens;
- Vincent Street as an 'eat-street' – al-fresco dining;
- Upgrade H.J Stienback Lions Park;
- Beautification of the open drainage channels;
- Improve passive surveillance of public domain:
 - Improve permeability/access;
 - Consolidate car parking;
 - Attract more CBD residents;
 - Promote night-time economy.
- Provide high level of lighting throughout CBD;
- Maximise people living in and using the CBD;
- Develop public art strategy;
- Develop signage/interactive strategy; and
- Coordinate street furniture elements and de-clutter.

In the five years since the CBD Masterplan was adopted by Council, a handful of the initiatives have been implemented, including improvements to Vincent Street, a small number of new residents are now living in the CBD, and a number of public art and community festivals, events and markets have encouraged locals and visitors to the CBD.

There is still much to do to realise the CBD Masterplan's 'Attractive, Thriving, Welcoming' vision for the CBD. In setting goals for the improvement of the public domain over the next twenty years, it is recommended that public domain initiatives are focussed and are realistic in scope and budget, in order to maximise the potential for delivery.

12.3 Recommended Amendments to CLEP 2011

The Indicative Layout Plan describes the proposed long term urban planning outcome for the Cessnock Commercial Precinct – it looks ahead to the next 25 years and describes the key strategic moves that will be required to achieve the overall 'vision' for the Centre.

Whilst the focus of this study has been the preparation of a Cessnock Commercial Precinct DCP and the preparation of an accompanying Public Domain Plan, there are a number of urban planning strategies that would most effectively be realised through the amendment of existing CLEP legislation. The recommended amendments to CLEP 2011 are described in the following pages.

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12.0 Implementation Plan

12.3.1 Zoning

The following amendments to the zoning of land within the Cessnock Commercial Precinct are proposed:

Cessnock South Arterial Bypass

Wollombi Road, Maitland Road and Allandale Road currently provide an effective northern arterial access for the Commercial Precinct and one zoned SP2 Infrastructure. Vincent Street is currently classified as a State Road (RMS responsibility) with a zoning of SP2. The opening of the Hunter Expressway has reduced the volume of through-traffic using Vincent Street – Cessnock's retail 'Main Street'; however, it still carries significant traffic, as it is the primary route for travellers heading northwest to the Hunter and southeast to Sydney.

In the long term, the Cessnock Transport Strategy has identified two potential South Bypass routes: Firstly, West Avenue, South Avenue and Aberdare Road; and secondly, a new alignment extending southwest via the B7 Business Park lands – this route has the added benefit of opening up these lands, providing improved access and a street address for potential businesses.

On approval by Council of the bypass strategy, the new South Arterial Bypass corridor would be rezoned to SP2 Infrastructure. The opportunity this presents is to rezone Vincent Street from SP2 Infrastructure (State Road) to a local street, under Council's jurisdiction, falling within the overall B2 and B4 business zones. These changes would provide opportunities to improve the pedestrian amenity and safety of the Vincent Street retail 'spine', encouraging 'al-fresco' dining and an attractive promenade.

Cessnock Baths Site

The Baths are currently the subject of a Council study assessing the feasibility of upgrading and relocating the facility to a new site outside the Commercial Precinct. If Council resolves to relocate the swimming pool facility (rather than an upgrade on-site), it is recommended that the site be rezoned from its current zoning of RE1 Public Recreation, to B4 Mixed Use. This is consistent with the adjoining zoning, and is the most appropriate zoning for this location.

Charlton Street Extension North to Wollombi Road

Completing this street through to Wollombi Road is advantageous as it completes another section of the street grid, building-in improved access to properties and feasibility for access within the centre. A further benefit is that a current crime 'hot spot', if opened

up with regular passing traffic, will improve passive surveillance and therefore safety in this part of the Commercial Precinct. It is recommended that the proposed public park along the west edge of the existing canal, is rezoned RE1 Public Recreation.

Cessnock Leagues Club + Keane Street Connection

Land currently zoned B4 Mixed Use is proposed to be rezoned B3 Commercial Core for consistency in zoning for the core of the

Commercial Precinct. The proposed completion of Keane Street, linking through to Wollombi Road, traverses this land parcel.

Existing 'Public' Parks

The Wollombi Road Gateway Park and Edward Street East Closure are both identified by Council as valued public park spaces for the community. HU Sternbeck Lions Park on the corner of Vincent Street and Edward Street, is currently privately owned, however it

is recommended, when funds are available, that it be purchased by Council for public recreational use, in perpetuity. The rezoning of these land parcels to RE1 Public Recreation would reflect the intended long term use.

Proposed Cessnock Town Square

A new 'Town Square' is proposed along Cooper Street (west of Charlton Street). It is proposed that lands currently used

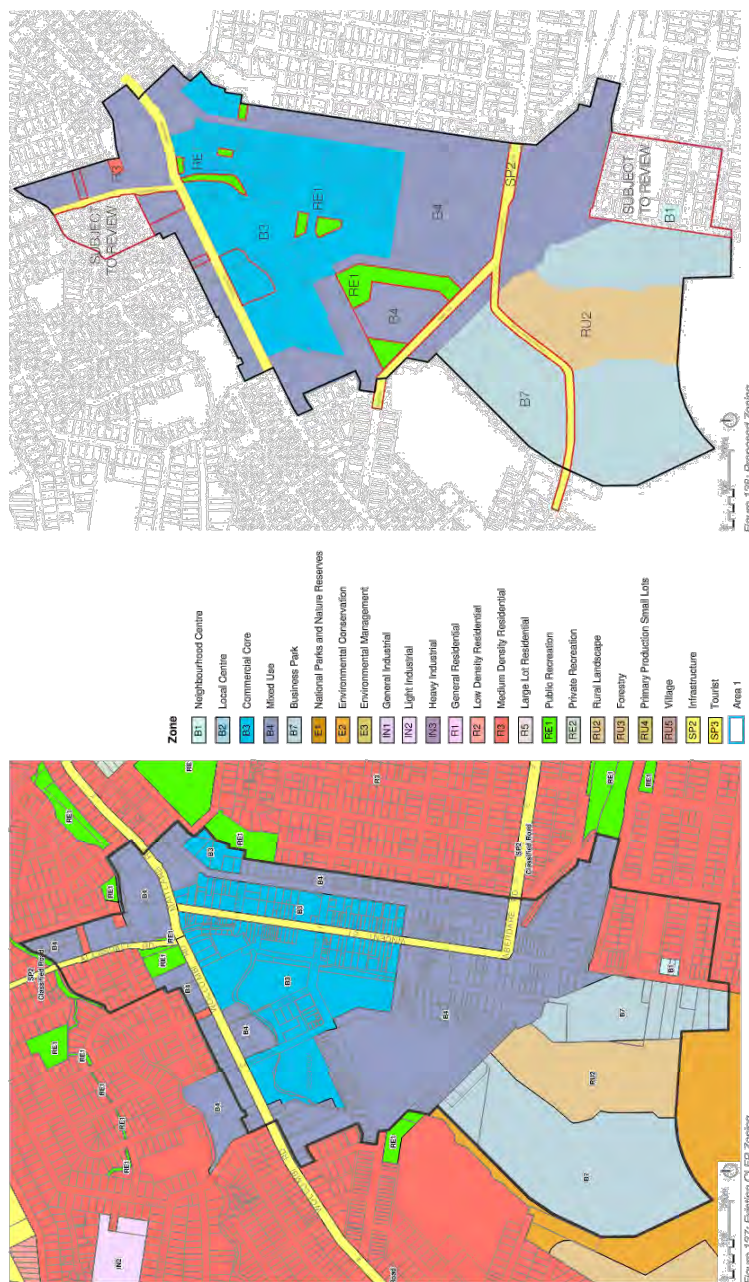


Figure 137: Existing CLEP Zoning

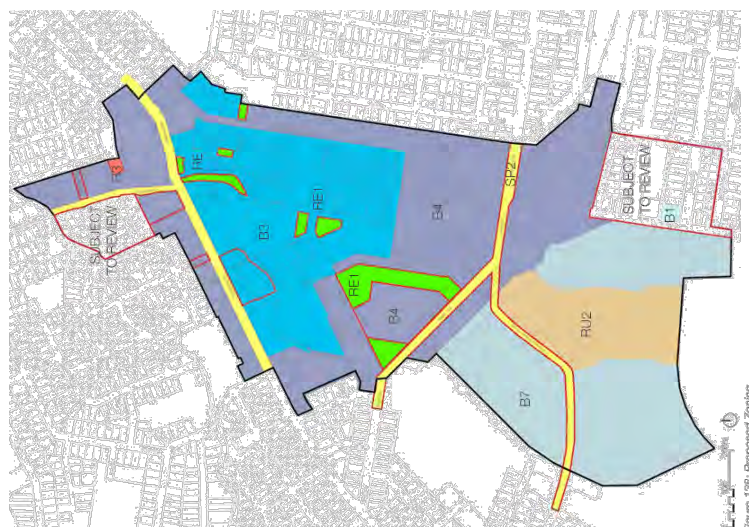


Figure 138: Proposed Zoning

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12.0 Implementation Plan

as shopping centre car parking areas, would be dedicated by the landowners to Council for the purpose, and the owners compensated with new car parking spaces provided in adjacent multi-level public car parking facilities provided by Council. It is recommended that these land parcels are rezoned RE1 Public Recreation.

R3 Medium Density Residential

Two residential areas, one in the north, another in the south of the Commercial Precinct, are the subject of a separate land use study, and their future zoning is therefore pending, awaiting the outcome of this study.

12.3.2 Land Reservation Acquisition

The following Cessnock Commercial Precinct land parcels are recommended for land reservation or acquisition:

Cessnock South Arterial Bypass

On approval by Council of the bypass strategy, the new South Arterial Bypass corridor would be rezoned to SP2 Infrastructure.

Keane Street Connection

The acquisition of the proposed completion of Keane Street, linking through to Wollombi Road, is essential to allow for alternate bus routes through the Commercial Precinct, thereby facilitating the proposed Cooper/Charlton Street Shared Zone and the Cessnock Town Square.

Proposed Cessnock Town Square

A new Town Square is proposed along Cooper Street (west of Charlton Street). It is proposed that lands currently used as shopping centre car parking areas, would be dedicated by the landowners to Council for the purpose.

Charlton Street Extension North to Wollombi Road

Completing this street through to Wollombi Road is advantageous as it completes another section of the street grid, building-in improved access to properties and flexibility for access within the centre. It is recommended that lands are acquired at the canal crossing point, for the final section of street through to Wollombi Road, and for the proposed public park along the west edge of the existing canal.



Figure 139: Existing CLEP Land Reservation Acquisition

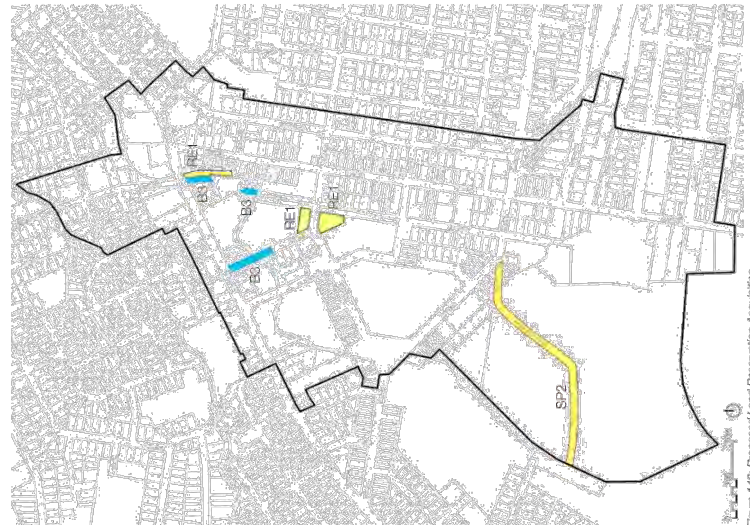


Figure 140: Proposed Land Reservation Acquisition

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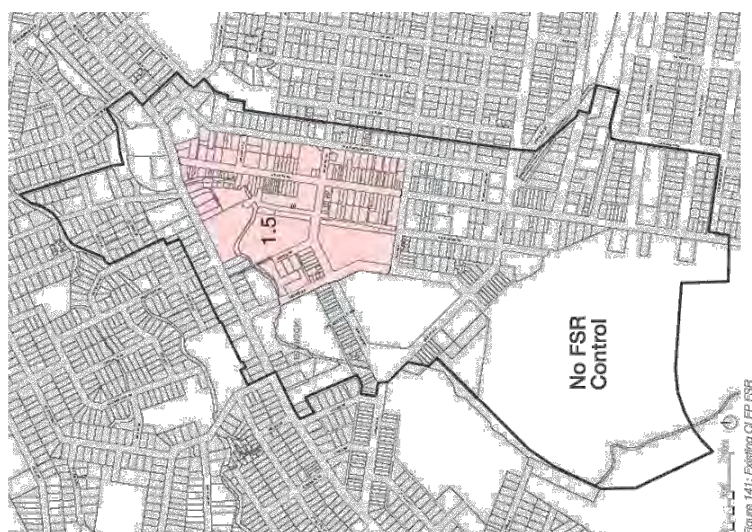
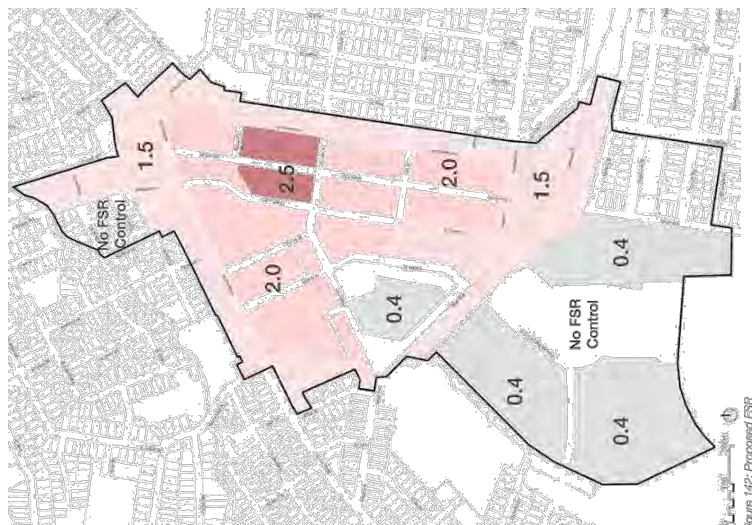
12.0 Implementation Plan

12.3.3 Floor Space Ratio (FSR)

Amendments to the Floor Space Ratio (FSR) of land within the Cessnock Commercial Precinct are proposed. Revised density controls are proposed which will cover most of the properties within the Precinct – the only exceptions being those areas the subject of ongoing Medium Density Residential review, and the area in the south of the Precinct, adjoining the Business Park which is set aside for environmental protection.

TAFE and Business Park areas maintain a lower density in keeping with the existing and expected long term land uses and activities. Density of development continues to focus in the northern 'core' of the centre, reinforcing the Vincent Street retail/commercial 'spine' and embracing the cluster of shopping centre developments which form the core of the existing and projected future retail focus for the Commercial Precinct.

Higher density, to a maximum of FSR 2.5:1, is permitted in two precinct blocks at the core of the precinct, reflecting the importance of these urban blocks to future growth. These key development blocks include the Cessnock City Council offices, an entertainment hub and a large vacant site, have good vehicular access and are suitable for increased density. These blocks reinforce the Precinct urban structure and the commercial importance of the north of the centre.



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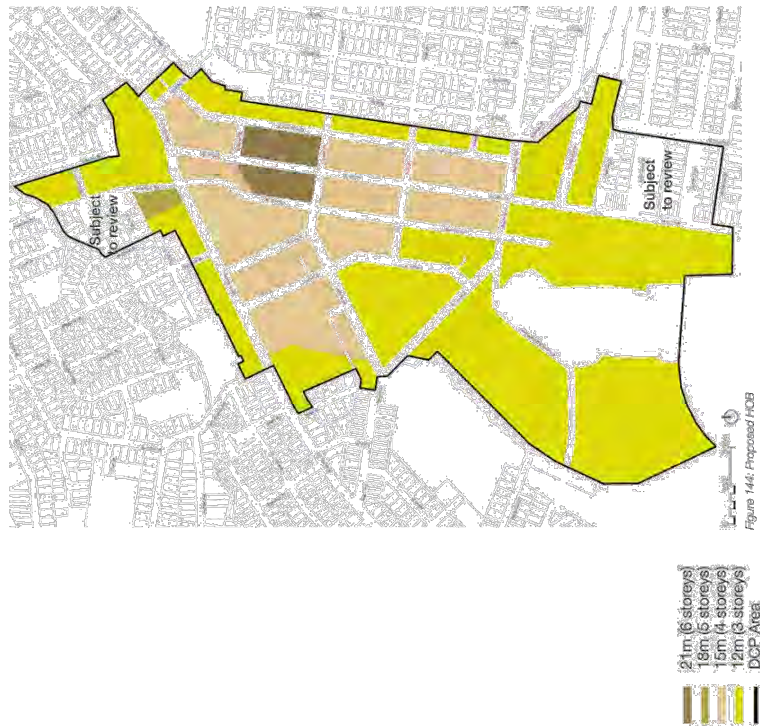
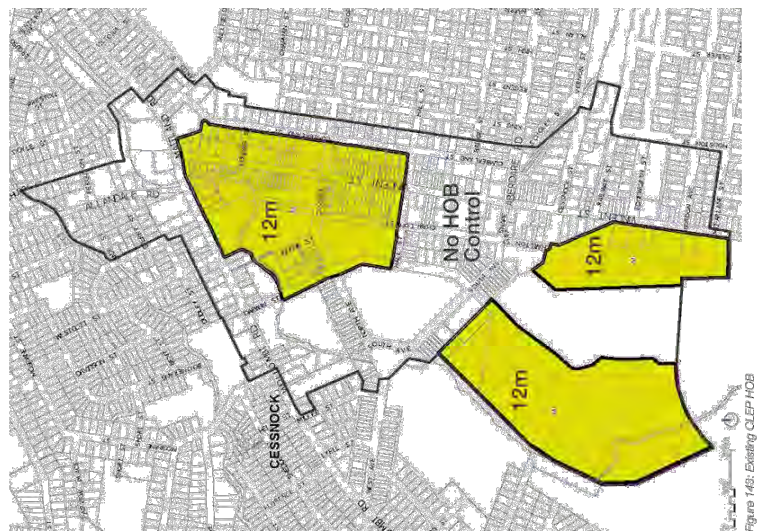
12.0 Implementation Plan

12.3.4 Height of Building (HOB)

Amendments to the Height of Buildings (HOB) permitted for land within the Cessnock Commercial Precinct are proposed. Revised building height controls are proposed which will cover most of the properties within the Precinct – the only exceptions being those areas the subject of ongoing Medium Density Residential review, and the area in the south of the Precinct, adjoining the Business Park which is set aside for environmental protection.

TAFE, Business Park and peripheral residential areas maintain a lower height, providing an appropriate height transition to surrounding neighbourhoods and environmental zones. Taller buildings continue to be focused in the north of the centre, reinforcing the Vincent Street retail/commercial 'spine' and embracing the cluster of shopping centre developments which reflects the existing and projected future retail focus for the Commercial Precinct.

The tallest buildings, to a maximum of six storeys (21m), are permitted in two blocks of the precinct, reflecting the importance of these to future growth. These key development blocks mark the 'core' of the Commercial Precinct, reinforcing urban structure and the commercial focus.



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12.0 Implementation Plan

12.4 Preliminary Budget Estimate

12.4.1 Introduction

Based on the Public Domain Plan and materials/finishes, furniture and landscape details provided by the design team, preliminary Budget Estimates have been prepared by BDA Consultants Pty Ltd (Quantity Surveyors and Building Economists) for the upgrade of the Commercial Precinct public domain. The estimated cost has been calculated for each street (there are over 30 streets in the Centre), and for each laneway, public park, gateway, and for new urban places, such as the proposed Town Square

12.4.2 Public Domain Plan - North

In the north of the Commercial Precinct the priority upgrade projects include construction of the Cooper Street/Charlton Street Shared Zone, the north part of the proposed Cessnock Town Square, the Keane Street multi-level public carpark, and the completion of key city streets, including, as a priority, Keane Street (to permit the re-routing of buses) and then followed by Charlton Street through to the Wollombi Road/Maitland Road/Alendale Road intersection.



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12.0 Implementation Plan

In the middle of the Commercial Precinct the priority upgrade projects include construction of the Civic Park Upgrade at the north east corner of the TAFE site, the south part of the proposed Cessnock Town Square and the construction of the Charlton Street Public Carpark and potentially on the ground floor level a new Cessnock Library facility.



Figure 146: Preliminary Budget Estimate - PDP (Mobile)

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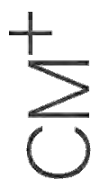
12.0 Implementation Plan

12.4.4 Public Domain Plan - South

In the south of the Commercial Precinct the priority upgrade project is the construction of the South Bypass / Enterprise Road. This will open up new opportunities for improving the amenity of Vincent Street and make the business park zoned lands much more accessible and inviting for businesses.



Figure 147: Preliminary Budget Estimate - PDP (South)



12.0 Implementation Plan

12.5 Nexus of Key Projects

12.5.1 Planning Pathway

The proposed legislative changes would be realised through lodging a consolidated Commercial Precinct Planning Proposal to the Department of Planning and Environment. If granted 'Gateway' status by the Department, the planned changes would go on Public Exhibition for the statutory period, and if resolved well by the community, would then be adopted by Council as policy and the planning legislation for Cessnock would then be gazetted by the Department (thereby legally enacting the planning controls).

12.5.2 The Nexus

The 'roll-out' of a series of interrelated public domain upgrade projects is proposed over the next 5-10 years. The final timing of implementation will be determined by the funding available through yearly Council budget allocations, State Government grants and from levies / contributions provided by the private sector.

With regard to the proposed Town Square, a collaborative 'partnering' of Council with key private land owners is proposed, to leverage Council's land holdings in the Precinct. In order to realise an important new draw-card public place for Cessnock.

The 'priority projects' identified, build upon the upgrade works along Vincent Street, carried out previously by Council, and extend these works, as the next logical step in the development of the Commercial Precinct, connecting and tying together key retail development in the precinct. Each project unlocks potential growth and business in the precinct and has a significant community-building potential – creating major new public places (Town Square), library facilities and recreational facilities.

Each project, supports and strengthens the other, and collectively will generate a synergy - in combination enhancing the precinct's overall attractiveness to local residents and visitors alike. The Town Square forms the 'nexus' of the public domain upgrade proposals, forming an activity 'hub' around which the various existing streets and public places are reconnected to form a new Cessnock public domain that is integrated, safe, welcoming and attractive.

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Cessnock Commercial Precinct Project

Appendix 1 - Preliminary Budget Estimate

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Preliminary Budget Estimate

Ref.	Street	From	To	Type	Length (m)	Rate	Intensity	Amount
A. NORTHERN ZONE:								
1	Vincent	Cooper	Wollombi	Main	366	5,050	20%	5,097,640
2	Wade	Boundary	Allendale	Residential	148	3,194	60%	369,660
3	Sperry	Boundary	Allendale	Residential	135	3,194	70%	283,605
4	Ferguson	Boundary	Wollombi	Arterial	330	5,763	25%	301,809
5	Allendale	Boundary	Wollombi	Arterial	330	5,763	25%	167,672
6	Westcott	Boundary	Wollombi	Arterial	636	3,194	10%	475,406
7	Wollombi	Boundary	Wollombi	Arterial	154	5,763	25%	916,238
8	Maitland	Boundary	Wollombi	Arterial	108	3,200	35%	221,856
9	Millfield	Boundary	Wollombi	Secondary	81	3,200	50%	129,600
10	Edward	Boundary	Wollombi	Secondary	40	3,194	50%	63,875
11	Market	Boundary	Wollombi	Secondary	228	3,200	15%	109,440
12	Darwin	Boundary	Wollombi	Secondary	108	3,200	25%	86,400
13	Keene	Boundary	Wollombi	Secondary	300	3,200	25%	240,000
14	Charlton	Boundary	Wollombi	Secondary	400	3,200	25%	320,000
15	Cumberland	Boundary	Wollombi	Secondary	140	2,025	90%	255,150
16	Laueways	Boundary	Wollombi	Secondary	3800	150	(m2)	570,000
17	Gateway & pocket parks	Boundary	Wollombi	Secondary		PC		450,000
18	Signage, furniture & artworks	Boundary	Wollombi	Secondary				
Sub-Total (34%) \$								
15,052,253								

Ref.	Street	From	To	Type	Length (m)	Rate	Intensity	Amount
B. CENTRAL ZONE:								
1	Vincent	Cooper	Wollombi	Main	517	5,050	30%	783,255
2	Cooper	Boundary	Wollombi	Cross	191	5,023	75%	719,473
3	North	Boundary	Wollombi	Cross	483	5,023	50%	1,212,934
4	Hall	Boundary	Wollombi	Cross	280	5,023	75%	1,054,725
5	Snape	Boundary	Wollombi	Arterial	222	5,763	75%	959,456
6	South	Boundary	Wollombi	Arterial	458	5,763	60%	1,583,535
7	Bridge	Boundary	Wollombi	Residential	40	3,194	10%	12,775
8	Castlemaine	Boundary	Wollombi	Cross	80	5,023	10%	40,180
9	Darwin	Boundary	Wollombi	Secondary	358	3,200	35%	400,960
10	Charlton	Boundary	Wollombi	Secondary	284	3,200	40%	363,520
11	Charlton	Boundary	Wollombi	Secondary	226	3,200	60%	433,920
12	Cumberland	Boundary	Wollombi	Secondary	486	3,200	35%	544,320
13	Laueways	Boundary	Wollombi	Secondary	320	2,025	90%	583,200
14	Gateway & pocket parks	Boundary	Wollombi	Secondary	3900	150	(m2)	585,000
15	Town square	Boundary	Wollombi	Secondary	5250	600	(m2)	3,150,000
16	City events / markets	Boundary	Wollombi	Secondary	5000	75	(m2)	375,000
17	Civic park	Boundary	Wollombi	Secondary	9000	150	(m2)	1,350,000
18	Wine & mine centre	Boundary	Wollombi	Secondary		PC		excluded
19	Signage, furniture & artworks	Boundary	Wollombi	Secondary				900,000
Sub-Total (68%) \$								
15,052,253								

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Appendix 2 - Public Domain Furniture and Finishes Schedule

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Materials Schedule - Parks and Open Spaces

1.0 CIVIC PARK									
REF	ITEM	IMAGE	MATERIAL/PRODUCT	TEXTURE / FINISH	NO. OF ITEMS	PRODUCT / SUPPLIER	PRODUCT / SUPPLIER	PRODUCT / SUPPLIER	PRODUCT / SUPPLIER
NOTE: Refer Fig. 35: Public Domain Plan of Public Domain Plan Document									
1.1	Pavement Type 1 Coloured concrete broomed finish		Concrete broomed finish	Concrete broomed finish	1	Product: Coloured concrete broomed finish Supplier: http://www.broomed.com.au	Product: Coloured concrete broomed finish Supplier: http://www.broomed.com.au	Product: Coloured concrete broomed finish Supplier: http://www.broomed.com.au	Product: Coloured concrete broomed finish Supplier: http://www.broomed.com.au
1.2	Pavement Type 2 Joint paving		Concrete paver	Concrete paver	1	Product: Concrete paver Supplier: http://www.concretepaver.com.au	Product: Concrete paver Supplier: http://www.concretepaver.com.au	Product: Concrete paver Supplier: http://www.concretepaver.com.au	Product: Concrete paver Supplier: http://www.concretepaver.com.au
1.3	Pavement Type 3 Gravelled turf		Gravelled turf	Gravelled turf	1	Product: Gravelled turf Supplier: http://www.gravelledturf.com.au	Product: Gravelled turf Supplier: http://www.gravelledturf.com.au	Product: Gravelled turf Supplier: http://www.gravelledturf.com.au	Product: Gravelled turf Supplier: http://www.gravelledturf.com.au
1.4	Pavement Type 4 Polished concrete		Polished concrete	Polished concrete	1	Product: Polished concrete Supplier: http://www.polishedconcrete.com.au	Product: Polished concrete Supplier: http://www.polishedconcrete.com.au	Product: Polished concrete Supplier: http://www.polishedconcrete.com.au	Product: Polished concrete Supplier: http://www.polishedconcrete.com.au
1.5	Furniture Type 1 Bench Seat		Aluminium	Aluminium	1	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au
1.6	Furniture Type 2 Bench Seat		Aluminium	Aluminium	1	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au
1.7	Furniture Type 3 Picnic Bin		Aluminium	Aluminium	1	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au
1.8	Furniture Type 4 Bicycle Rack		Aluminium	Aluminium	1	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au
1.9	Furniture Type 5 Shading Furniture		Aluminium	Aluminium	1	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au
1.10	PC Item Sculptural Play Item		Aluminium	Aluminium	1	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au
1.11	PC Item Sculptural Play Item		Aluminium	Aluminium	1	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au
1.12	PC Item Sculptural Play Item		Aluminium	Aluminium	1	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au	Product: Aluminium Supplier: http://www.aluminium.com.au

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Materials Schedule - Parks and Open Spaces







3.3	Furniture Type 3 Recycled Bin		Handwood Body Business Steel Ltd Business Recycling		2	Product: Supplier:	Simple Bin Balanced Steel Furniture Australia Phone: 1300 827 788 http://simplebin.com.au
3.5	Furniture Type 4 Ducktail Fountain		Aluminium	Power-actuated	3	Product: Supplier:	Aqua Fountain Steel Furniture Australia Phone: 1300 827 788 http://simplebin.com.au
3.7	PC Item Play Item		Steel		1	Product: Supplier:	N/A
3.8	Landscape Tree Fencing	N/A	TOLL Tree Stock	N/A	Approx. 5	Product: Supplier:	N/A
3.9	Landscape New Grass-Grass Paving	N/A	Grass Grass Grass Grass + Groundcover	N/A	Approx. 100m ²	Product: Supplier:	N/A
4.0	TOWN SQUARE	ON HOLD					
4.1	REF NO.	ITEM	MATERIAL / PRODUCT	TEXTURE / FINISH	NO. OF ITEMS	Product: Supplier:	PRODUCT / SUPPLIER (OR APPROVED EQUIVALENT)
			Kxxx	xxxx	N/A		
5.0	CATALYST SITES						
5.1	REF NO.	ITEM	MATERIAL / PRODUCT	TEXTURE / FINISH	NO. OF ITEMS	Product: Supplier:	PRODUCT / SUPPLIER (OR APPROVED EQUIVALENT)
			N/A	N/A	PC Item		
5.2	Cessnock City Event Site	N/A	N/A	N/A	PC Item		

Materials Schedule - Street Typologies

[illegible]

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Materials Schedule - Street Typologies

REF	ITEM	IMAGE	DATUM/PRODUCT	TEXTURE/FINISH	NO. OF ITEMS	PRODUCT/SUPPLIER
5.3	Landscaping Street Tree Planting	N/A	100x2 Trees Plant on both sides of street at one end of block	N/A	Product: Supplier:	N/A
5.4	Landscaping Rain Garden Planting	N/A	20cm x 20cm Plant on both sides of street adjacent road curb	N/A	Product: Supplier:	N/A
6.0 LANEWAYS						
NOTE: Refer Fig 42: Street Typologies - Hierarchy of Public Domain Plan Document Refer Fig 54: Typical Laneway Section of Public Domain Plan Document						
6.1	Pavement Type 1 Coloured Interlocking Bricks		Colour: Colour: coloured concrete NEW	Brick finish permeable edge	N/A	Product: Supplier: Boral Boral www.boral.com.au
6.2	Pavement Type 2 Joint paving		Concrete paving 400x400x60mm	Brick	N/A	Product: Supplier: Boral Boral www.boral.com.au
6.3	Pavement Type 3 Movable bricks		Brick	Brick	8 / Laneway = 48 Total	Product: Supplier: Boral Boral www.boral.com.au
6.4	Art Installation Sculpture Piece		Brick	Brick	1 / Laneway = 6 Total	Product: Supplier: Boral Boral www.boral.com.au
6.5	Art Installation Wall Art		Brick	Brick	1 / Laneway = 6 Total	Product: Supplier: Boral Boral www.boral.com.au
6.6	Lighting Type 1 Recessed Lighting		Brick	Brick	N/A	Product: Supplier: Boral Boral www.boral.com.au

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Appendix 3 - Wayfinding Signage / Information Kiosks

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Wayfinding Signage / Information Kiosks

Information Kiosks

Information Kiosks are located at the following locations:

1. Council Administration Building;
2. Library;
3. City Heart; and
4. Cessnock Performing Arts Centre (CPAC)

Design

Kiosk Design would be a proprietary product similar to the following kiosks by In Touch Screens.

Kiosks could also be integrated into the facade of a building to reduce the probability of vandalism.

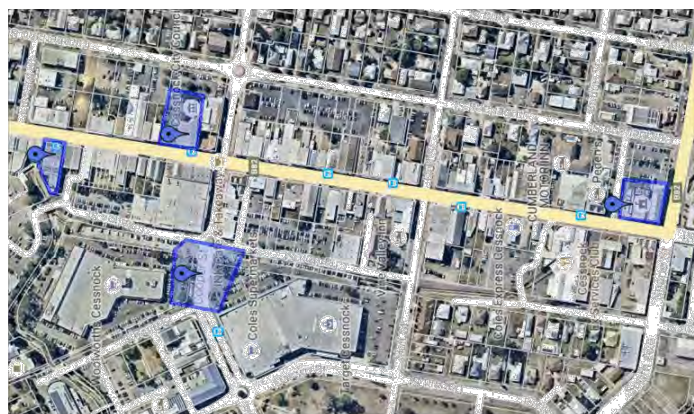
Information

Information communicated at each Kiosk would include:

- A map of the Cessnock Commercial Precinct containing the following information:
 - Street Names;
 - Shop Names / Location;
 - Shop Types / Location;
 - Pedestrian Network;
 - Pocket Parks / Seating / Plazas / Amenities, etc.
- A map of the Vineyards District containing the following information:
 - Street Names;
 - Vineyard Names / Location;
 - Vineyard specific information - Red or White / Cellar Door / Chocolate, etc.;
 - Events - Jazz in the Vines, etc.;
 - Public Amenities

Internet Site

A dedicated webpage to wayfinding in the Cessnock Commercial Precinct and Vineyards District that is kept up to date. The webpage would also allow people to search for shops, services, companies listed in the designated precincts that would be mobile phone friendly.



42" Commercial Grade Touch Screen

High clarity 1920 x 1080 full HD resolutions, energy efficient LED display has a wide viewing angle and multi-touch gesture support (pinch, zoom and pan).

40 Degree Viewing Angle

Developed in collaboration with industry experts, the kiosk is considered 40 degree angle of the screen delivers the most ergonomic and comfortable user experience. The kiosk is designed to be viewed from a wide range of angles, ensuring that the information is always accessible. This optimal angle also allows for digital signage messages to be viewed from a distance without image distortion.

Cleaning & Cooling

The elegant brushed aluminium trim and edge to edge glass combined with a robust powder-coated steel body allows for easy care and cleaning. The kiosk body allows for passive cooling with downward facing louvers on the rear for additional ventilation.

Australian Designed & Manufactured

Local manufacturing and support ensures full control of engineering and support for your kiosk roll-out and deployment.

Temper resistance

Anti-vandal recessing plates allow for free-standing operation or can be bolted down using guide holes. The kiosk is designed to be tamper resistant and is fitted from underneath or from the rear. Internal PC and components can be accessed through side-access panels which allow you to position the kiosk directly flat up against a wall, or in the centre of a room.

Custom branding

Kiosk body can be powder-coated in 100 colours with custom branding to suit your corporate branding. Guide lines. Optional printed signage can wrap the kiosk for a true personalised look-and-feel as well as an optional printed form acrylic overhead signage board (optional only).

TECHNICAL DETAILS

1920 x 1080 Full HD resolution
42" active viewable area
350cd/m2 brightness
3000:1 contrast ratio
6.5ms response time
178 degree viewing angle

Australian designed & built
Powder-coated steel body
Toughened glass front
Multi-touch gesture support
USB touch interface
Compatible with Win 7, 8 & 10



Steel plate kiosks are 480mm (1'6") x 480mm (1'6"). Kiosk frames with optional overhead signage board.



Cessnock Commercial Precinct Project

Milestone 5 - Background Reports Issue

Prepared for: Cessnock City Council
www.cmplus.com.au
16005 | March 2017

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Cessnock Commercial Precinct Project

Milestone 5 - Background Reports Issue

Cover Image: View along Cessnock Main Street Vincent Street to the hills beyond

Revision	Date	Description	By	Chk	App
01	21/04/16	Book up request	JPD	DA	
02	01/05/16	Book up request - Page 44 added	JPD	DA	
03	27/05/16	Final Draft - Chapter 12 (Public Domain Plan)	JPD	EC	DA
04	19/12/16	25-30 Public Domain Plan - 14/1/16	JPD	EC	
05	17/01/17	Milestone 5 - Background Reports Issue	JPD	DA	
06	18/02/17	Milestone 5 - Background Reports Issue	JPD	DA	

160005 | March 2017



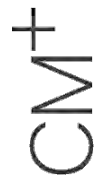
Prepared for: Cessnock City Council

By:

Conybeare Morrison International Pty Ltd.

Architecture + Master Planning + Urban Design | 52-58 William Street East Sydney NSW 2011
T +61 2 8534 8888 | info@cmi.com.au | www.cmi.com.au

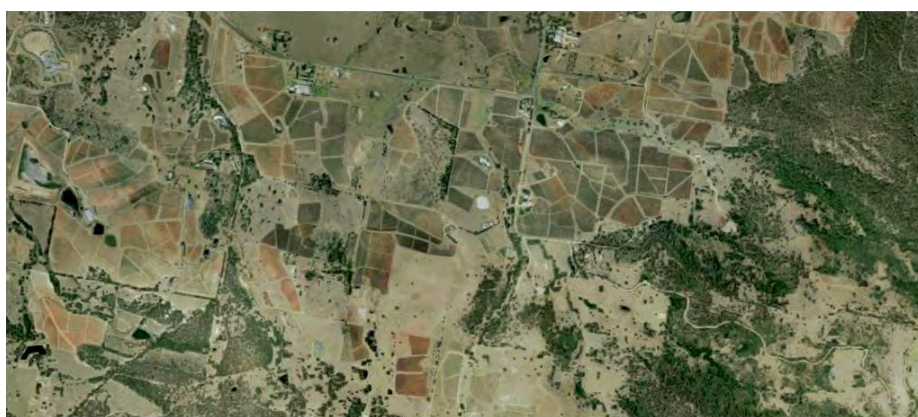
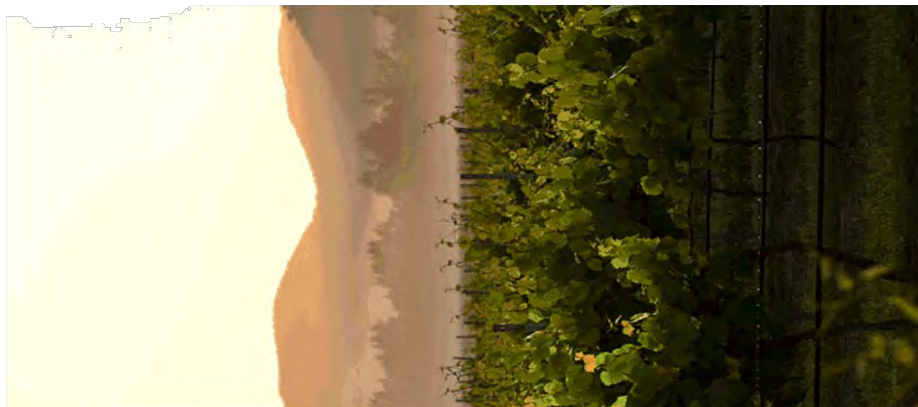
Cessnock Commercial Precinct Project | March 2017



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Appendices



March 2017 | Cessnock Commercial Precinct Project

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1.0 Introduction

1.1 The Project

'This Project aims to complement the Lower Hunter Regional Strategy in its vision for the city of Cessnock as a Major Regional Centre through the establishment of a site specific Development Control Plan and Public Domain Plan. In addition, the project aims to complete actions identified in the Cessnock CBD Masterplan, develop guidelines to improve the public domain of the Cessnock Commercial Precinct, and provide a document that will form the basis of a Developer Contributions Plan for public domain improvements.'

(Extract from Cessnock Commercial Precinct Project RFO, Section 2 – The Brief)

The three primary project deliverables are:

- Development Control Plan (DCP)
- Public Domain Plan (PDP)
- Implementation Plan (IP)

The project included a review of the CBD Masterplan prepared by Arup in 2012 and has developed the statutory framework (recommended changes to the current CLEP and specific Cessnock Commercial Precinct CDCP provisions) and a costed and prioritised concept design for the Commercial Precinct public domain. The outcomes of this urban planning work will provide a sound basis for the next implementation phase – the staged delivery of upgrades works over the next 25 years.

1.2 Study Area

The Cessnock Commercial Precinct study area is 110.6Ha in size and is generally defined by lands with a business zoning (refer to Figure 1).

1.3 Project Objectives

The project has the following Objectives:

- Develop a Development Control Plan and Public Domain Plan that will assist in establishing the Cessnock commercial precinct as a Major Regional Centre;
- Develop a set of achievable development incentives that will encourage good design within the Cessnock Commercial Precinct;
- Complete actions identified in the Cessnock CBD Masterplan;
- Ensure an adequate supply of housing (and appropriate mix of housing typologies) and employment land within the Cessnock Commercial Precinct for the next 25 years;
- Provide the base information that will inform future Contributions Plan funding improvements to the public domain of the Cessnock Commercial Precinct;
- Develop an Implementation Plan that:
 - Provides recommendations to improve CLEP 2011;
 - Schedules public domain and infrastructure improvements, identifying staging, priority, funding and contributions, timing and cost; and
- Develop and implement a Community Engagement Strategy that effectively engages the community of the Cessnock Commercial Precinct.



Figure 1: Study Area

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1.0 Introduction

1.4 Study Team

The Conybeare Morrison (CM⁺) led Consultant team brings together significant urban planning experience, with specialist expertise in the following areas:

- Project Management
- Urban Design
- Stakeholder + Community Consultation
- Statutory Planning
- Development Economics
- Landscape Design
- Quantity Surveying
- Transport Planning
- 3D Visualisation

The project organisational structure is illustrated in Figure 2.

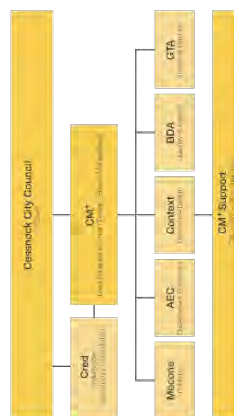


Figure 2: Project Team Structure.

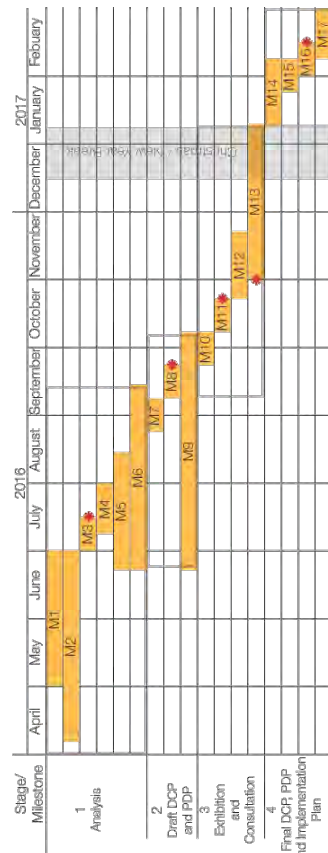
1.5 Study Methodology and Program

The CM⁺ consultant team commenced work on the project in April 2016 and will complete the project over an approximately one year timeline. The project program and milestones are illustrated in Figure 2. The following four stage methodology outlines the overall approach:

1. Undertake urban analysis;
2. Prepare draft:
 - a. Development Control Plan (DCP)
 - b. Public Domain Plan (PDP)
 - c. Implementation Plan (IP);
3. Undertake consultation and place on public exhibition; and
4. Finalise DCP, PDP and IP.

Stakeholder and community consultation lead by Cred Consultants is an important component of the project – informing the urban and landscape design at key intervals through the program.

The program incorporates workshops with the community and with Council planning officers, presentations to Cessnock City Council and a public exhibition of the draft plans to allow an opportunity for community and stakeholder comment and feedback.



- Milestones 1: Draft Supply and Demand Analysis, Draft Community Engagement Strategy
- Milestones 2: Draft DCP Workshop Presentation
- Milestones 3: Workshop
- Milestones 4: Final Supply and Demand Analysis, Community Engagement Strategy
- Milestones 5: Draft Background Reports
- Milestones 6: Draft DCP and PDP
- Milestones 7: Prepare Draft Workshop Presentation
- Milestones 8: Present to Council Staff
- Milestones 9: Draft Implementation Plan, final Background Reports, and Draft DCP and PDP
- Milestones 10: Prepare Draft DCP, PDP and Implementation Plan presentation
- Milestones 11: Present to Councilors
- Milestones 12: Exhibition Draft DCP, PDP and Implementation Plan
- Milestones 13: Community Consultation and Public Exhibition
- Milestones 14: Post Exhibition Draft DCP, PDP and Implementation Plan
- Milestones 15: Prepare Draft DCP, PDP and Implementation Plan presentation
- Milestones 16: Present to Councilors, Council Staff
- Milestones 17: Final DCP, PDP and Implementation Plan

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2.0 Planning Context

2.1 Regional Context

The Lower Hunter is the sixth largest urban area in Australia. It is located 160 kilometres north of Sydney, covers an area of 4,291 square kilometres and is a major population centre that includes the regional city of Newcastle, the State's second largest urban centre.

The Lower Hunter is a region of diverse landscapes and land uses. Its coastline, mountains, lakes, floodplains and rivers are key elements of the Region's identity, culture and lifestyle.

The Lower Hunter is characterised by large areas of rural, agricultural and forested lands. Based on a major river and coastal estuary, it contains areas of high quality agricultural land, important drinking water aquifers and internationally and nationally significant environmental assets. The region's rural and semi rural landscapes account for around 80 per cent of the Region's land, and are themselves further characterised by key industries of mining, wine production and tourism.

Cessnock is known as the 'gateway' to the Hunter Wine region which is located immediately to the northwest of the Centre (refer to Figure 4).

2.2 Local Context

Cessnock is a designated Major Regional Centre - identified in the Lower Hunter Regional Strategy prepared by the Department of Planning and Environment (DPE) in 2008. The strategy identifies significant population, dwelling and employment growth for the Lower Hunter projected through to 2031. However, growth in the Cessnock LGA and in the Commercial Centre is expected to be slower and more gradual. The subsequent Draft Hunter Regional Plan released by the DPE in November 2015 confirms this overall growth scenario.

The opening of the Hunter Expressway in March 2014 provided an attractive north-south route, removing through-traffic, including heavy vehicular traffic from the Cessnock 'Main Street' - Vincent Street.

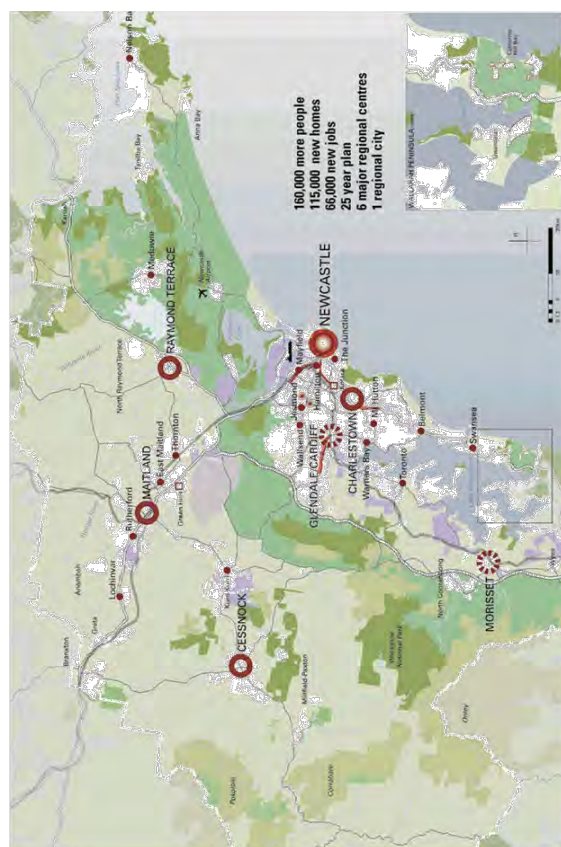


Figure 3: Lower Hunter Regional Strategy Map

Figure 3: Lower Hunter Regional Strategy Map

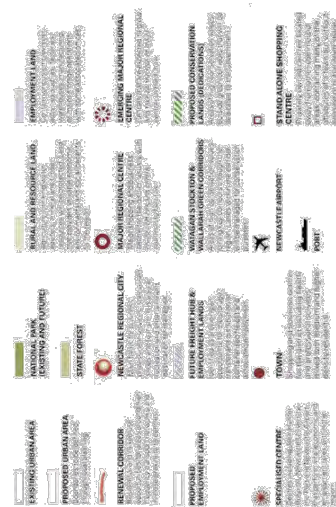
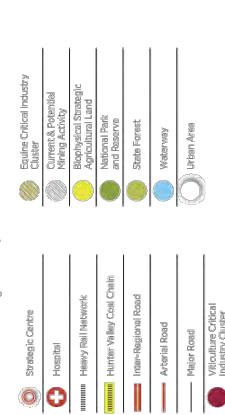


Figure 4: Western Hunter Regional Map

Figure 4: Western Hunter Regional Map



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2.0 Planning Context

2.3 Current Development Controls

A review was undertaken of the key development controls that currently apply to the Cessnock Commercial Precinct, and the findings are summarised below.

Land Zoning

Land Zones applying to the study area are:

- B3 Commercial Core
- B4 Mixed Use
- B7 Business Park
- RU2 Rural Landscape
- R3 Medium Density Residential
- RE1 Public Recreation

The Commercial Core (primarily retail) is concentrated in the north of the study area, with the remainder of the Centre Zoned Mixed Use. Medium Density Residential Zones surround the Centre, to the west, north and east, whilst Business Park and Rural Landscape zones occur to the south of the Centre. Refer to Figure 5.

Floor Space Ratio (FSR)

An FSR of 1.5 : 1 applies to the core of the Commercial Precinct, whilst the rest of the Centre is not subject to density controls. Refer to Figure 6.

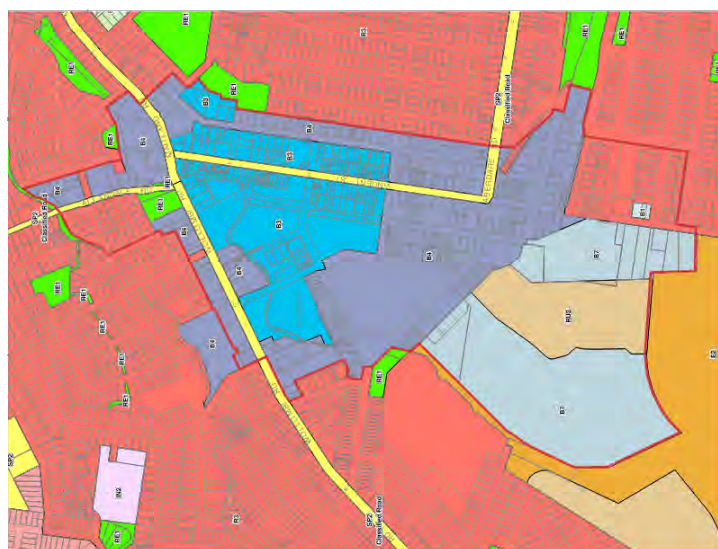


Figure 5: Zoning Map

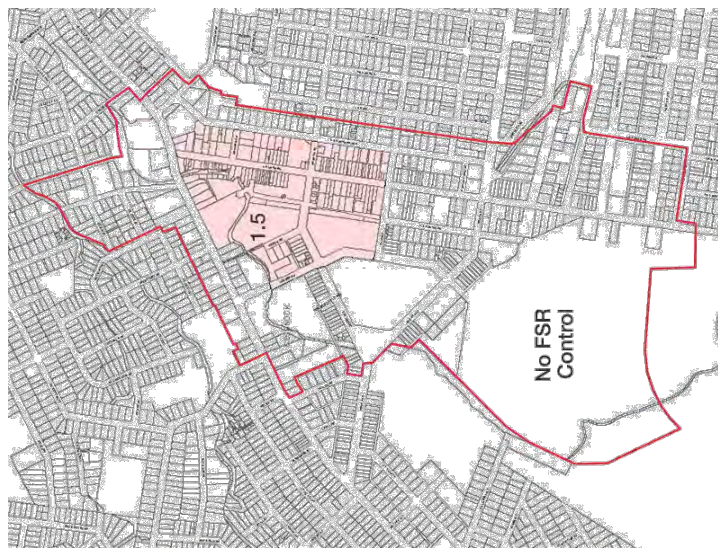


Figure 6: Floor Space Ratio Map

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2.0 Planning Context

Height of Building (HOB)

A 12 metre height limit applies to three areas of the Commercial Precinct, refer to Figure 7; the Commercial Core and to the Business Park area in the south.

Allowing for a larger ground floor height, and for conventional roof structures, this height limit permits mixed-use buildings of three storey height in the Commercial Core, or in the Business Park, a two storey commercial, or large scale 'big box' retail development.

Heritage Items

Cessnock CBD's most distinctive feature is the significant number of historic buildings which contribute a distinct identity and local character. Refer to Figure 8.

There are 29 Local Heritage Items and one State Heritage Item in the study area. Most of the heritage buildings are located along Vincent Street, however, there is also large areas to the South that were former mining areas, and the former South Maitland Railway, a landscape heritage item, which is listed as an item of State Heritage.



Figure 8: Heritage Item - Royal Oak Hotel

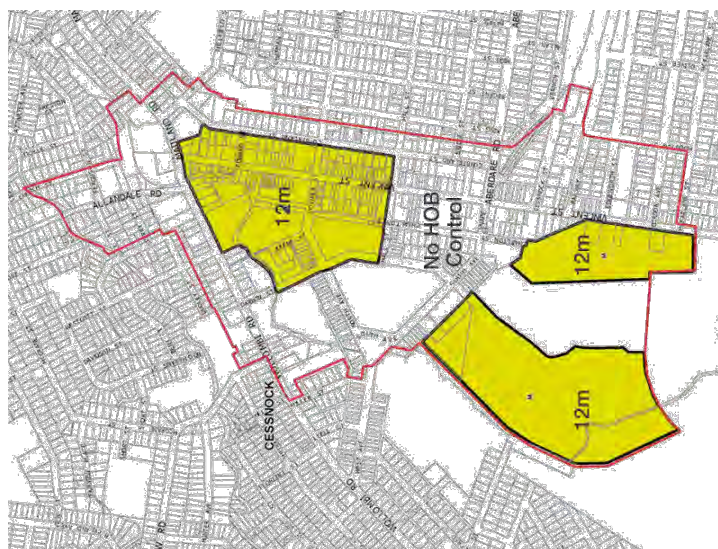


Figure 7: Height of Building Map

Maximum Building Height (m)
12



Figure 8: Heritage Map

Heritage
Item - General
Item - Acknowledged
Item - Landmark

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2.0 Planning Context

2.4 Precinct Constraints

Mine Subsidence

The Commercial Precinct has historically been subject to extensive sub-surface mining, including a zone of shallow underground workings that crosses the Precinct from the southwest, through to the northeast.

Recent flood events have seen 'sink holes' of significant size open up to the south of the Centre, in the Business Park. Refer to Figure 10.

Flood Risks

The Cessnock Commercial Precinct is located within the Black Creek Catchment area. Two tributaries of Black Creek drain from south of Cessnock, through the Centre, merging in the north, to form a single larger drainage line that continues northeast. Given the numerous creeks converging in Cessnock, flooding has occurred regularly including in 1949, 1977, 1980, 1982 and 2007.

Much of the Centre to the west of Vincent Street is subject to 1 in 100 years flood events. Drainage is addressed in the Centre with large scale open concrete canals or subsurface culverts. Refer to Figure 11.



Figure 10: Mining Subsidence Map

Shallow Underground
Working
MSB_DMP_CASS
Tombain Mine
Subsidence District
Great Coal Measures
Previous Coal Measures
DCP area

Figure 11: Flooding Map

100 year flood line
Creeks, channels
Green public space
Other open space
Contour - 2m
DCP area

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2.0 Planning Context

2.5 Property Ownership

Most of the lands in the Cessnock Commercial Precinct are privately owned. In the Commercial Core, several large shopping centres, occupy the largest private land parcels, they are Cessnock Plaza Shopping Centre, Coles Supermarket, Big W, ALDI Supermarket, IGA Supermarket, Cessnock Leagues Club and the Supercheap Auto store.

To the rear of the Vincent Street city blocks there are significant parcels of land, owned by council and utilised for public car parks. A large triangular block is situated in the west of the CBD and is owned by Department of Technical Education.



Figure 13: Cessnock City Centre Shopping Centre



Figure 14: Cessnock Plaza Shopping Centre



Figure 15: Cessnock TAFE Park





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3.0 Review of CBD Masterplan

3.1 Introduction

This chapter provides a review of the Cessnock CBD Masterplan and summarises the commitments made to improve the public domain of the Cessnock Commercial Precinct.

3.2 Masterplan Vision: 'Attractive / Thriving / Welcoming'

The CBD masterplan identified the important role Cessnock plays as the focus of civic, health, education, cultural life and local identity for people in the Hunter Region. The masterplan strategy outlines the path towards a revitalised CBD with improved liveability, aesthetics and increased opportunities for investment.

The Cessnock City community Plan 2020 identified the following 'vision' for Cessnock:

'Cessnock will be a cohesive and welcoming community living in an attractive and sustainable rural environment with a diversity of business and employment opportunities supported by accessible infrastructure and services which effectively meet community need.'

The Masterplan developed a suite of inter-related Objectives, summarised as:

Attractive

- A place that celebrates its history
- A safe environment
- A healthy place to live and work

Thriving

- An inclusive CBD for everyone
- A vibrant main street
- A CBD built on its strengths

Welcoming

- A strong community heart
- A connected and accessible place
- A friendly destination

3.3 Review of Spatial Initiatives

The Masterplan recommends providing better urban infrastructure, including:

- Completion of a number of existing city streets;
- Key road realignments;
- Improving pedestrian links;
- Beautification of drainage channels;
- Strengthening of the CBD 'gateways';
- Strengthening cultural and civic institutions;
- Enhancing 'Main Street' and city parks; and
- Optimising public car parking facilities.

At a social level, better connections and more public spaces for both locals and visitors in the centre of town are proposed.



Figure 17: Artist's impression of the potential future look and feel of Vincent Street



Figure 18: Artist's impression of the potential future look and feel of TAFE parklands

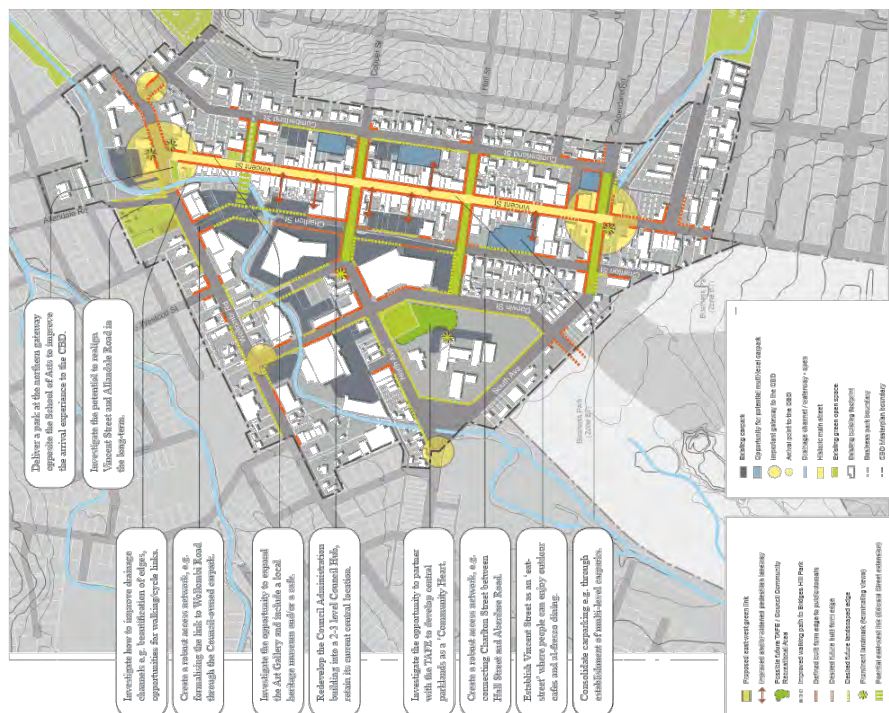


Figure 16: Key Spatial Initiatives Summary

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3.0 Review of CBD Masterplan

3.4 Review of Access and Movement

The Masterplan 'Access and Movement Framework' seeks to improve the CBD's pedestrian environment, and general CBD accessibility, connectivity and safety.

Proposed initiatives include:

- New pedestrian crossings;
- Upgrade of the laneways to public carparks;
- East-west green links;
- New street links; and
- Consolidated public car parking (multi-level).

The upgrade of Vincent Street in 2004 has greatly improved accessibility in Cessnock. This was achieved by upgrading paving, landscaping, lighting and street furniture. However, west of 'Main Street' the urban fabric falls away and streets and spaces are ill defined, lack amenity and feel unsafe. The opening of the Hunter Expressway reduces traffic flow along Vincent Street, offering opportunities to improve pedestrian amenity in the CBD 'Main Street'.

3.5 Review of Public Domain Framework

The Masterplan 'Public Domain Framework' seeks to improve the CBD pedestrian safety, legibility and overall street level experience. The CBD Masterplan aspires to:

"A public domain that provides places where children can play, friends can meet for a coffee and workers can have lunch will signal that Cessnock is lively, buzzing, successful - in short: a place desirable to be at."

Proposed initiatives include:

- Extension of the low speed environment to include key east-west streets;
- New and upgraded public spaces and recreational parks;
- Highlighting the CBD 'gateways'; and
- A comprehensive signage strategy.



Figure 18: Access + Movement Framework

- Major road
- Local road
- Proposed pedestrian link to car park
- Improved pedestrian crossing
- Existing pedestrian crossing
- Proposed additional pedestrian crossing
- Proposed new street link
- Proposed east-west green link
- Potential east-west link (downed street extension)
- Existing car park (Council-owned)
- Existing car park (privately owned)
- Opportunity for potential multi-level car park
- Existing bus stop
- Signalled intersection
- Important gateway to the CBD
- Existing green open space
- Business park boundary
- Study site boundary



Figure 20: Public Domain Framework

- Proposed closed-road environment (in addition to Vincent Street)
- Important public / civic space in the CBD
- Proposed improvement of H.J. Stomack Linear Park
- Public Art / WPE / Council Community Recreational Area
- Proposed east-west green link
- Potential east-west link (downed street extension)
- Proposed location of CBD-related signage
- Important gateway to the CBD
- Channel / waterway
- Topography contours (5m)
- Existing green open space
- Business park boundary
- Study site boundary

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3.0 Review of CBD Masterplan

3.6 Review of Urban form + Aesthetics Framework

The Masterplan 'Urban Form + Aesthetics Framework' seeks to improve the visual experience of the CBD, to enhance the aesthetic appeal, legibility and overall urban experience.

Proposed initiatives include:

- Respect and renew historic buildings to the CBD, to strengthen the unique local character;
- Frame vistas to the natural surroundings e.g. local hills;
- Enhance street views to key CBD buildings and city corners;
- Strengthen landmarks and prominent reference points e.g. prominent buildings;
- Highlight important cultural and community facilities; and
- Introduce new green links and green spaces within the CBD.

3.7 Review of Economic Opportunities Framework

The Masterplan 'Economic Opportunities Framework' seeks to improve the collective economic outcomes for all CBD stakeholders.

Proposed initiatives include:

- Exploring opportunities to increase density along the 'Main Street' commercial spine;
- Promote weekend markets and events that will draw new visitors to the CBD;
- Continue to improve the appearance and amenity of the historic 'Main Street'; and
- Invest in cultural, civic and community facilities as drawcards and new visitor attractions.

3.8 Implementation Plan Commitments

The Implementation Plan identified ten public domain 'Initiatives/Actions' and assigned an 'order of cost', priority, delivery timeframe and responsible stakeholder/s.

The ten initiatives are summarised:

1. TAFE Central Parklands + Community Gardens.
2. Vincent Street at-fresco 'Eat Street'.
3. Upgrade Open Spaces e.g. H J Sternbeck Park
4. Beautify Open Drainage Channels.
5. Permeable Urban Fabric/Consolidate Car Parking/Attract Residents to the CBD/Promote Night-time Economy.
6. Improve Lighting Throughout the CBD.
7. Encourage People to Live + Visit the CBD.
8. Public Art Strategy.
9. Signage + Interpretive Strategy.
10. Upgrade Street Furniture 'Brand'

All of these measures have been reviewed and the majority have been integrated into the preparation of the DCP and PDP documents.



Figure 21: Urban Form + Aesthetics Framework

- Defined built form edge to public domain
- Desired future built form edge
- Desired future setback edge
- Prominent corners with high visibility
- Prominent landmarks (surrounding streets)
- Classified Views
- View Shed
- Proposed east-west green link
- Potential east-west link
- Responsible TAFE / Council Community
- Residential Area

- Heritage listed building / item
- Important cultural / community facility
- Potential location of CBD visitor centre
- Important gateway to the CBD
- Heritage street corner
- Existing building footprint
- Existing green open space
- Business park boundary
- Study area boundary

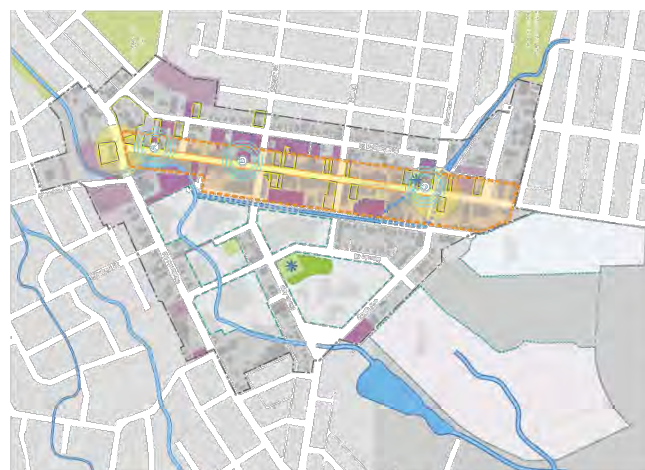


Figure 22: Economic Opportunities Framework

- Investigates area for potential increase in development density (e.g. increased family units, e.g. increased density)
- Heritage listed building / item
- Important cultural / community facility
- Potential location of CBD visitor centre
- Important gateway to the CBD
- Heritage street corner
- Existing building footprint
- Existing green open space
- Business park boundary
- Study area boundary



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4.0 Development Options Review

A Development Options Review (DOR) has been undertaken for the Cessnock Commercial Precinct by the AEC Group (AEC). The following is a summary of the DOR study, including identifying the key findings and conclusions, and AEC's recommendations for the future economic planning of the Cessnock Commercial Precinct.

4.1 Background

The precursor to this study, the Supply and Demand Analysis sought to understand the nature of demand for residential and employment floor space in the Precinct, particularly the socio-economic and socio-demographic trends that influence this demand. The analysis additionally examined supply conditions, and if current planning controls facilitate feasible development of required building types and floor space.

The study reviews catalyst development sites identified in the Indicative Layout Plan and explores if they are likely to be financially feasible to develop, or if alternate controls may be required.

4.2 Key Observations

A number of observations emerge from the review of select sites.

- Modest End Sale Values of Residential Units**
Overall, the relatively modest end sale value of residential units constrains the financial feasibility of higher density development. The Cessnock market for higher density living is less established, with households generally displaying a preference for medium density product (e.g. villas, townhouses and duplexes) over higher density product. This market trend is likely to prevail until such time that the benefits of city-living (in the City Centre) result in higher pricing levels for higher density residential product.

- Cost of Site Consolidation**
The financial feasibility of higher density living is additionally constrained by the relatively high cost to consolidate a site in the Commercial Precinct. In instances where a vacant site is able to be consolidated, a lower density threshold is required for feasible development to occur. Where sites are improved (with buildings), a higher FSR is generally required to displace existing uses.

For non-residential uses, end sale values are generally lower than residential uses. As such, development of non-residential uses is likely to occur on vacant sites or comprise refurbishment/extension of existing space.

- Basement Parking v Deck Parking**
In the case of Council-owned car park sites, if a redevelopment was required to at least replace the car spaces in a basement structure, a relatively high FSR of 4:1 is conceivably required. Should the car spaces be able to be incorporated in a multi-deck structure, a lower FSR of around 2:1 could be sufficient to cross-subsidise delivery of those car spaces. At current pricing levels of residential units, basement car parking is not feasible.

4.3 Conclusion and Recommendations

4.3.1 Observations and Influencing Factors Development and Market Activity

Residential development is observed to be responding incrementally to market need as the population of Cessnock grows. Residential flat buildings and shop top housing are not observed to be widespread. Rather, medium density product (villas, townhouses, duplexes) are observed to be the most dominant

development type in the broader Cessnock area, owing not only to market preference but also the availability of cheaper consolidation opportunities in residential zones.

The broad area of Cessnock generally accommodates a lower socio-economic profile and residential product is accordingly priced. Furthermore, the market for higher density living can be considered to be in its infancy, the current level of pricing not as yet at a level that would incentivise large scale development of higher density product in the Precinct.

In order for a lift in the Cessnock Commercial Precinct's profile and commensurate increase in prices paid for completed residential product, an improvement in the overall amenity of city-living is required. Whilst yet to be witnessed, development in the Precinct and progress of Council's public domain plan will help improve the overall amenity and desirability of the area.

Development and market activity can be observed to be reflective of these market dynamics:

- Development to higher density in the Precinct has only occurred where a site is able to be consolidated for \$500/sqm of site area, or less.
- Lower density development (low rise residential and medium density) observed outside the Precinct, where vacant sites in residential zones are able to be consolidated.
- The principle of substitution can be observed to be working in a number sub-markets. The price of existing detached houses generally forms a ceiling for prices able to be achieved by higher density product. For example, if a three bedroom detached house is available outside the Precinct for \$300,000, there is low prospect of a unit selling for a price in the same order of magnitude. For this reason, the price threshold of new product in some scenarios is below the economic price that is required for feasible development.

Industry	Supply (2016 sqm)	Demand (2016-2041 sqm)
Mining	-	3,209
Construction	548	6,622
Manufacturing	-	8,617
Accommodation and Food Services	19,779	10,793
Retail Trade	65,460	34,353
Health Care and Social Assistance	327	24,485
Education and Training	7,029	8,401
Public Administration and Safety	2,838	1,931
Rental, Hiring and Real Estate Services	12,433	9,036
Other Services	4,451	2,526
Financial and Insurance Services	1,515	5,290
Administrative and Support Services	3,432	2,835
Professional, Scientific and Technical Services	5,968	16,979
Transport, Postal and Warehousing	1,816	3,400
Wholesale Trade	14,969	878
Arts and Recreation Services	-	3,438
Agriculture, Forestry and Fishing	2,066	1,968
Information Media and Telecommunications	-	1,670
Electricity, Gas, Water and Waste Services	149,226	155,763
Sub-total	594	157,106
Not Specified and Unknown	22,140	157,106
Total (excluding Residential)	171,960	157,106

Table 1: Employment Floor Supply v Demand, Cessnock City Centre (2011-2041)

Category of Use	2011	2016	2021	2026	2031	2036	2041	Change (2016-41)
Commercial Premises								
Retail	47,113	49,564	52,691	55,791	58,770	60,913	63,001	13,437
Office	16,851	16,324	16,634	17,409	18,224	18,932	19,698	3,374
Business	34,862	35,405	37,475	39,591	41,326	42,122	42,692	7,287
Tourist and Visitor Accommodation	3,624	3,886	4,151	4,433	4,722	5,037	5,377	1,491
Total	102,449	105,179	110,951	117,224	123,042	127,004	130,768	25,589
Retail Premises	47,113	49,564	52,691	55,791	58,770	60,913	63,001	13,437
Shops	24,950	25,170	27,845	29,555	31,167	32,320	33,307	7,137
Food and Drink	7,169	7,688	8,214	8,771	9,342	9,966	10,638	2,990
Bulky Goods	14,994	15,706	16,632	17,465	18,261	18,626	19,066	3,560

Source: BRS (2016), AEC

Table 2: Employment Floorspace Projections for Specified Categories, Cessnock City Centre (2011-2041)

Dwelling Type	2011	2016	2021	2026	2031	2036	2041	Change (2016-41)
Detached dwellings	1,759	1,842	1,928	2,003	2,081	2,124	2,160	318
Medium density dwellings*	238	261	286	310	335	355	377	116
Flats/units (and shop top housing)	-	4	9	14	19	25	30	26
Other	18	19	20	21	22	22	23	4
Total	2,015	2,126	2,243	2,348	2,457	2,527	2,590	464

*Including dual occupancies, multi-dwelling housing

Table 3: Distributed Dwelling Projections, Cessnock City Centre (2011-2041)

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4.0 Development Options Review

<p>New retail and commercial development is observed to be minimal, not surprisingly given the relative demand v supply situation, where supply presently outstrips demand. Until market rents are at an economic level, in many instances there is little incentive for landowners of strip retail to reinvest in their shopfronts. A medical centre proposed at 2/5 Vincent Street takes advantage of the availability of vacant land in the B7 Business Park zone.</p> <p>Existing Uses and Hierarchy of Land Uses</p> <p>Some sites (particularly vacant sites) have better prospects from a financial feasibility perspective if they are in single ownership, i.e. no premium payment is necessary to incentivise numerous landowners to sell. The reality of brownfield or mill development though (and particularly in centres), is that sites are typically small in size and owned by a number of parties, necessitating payment over and above market value.</p> <p>Landowner expectations generally shift with planning controls, i.e. higher expectations accompany higher permissible densities and higher order uses. As an example, B7 Business Park land that permits a wider variety of uses (and potentially a more intensive use of land) is valued higher by the market than IN1 and IN2 zoned land. Industrial uses do not generally respond to density, typically incorporating relatively low densities, after allowing for loading and vehicle circulation space.</p> <p>Land use and intensity of use generally underpins land values. This is due to the different and sale values achievable when sites are redeveloped. A relationship between permitted uses and land values can be observed, all things being equal. This is also referred to as the 'hierarchy of land values'. The following illustrates:</p> <ul style="list-style-type: none"> • Vacant industrial sites (zoned IN1 or IN2) in the Cessnock/Kurti site area. • By contrast, vacant business park sites (zoned B7) generally achieve \$75/sqm to \$125/sqm of site area. Under the Cessnock LEP the Business Park zone accommodates a greater diversity of land use and development typologies, compared to industrial zones and is accordingly considered more valuable by the market. <p>Where there is vacant land available and at lower prices compared to improved sites, the market will understandably gravitate to acquiring more cost-effective development opportunities. For this reason, until such time that vacant land within the Cessnock</p>	<p>Business Park (B7 zone) is absorbed, it is unlikely that sites along South Avenue will be taken-up for development opportunities in a B6 zone.</p> <p>Development Activity and Take-up</p> <p>Development take-up is subject to demand-side and supply-side factors. Development activity in Cessnock is generally characterised by residential product and small and medium size developments (<60 dwellings). This reflects the developer profile that is active in Cessnock - smaller builder/developers who are based locally. This is also reflective of the pace of market demand for new residential product. As identified earlier there is minimal development of non-residential uses.</p> <p>The direct relationship between population growth and housing need requires no explanation. In decided where and what they will be accommodated in, households will have regard to factors including cost, location and convenience to their place of work by selecting accommodation of the type and quality within their financial capability.</p> <p>It is effective demand that underpins development activity, developers responding to the willingness and ability of households to pay for new accommodation. Given the comparably lower income profile of households in the Cessnock area, the financial capability of households to pay for housing is fragile.</p> <p>The feasibility of development in Cessnock is generally positive outside the Commercial Precinct where existing uses are cheaper to assemble. Many retail/commercial properties that are well positioned in the core of the Commercial Precinct are at present too valuable to be redeveloped. Development outside the Precinct is expected to endure as developers assemble vacant and dilapidated sites.</p> <p>While poor feasibility at lower densities would appear to suggest that higher densities are required to displace existing uses in the Precinct, higher density development is not without its challenges, particularly as end sale prices of units do not facilitate the provision of basement car parking.</p> <p>New development in Cessnock's Commercial Precinct (both residential and non-residential) is challenged from the perspective of costly site amalgamation and fragile end sale values of completed product. Exceptions to this observation are where a vacant site may have been procured (e.g. 38 Vincent Street), or where a site has been owned for a number of years and is</p>	<p>released from land-banking.</p> <p>4.3.2 Recommendations</p> <p>Market research and feasibility analyses suggest that higher density controls do not necessarily incentivise change on all sites. Vacant sites and sites with minimal buildings present the most immediate opportunities for redevelopment.</p> <p>While planning and development controls are intended at stimulating redevelopment and renewal in Cessnock, large scale development is difficult and challenged for a number of reasons. The availability of suitable sites is a major impediment to development in any established urban area. In some instances market cycles and factors (e.g. valuable existing uses, poor effective demand) are the reason for the lack of development activity, this beyond the control of planning authorities.</p> <p>Not all development will be feasible in the Commercial Precinct (particularly where existing uses are valuable or sites are fragmented in ownership). These are market issues, beyond the control of planning authorities.</p> <p>We recommend Council consider the following:</p> <ul style="list-style-type: none"> • Improve walkability within the Precinct Each of the retail sub-precincts within the Precinct are inward focused, leading to a lack of connectivity and synergy within the Precinct. Travel is very much car-based, there being much opportunity for pedestrian foot traffic to improve patronage levels in the Precinct even without increasing the number of visitors to the Precinct. • Invest in public domain and aesthetic presentation Outward presentation and impressions of an area are critical to business and consumer confidence. The investment by Council into the public domain (be it improved signage, street furniture and lighting, pedestrian amenity, etc.) will make the Precinct a more pleasant place for consumers to visit and linger and for businesses to invest. • Work with landowners keen to upgrade/refurbish their properties Anecdotal feedback from local real estate agents suggests some frustration by landowners during the DA assessment process. A smooth process that facilitates reinvestment into existing buildings will benefit renewal outcomes of the Precinct. 	<ul style="list-style-type: none"> • Facilitate provision of car parking at-grade or in a decked structure Owing to the costly provision of basement parking, the ability of developments to provide at-grade or deck parking will assist with financial feasibility. • Destination retailer to help anchor the cultural/arts hub in the south Given the sheer length of Vincent Street and in the interest of ensuring an integrated and connected retail strip, it would be important to attract a destination retailer that would serve as a retail/entertainment anchor for the cultural hub on Vincent Street at Abardere Road. Examples could include food and wine retailers, specialist food provider showcasing wine and cheese of the region, etc. <p>Aside from a select number of sites in the Precinct where higher density living is demonstrated to have market acceptance, it would be preferable for Council to work with developers to encourage continued infill development outside the Precinct. Incremental infill development (even if outside the Precinct) will in time bring about an increased number of residents, thereby generating demand for retail and non-retail services in the Commercial Precinct and thereby contributing to increased property rents and prices.</p> <p>Working in tandem with making improvements to the functionality and aesthetic presentation of the public domain and pedestrian connectivity within the Commercial Precinct, eventually the prices of medium and higher density residential product will then be able to justify and sustain their development.</p> <p>The growth and momentum of urban renewal is a self-perpetuating cycle - improving rents and prices will incentivise owners to re-invest in their properties or redevelop their properties, which in turn contributes to renewal of the Precinct.</p>
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5.0 Urban Form Analysis

5.1 Introduction

This urban form analysis is informed by the Cessnock CBD Masterplan and the Supply and Demand Analysis undertaken by the AEC Group. The Analysis explores precinct development at timelines of 2021, 2031 and 2041, and addresses planning, 3D built form and development yield for each scenario.

The urban form strategy has considered the protection of heritage buildings, key view corridors, development incentives to encourage best practice urban design, the pedestrian and cycle network of the Precinct, and the strategic location of public car parking in the CBD.

5.2 Supply and Demand Analysis Outcomes

The Supply and Demand Analysis and Development Options Review undertaken for the Cessnock Commercial Precinct by the AEC Group, projected that there would be modest growth in retail, office, business and tourist and visitor accommodation over the next twenty five years.

The resulting Precinct building footprints and urban form will progressively evolve over the period rather than radically transform.

Retail, Office/Business and Tourist/Visitor Accommodation

An audit of existing Retail Trading space in the Commercial Precinct identified approx. 65,000sqm available in 2016, whilst the demand for Retail Premises is only projected to reach approx. 63,000sqm in 2041. In terms of future supply, the current allocation (and general footprint) of retail floor space in the Precinct is adequate, and could theoretically be left unchanged into the foreseeable future.

On face value, retail will not be a development 'driver' for the Centre over the next 25 years – with no impact on the Precinct urban form. However, several factors could potentially alter this scenario:

- Tourism Driven Demand** - there is the potential to grow tourism within the Precinct (that does not rely on population growth) and thereby encourage private sector redevelopment of 'Main Street' sites (for café, restaurant and accommodation uses), and also 'drive' the development of cultural and community facilities, by the public sector, such as new art gallery, library facilities, and upgrade to Council's administration offices.
- Wines and Mine Visitor Centre** - the provision of a drawcard and architecturally striking facility that would attract visitors from Sydney, Newcastle and regional NSW, (recommended for the

former TAFE site) would bring back to life the west of the Precinct.

- Regional Shopping Centres** - the Commercial Precinct is an important and successful retail shopping centre hub serving the region. Consolidation, development and measured growth of these centres over the next 25 years could provide opportunities to improve the urban form of the west of the Precinct, linking the Vincent Street 'spine', west to the shopping centres, via the east-west Cooper Street axis. And also creating a new urban piece within the Centre - Cessnock Town Square, Bypass Route Development - the reconfiguring of properties due to the southern bypass and the new exposure properties would enjoy from passing vehicles, would likely trigger new development to emerge along the route i.e. South Avenue/Snape Street and within the west of the part of the Business Park.
- 'Main Street' Retail Focus** - the overall quantum of retail space is unlikely to change significantly, however the quality of the retail on offer is envisaged, and should be encouraged, to improve and develop over time. It is recommended, due to the extended length of Vincent Street, to focus street retail activity to the northern city blocks, and to strengthen connections west along Cooper Street to the Coles, Woolworths and IGA shopping centres.

The small projected increase in Office and Business floor space over the next 25 years can easily be accommodated in either the upgrade of existing 'Main Street' premises or in the development of new buildings on the currently vacant business park lands.

Residential Dwellings

Projections for new dwellings, once Detached Dwellings are excluded (that would occur outside of the study area), indicate a very small number (146) of new dwellings would be constructed in the Commercial Precinct over the next 25 years. New dwellings will consist of Medium Density residences and Apartments (incl. shop top housing). A small number of 'Main Street' consolidations may occur, whilst the former Cessnock Baths site and a smattering of other CBD sites will likely be developed.



Figure 23: Existing Built Form - View from North West



Figure 24: Existing Built Form - View from South East

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5.0 Urban Form Analysis

5.3 Commercial Precinct Urban Development

The preferred Commercial Precinct urban form has considered the strategic urban planning objectives for the Precinct, and has specifically addressed the following criteria:

View Corridors

The CBD Masterplan identified the key views and vistas to be considered in the planning of the Precinct. Refer to Figure 28. Key CBD street views include Vincent Street north to Cessnock Hotel, and west along Cooper Street and Hall Street. Also along Aberdare Road, west, to the Royal Oak Hotel.

Pedestrian + Bicycle Network

The Active Transport Strategy is illustrated in Figure 87, and includes key pedestrian connections and through-site links and the proposed cycleway network. The enhancement of the network of 'on' and 'off-road' cycleways and parking facilities is consistent with the Cessnock Bicycle Strategy (2016) and will prioritise pedestrian and bicycle access, connectivity and safety in the Commercial Precinct. The Precinct urban form supports this access network and the Public Domain Plan integrates cycleways into the proposed street cross sections.

Public Car Parking Areas

The Public Car Parking Strategy is illustrated in Figure 77, and includes six Council owned sites that are set back from the Vincent Street 'Main Street', yet still within short walking distance of the retail spine - accessed from side or parallel streets and by pedestrians via laneways. These public car parking areas are well situated to support the 'Main Street' retail whilst not taking up prime retail frontage. The two Charlton Street public car parks also support the Woolworths and Coles shopping centres.

Any future upgrade of the Coles and Woolworths shopping centre developments would require a rethinking of car parking arrangements. Opportunities exist for shopping centre redevelopment to establish active street frontages, to better define CBD streets, and to support Council's objective to reinforce the Cooper Street axis and to establish a new Town Square. The square would provide a new meeting place, social focus and venue for events, enriching the public domain experience.

In the future, multi-level car parking structures could be constructed on these sites if demand warrants. Multi-level car parking structures can, if not designed well, become eyesores when viewed from

city streets. It is recommended that the ground floor level of such structures that face street frontages, are activated with retail or community uses, to bring activity and passive surveillance to the city streets, and that the rest of the structure is well designed and incorporates landscape treatments, such as planters, green walls or contemporary artwork.



Figure 25: Mountain View West along Hall Street



Figure 26: Mountain View West along Cooper Street



Figure 27: View Corridor - Vincent Street North to Cessnock Hotel

Sources: Google



Figure 28: View Corridor Map
Sources: Cessnock CBD Masterplan Background Report 2012

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5.0 Urban Form Analysis

5.4 Urban Design Strategy

5.4.1 Existing Urban Form

The existing Precinct built form comprises a mix of building scales, setbacks and frontage treatments. The Precinct 'Main Street', Vincent Street is the most consistent and continuous urban streetscape in the Precinct, extending over four city blocks.

The urban definition of city streets and blocks diminishes as you progress down the city cross streets to the west and east of Vincent Street. And the built form is predominantly low scale detached villas in the residential streets in the north, south and western extents of the Precinct.

Whilst the predominant building height at the street frontage is one or two storeys, the overall height and scale of building varies significantly between the finer urban 'grain' of most Vincent Street buildings to the 'large box' architecture of the shopping centres located to the west and of the big box retail development e.g. Burnings Warehouse, located in the south of the Precinct.

The tallest buildings in the Precinct are the recently completed three storey mixed use development at the corner of Cooper Street and Darwin Street, the one storey shopping centres in the west of the Centre, the Burnings Warehouse development to the south and the higher, setback portion of the Cessnock Performing Arts Centre (CPAC) on the 'Main Street'.

Generally a low scale is maintained throughout the Precinct, allowing heritage buildings along Vincent Street to generally retain their prominence and integrity. With the exception of Vincent Street, there is a concern in regard to the poor definition of most city streets, particularly in the west of the Precinct, where many streets are exposed and poorly defined.

The 12m height limit has resulted in a reasonably uniform built expression and street scale for both heritage (generally 1-2 storey buildings of large floor-to-floor height) and more contemporary mixed use developments or large scale retail buildings (often 1 storey high with very large floor-to-floor heights). Along Vincent Street, and extending into some side streets, there is generally a consistent street wall height, often marked by older building parapet details.



Figure 30: Recently Mixed Use Development



Figure 31: Cessnock Performing Arts Centre

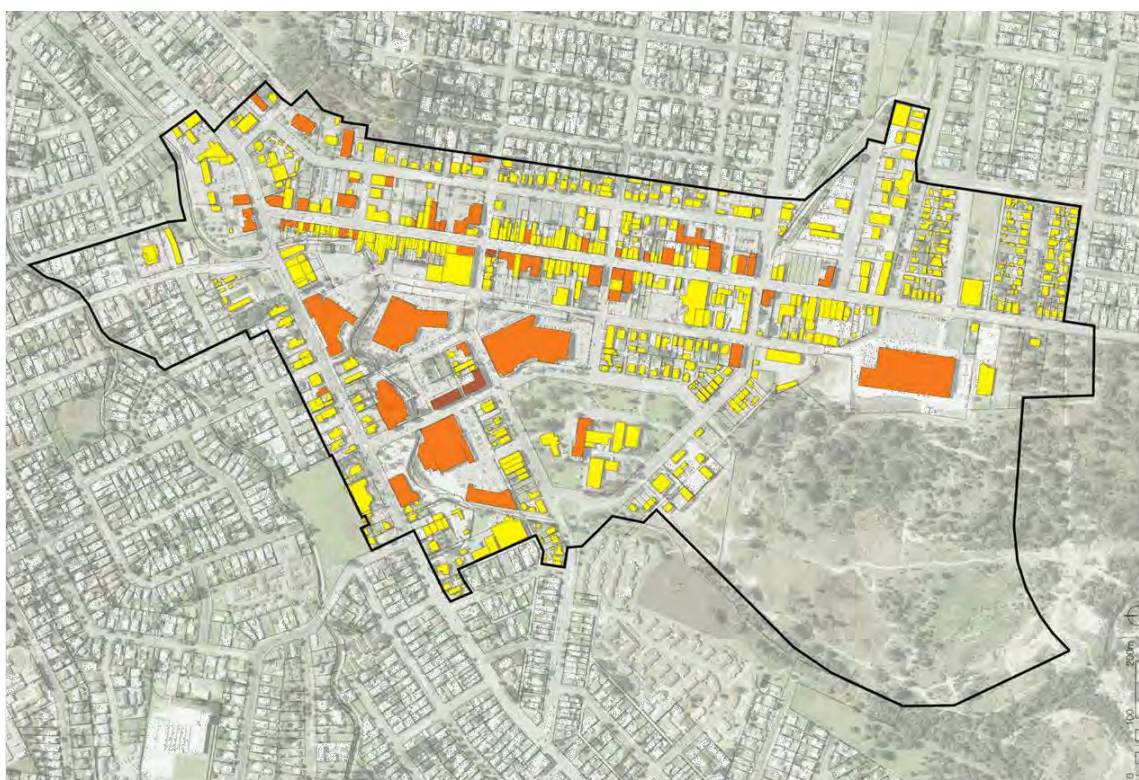


Figure 28: Existing Building Heights

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5.0 Urban Form Analysis

5.4.2 Recommended Urban Form Strategy

The focus will be to reinforce the well-defined Vincent Street 'Main Street' urban 'grain' and to focus development and street activity in the northern blocks of Vincent Street – between Hall Street and Melville Road.

Cooper Street

Cooper Street is identified as a key street to link the two retail precincts – the Vincent Street 'Main Street' retail and the cluster of shopping centre developments to the west. New development should define Cooper Street west of Vincent Street, as a desirable and active CBD street, linking west.

Town Square

The proposed Town Square would establish a central meeting place within Cessnock, providing an attractive destination for the community and for visitors travelling to the area. When combined with a strengthening of the Cooper Street east-west link, the Town Square has the potential to draw together the most successful retail elements of the Centre (the Main Street and the shopping centres) into a new integrated whole.

The urban form opportunity is to define the Town Square space in the urban fabric of the CBD through the modification and redevelopment of the existing shopping centre development frontages and car parking arrangements. All development that fronts the square should be activated with retail or similar activity generating uses.

Building Height

The low growth projections generally also result in a lack of demand for additional height in the CBD. Some interest by the owner of a vacant site to redevelop (between Vincent Street and Cumberland Street) and Council's future plans to potentially expand their office accommodation on their current site has been accommodated by a proposed increase in allowable building height (to a maximum of six storeys) and in conjunction with an increased FSR for this northern city block.

Heritage Character

Vincent Street heritage buildings are key definers of the existing street character, are generally one or two storey in height, and feature articulated facades and ornate parapets. Developments should respect adjoining heritage buildings, maintain an appropriate street scale, and reinforce the predominant street wall height, and parapet and awning lines.



Figure 32: Existing Cooper Street



Figure 33: Existing Cessnock City Centre

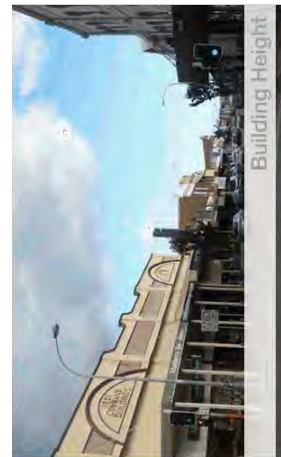


Figure 34: Existing intersection of Vincent Street and Cooper Street



Figure 35: Active Street Frontages at House Hill Town Centre



Figure 36: House Hill Town Square



Figure 37: New Development along Main Street in Mannickville

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5.0 Urban Form Analysis

Best Practice Urban Design - Incentives

Building height, development density or land use are not useful tools to incentivise best practice Urban Design outcomes in the Cessnock Commercial Centre, due to the generally weak market conditions present. However, there is the potential to incentivise desirable development outcomes through permitting lower car parking rates in new developments that achieve Design Excellence or provide Affordable Housing.

It is proposed that additional height is permitted, not as an incentive, but rather to:

- Reinforce the Vincent Street commercial 'spine' and the retail 'core' of the Precinct; and
- To build-in to the planning controls a degree of flexibility, and to not preclude higher development if particular site-specific economic factors support taller development in the Precinct.

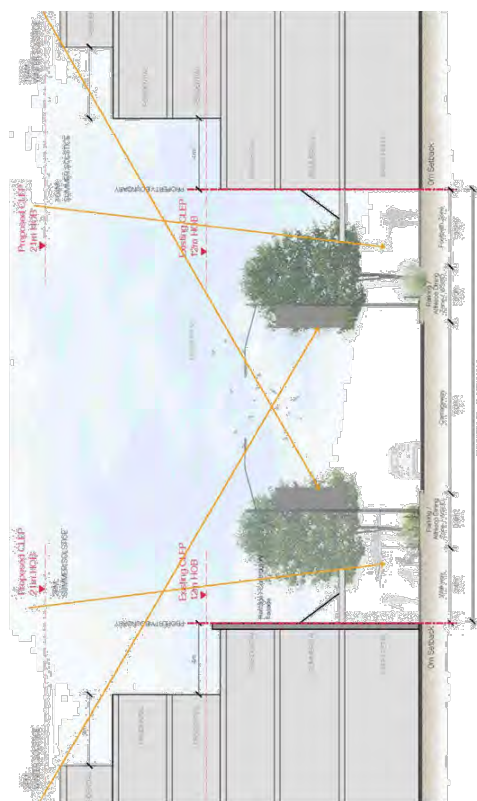


Figure 98a: Main Street Section

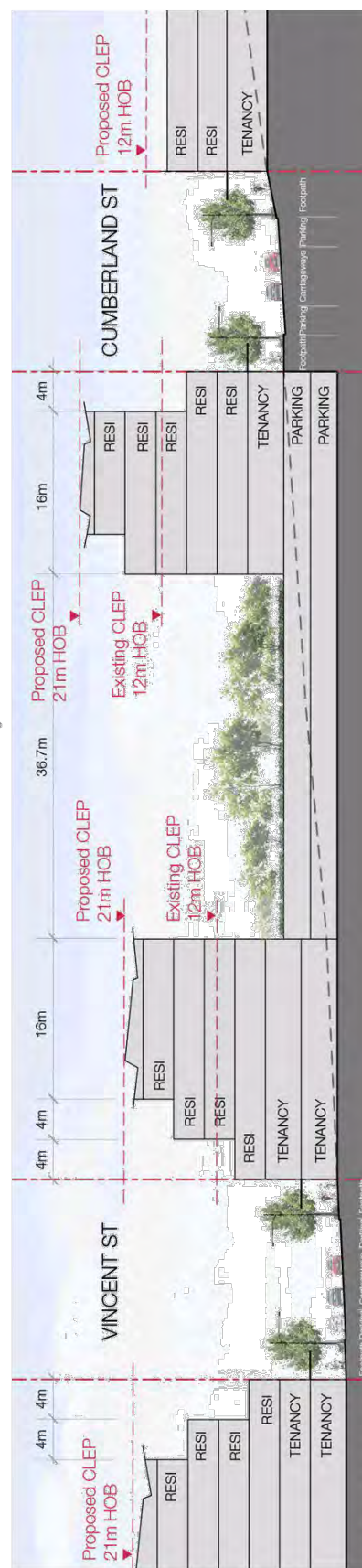


Figure 98b: Vincent Street Section

5.0 Urban Form Analysis

5.5 Development Scenarios

5.5.1 Projected 5 Year Urban Form

Figure 40 illustrates a typical growth scenario for the Commercial Precinct in five years' time, based on the projected development floor space, established in the Supply and Demand study prepared by AEC Group.

Current vacant sites along Vincent Street are envisaged to be redeveloped first, whilst it is hoped that one of the shopping centres will also be redeveloped in order to start defining a new Town Square for the CBD.

It is envisaged that the TAFE facility could be reopened for educational purposes, or alternatively, the proposed 'Cultural Mine and Wine Centre' might be funded and opened on site, creating a new venue that would draw visitors from Sydney and become a regional attraction. A new Cessnock Library facility is planned - either located on Charlton Street adjacent to the proposed Cessnock Town Square and new multi-level public carpark, or as part of the existing CPAC cultural hub, incorporating Cessnock Art Gallery located on Council's currently vacant Vincent Street property.

Cessnock City Council administration building could be expanded on the existing site, incorporating a multi-level car parking facility with retail/community uses at the ground floor level street frontages.

Projected Net Growth at 5 Years:

Retail	3,150 m ²
Office	300 m ²
Business	2,050 m ²
Tourist	300 m ²

Residential	
Medium density dwellings	25
Flats/units/shop top housing	6



Figure 40: Indicative 5 Year Growth Scenario

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5.0 Urban Form Analysis

5.5.2 Projected 15 Year Urban Form

Figure 41 illustrates a typical growth scenario for the Commercial Precinct in fifteen years' time, based on the projected development floor space, established in the Supply and Demand study prepared by AEC Group.

It is anticipated that by the end of this stage the second shopping centre would have redeveloped to complete the full extent of the new Town Square. And the first stage of Cessnock Library and Art Gallery would be completed. The former Cessnock Baths site would have been sold, and redeveloped as a mixed use precinct. Several apartment developments, consolidating several smaller properties along Vincent Street, and Cumberland Street, are envisaged over this timeline.

Projected Net Growth 6-15 Years:

Retail 6,100 m²
Office 1,600 m²
Business 3,850 m²
Tourist 600 m²

Residential
Medium density dwellings 50
Flats/units/shop top housing 10



Figure 41: Indicative 15 Year Growth Scenario

Legend
 Potential Future Development
 Potential Future Car Parking
 Existing Buildings (2016)
 Proposed Town Square Stage 1, 2 & 3

5.0 Urban Form Analysis

5.5.3 Projected 25 Year Urban Form

Figure 42 illustrates an estimated growth scenario for the Commercial Precinct in twenty five years' time, based on the projected development floor space, established in the Supply and Demand study prepared by AEC Group.

At this stage, it is envisaged that some further site consolidations along Vincent Street would allow some additional mixed use developments to be realised, with additional accommodation and commercial development to occur along the proposed South Bypass route and along Wollombi Road.

Projected Net Growth 15-25 Years:

Retail	4,250 m ²
Office	1,500 m ²
Business	1,950 m ²
Tourist	650 m ²

Residential	
Medium density dwellings	40
Flats/units/shop top housing	10



Figure 42: Indicative 25 Year Growth Scenario



6.0 Public Domain Infrastructure Audit

6.1 Introduction

This audit maps and assesses the condition of all public domain infrastructure located within the Cessnock Commercial Precinct study area, including pedestrian paving, street furniture, wayfinding and interpretive signage, public art and car parking infrastructure.

Vincent Street

Cessnock's retail 'Main Street' is the only street in the centre to have been upgraded and the improvements extend four city blocks from Maitland Road, in the north, to Aberdare Road, in the south. A 'family' of related streetscape elements has been implemented in stages over the past decade and in recent years the landscaping has been further refined and commemorative plaques installed highlighting local people of note in the community.

The upgraded streetscape includes honed concrete unit paving with coloured banding/frame, new street trees and roadside planter beds, lighting and street furniture, including seat benches, bollards, outdoor café seating areas, defined by low walls/fences and by trellises, litter bins, and a laneway artwork program has been implemented.

Assessment

The footpath paving is slightly 'dated' in design, however is in good condition, functional, robust, and generally low maintenance. Street crossings are provided at regular intervals along Vincent Street, are safe signalled crossings (some scramble crossing type) and incorporate pedestrian ramps.

Street trees and kerbside planter beds are generally effective in bringing 'green', shade and colour to the street experience. Ornamental grape vines on trellises, and out-out designs are an effective means of developing local identity and highlighting Cessnock's unique Hunter Valley location. No references to the local coal mining industry are evident.

Outdoor café seating areas on Vincent Street are at times utilised successfully, however at some locations are not used at all. In these situations the grey block walls are visually prominent and detract from the streetscape.

No Water Sensitive Urban Design (WSUD) measures, or ESD initiatives, such as litter recycling or reuse/recycling of materials in furniture - such as timber in seating, or the use of energy efficient luminaires, such as LED fittings are evident.

Generally, street lighting is provided by standard 'goose neck' tapering natural galvanised steel poles with traditional luminaires - a functional, low cost road lighting solution, however not ideally suited to a lower scale pedestrian environment, nor is it integrated with traffic signals or road/street signage, resulting in some footpath clutter. Banners are obvious add-ons, rather than integrated with the pole design. Some pedestrian scale pole top light fittings are provided in key cross streets such as Cooper Street.

Whilst the signature furniture colour reflects Council's corporate branding/identity, light colours such as this are often not ideally suited to street furniture items, such as steel framed items, that preferably utilise a dark colour palette.

Other Precinct Streets (Commercial + Residential)

Beyond the Vincent Street 'Main Street' spine, the furniture of the public domain is basic and functional - concrete footpaths, timber light poles, standard street and road signs, and basic street tree planting, and the limited provision of standard 'off the shelf' street furniture.

Infrastructure Audit

The audit undertaken in August 2016, covers every street in the study area, and was assessed on a street-by-street basis. The type, extent and number of infrastructure items have been summarised in three maps covering the north, middle and south of the precinct. Upgrade opportunities for each street where also identified, informing the subsequent development of the Public Domain Plan.



Figure 45: Bench Seating



Figure 44: Bollards

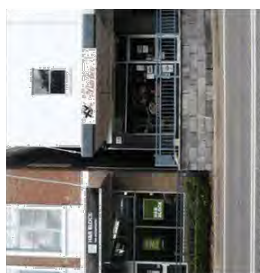


Figure 45a: Cable Sealing Fencing + Wall



Figure 46: Litter Bin



Figure 47: 'Main Street' paving



Figure 48: Pedestrian Fence



Figure 46a: Street Light Pole

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6.0 Public Domain Infrastructure Audit

6.2 Audit of North of Precinct

Apart from Vincent Street, there is only limited existing public domain infrastructure, with some upgrade works completed for key cross streets, such as Edward Street. Generally a low level of amenity is afforded users, businesses and residents of major precinct streets, including Wollombi Road/Mallard Road and Cumberland Street. Charlton Street, Kerrie Street or Darwin Street.

To the west of Vincent Street adhoc open car parking areas dominate resulting in a confusing and uneasy public domain experiences. The laneway that leads west to Woolworths is brought to life by public art display, however, at its west end the laneway delivers pedestrians in a confused 'back of house' parking area.

Street tree planting within the study area (except for Vincent Street) is often non-existent, sparse or adhoc in nature – with some residential streets having impressive avenue planting, whilst others with almost no trees, are hot and exposed environments.

Streetscape	Infrastructure Type	Audit of Existing Infrastructure (15 August 2016)	Upgrade Opportunity
Vincent Street - North (40km/hr speed limit - designated HPA)	Paving	Special 'Main Street' paving - honed coloured concrete unit pavers with three colour banding and edge details, footpath widening (outdoor seating areas), plant verge at most crossing points, tactile indicator pavers.	Carry special paving down laneways for continuity. Investigate resurfacing of VSD opportunities.
	Lighting	Underground power supply, standard tapering steel 'gooseneck' light poles along both sides, floodlights at junction crossings, vertical pole top light at the HJ Stenbeck Lane Park.	Investigate use of LED luminaires for efficiency, long life, low maintenance. Integrate existing/retained luminaires into new car parking areas behind 'Main Street' to improve visibility. Introduce feature lighting for key heritage buildings.
	Street Furniture	Standard bench seats and litter bins provided along both sides of street, continuous steel bollards along west side of Vincent Street, between Edward Street and Cooper Street, street tree planting and planters, 'Hill of Fame' plaques in planters, temporary private cafe chairs/seats, utility boxes, PO boxes, telephone booths, Roads + Maritime signage, picnic table/bench seats at the HJ Stenbeck Lane Park.	Introduce recycling components. Provide bench/entry sign at north 'gateway' to Centre i.e. at Mallard Road corner park. Introduce directional signage to supermarkets and parking areas.
	Outdoor Seating Areas	Honed aggregate concrete block walls with sandstone colour concrete capping unit and metal pedestrian fences near above crossings.	Review locations for footpath widening to ensure alignment with cafe/food tenancies.
	Signal Crossings	Signalised intersections at Mallard Road and Cooper Street (remember 'type crossing' - Vincent Street intersection and mid-block between Edward Street + Cooper Street).	Roads + Maritime to review signal timing if Bypass proceeds. Investigate potential for raised pedestrian crossings.
	Zone Crossing	Philips Lane - Public art on laneway walls.	Support + extend this successful initiative to include laneway light installation networks.
	Drainage	Drainage - 100% covered drainage system (Vincent Street) - 100% covered drainage system (Vincent Street) - 100% covered drainage system (Vincent Street).	Investigate use of public transport lanes for public transport lanes.
	Street Trees	Located in small groups along 'Main Street' (11 total). No footpath.	Review landscaping of HJ Stenbeck Lane Park and north 'gateway' park (at Mallard Road intersection). Provide standard concrete footpath.
Wide Street	Lighting	Standard timber light poles (2 total). Significant existing street trees.	Review lighting levels with utility. Consolidate existing avenue.
Sperry Street	Paving	No footpath.	Provide standard concrete footpath.
	Street Trees	Standard timber light poles (2 total). Limited planting, no trees on south side.	Review lighting levels with utility. Plant trees on both sides to create avenue.
Ferguson Street	Lighting	Standard timber light poles (2 total). Limited planting, no trees on south side.	Review lighting levels with utility. Plant trees on both sides to create avenue.
	Street Trees	Standard timber light poles (2 total). Limited planting, no trees on south side.	Review lighting levels with utility. Plant trees on both sides to create avenue.
Alaridge Road	Paving	Standard concrete paving along both sides.	Maintain existing concrete footpaths.
	Lighting	Standard timber light poles (1 total) + tapering steel 'gooseneck' light poles (1 total).	Review lighting levels with utility. Plant trees on both sides to create avenue.
Westcott Street	Street Trees	Exposed landscape with limited street tree planting.	Maintain existing concrete footpaths.
	Paving	Standard concrete paving along both sides.	Review lighting levels with utility. Plant trees on both sides to create avenue.
Wollombi Road + Mallard Road	Street Trees	Exposed landscape with limited street tree planting.	Consolidate/repair existing concrete footpaths, special paving treatments based on 'Main Street' paving palette to highlight 'gateway' role.
	Lighting	Generally standard timber light poles (22 total) with tapering steel 'gooseneck' light poles (6 total) at intersection.	Provide lighting at Vincent Street 'gateway' park.
	Street Trees	Exposed roadway with limited street tree planting.	Plant trees on both sides to create avenue, landscape median + roundabout islands. Provide planting and signage at Vincent Street 'gateway' intersection.
Millfield Street	Paving	Generally standard concrete footpaths + short section of asphalt footpath.	Complete footpaths on northeast side.
Edward Street	Lighting	Standard timber light poles (3 total).	Review lighting levels with utility.
	Paving	Special 'Main Street' paving near Vincent Street intersection, standard concrete paving along the rest of Edward Street.	Extend 'Main Street' paving through to Cumberland Street in the long term.
	Lighting	Tapering steel 'gooseneck' light poles (1 total) + standard timber light poles (1 total).	Investigate use of LED luminaires for efficiency, long life, and low maintenance.
Darwin Street - North	Street Furniture	None.	Maintain seating.
	Street Trees	Standard timber light poles (1 total).	Plant trees on both sides to create avenue.
	Paving	Standard concrete paving on both sides, full width adjacent Leagues Club, special 'Main Street' paving at the North Avenue intersection.	Maintain existing footpaths, upgrade to full width as redevelopment takes place.
Charlton Street - North	Lighting	Very few street trees.	Investigate use of LED luminaires for efficiency, long life, and low maintenance.
	Paving	Short length of special 'Main Street' paving on east side at former cinema.	Provide full width footpaths for entire length of street.
	Street Furniture	'Tapering steel' 'gooseneck' light poles (1 total).	Review street lighting levels and coverage to reinforce safety.
	Street Trees	Bench seats located at roundabout (2 total).	Maintain seating, provide further seating as required.
Cumberland Street - North	Paving	Generally exposed environment, four trees on east side.	Plant trees on both sides to create avenue (area permitting). Landscape roundabout island.
	Lighting	Standard concrete footpaths on both sides, special 'Main Street' paving at the Cooper Street intersection.	Provide full width footpaths for entire length of street.
	Street Trees	Tapering steel 'gooseneck' light poles (2 total) - standard timber light poles (2 total). Very few street trees.	Investigate use of LED luminaires for efficiency, long life, and low maintenance. Plant trees on both sides to create avenue.

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6.0 Public Domain Infrastructure Audit

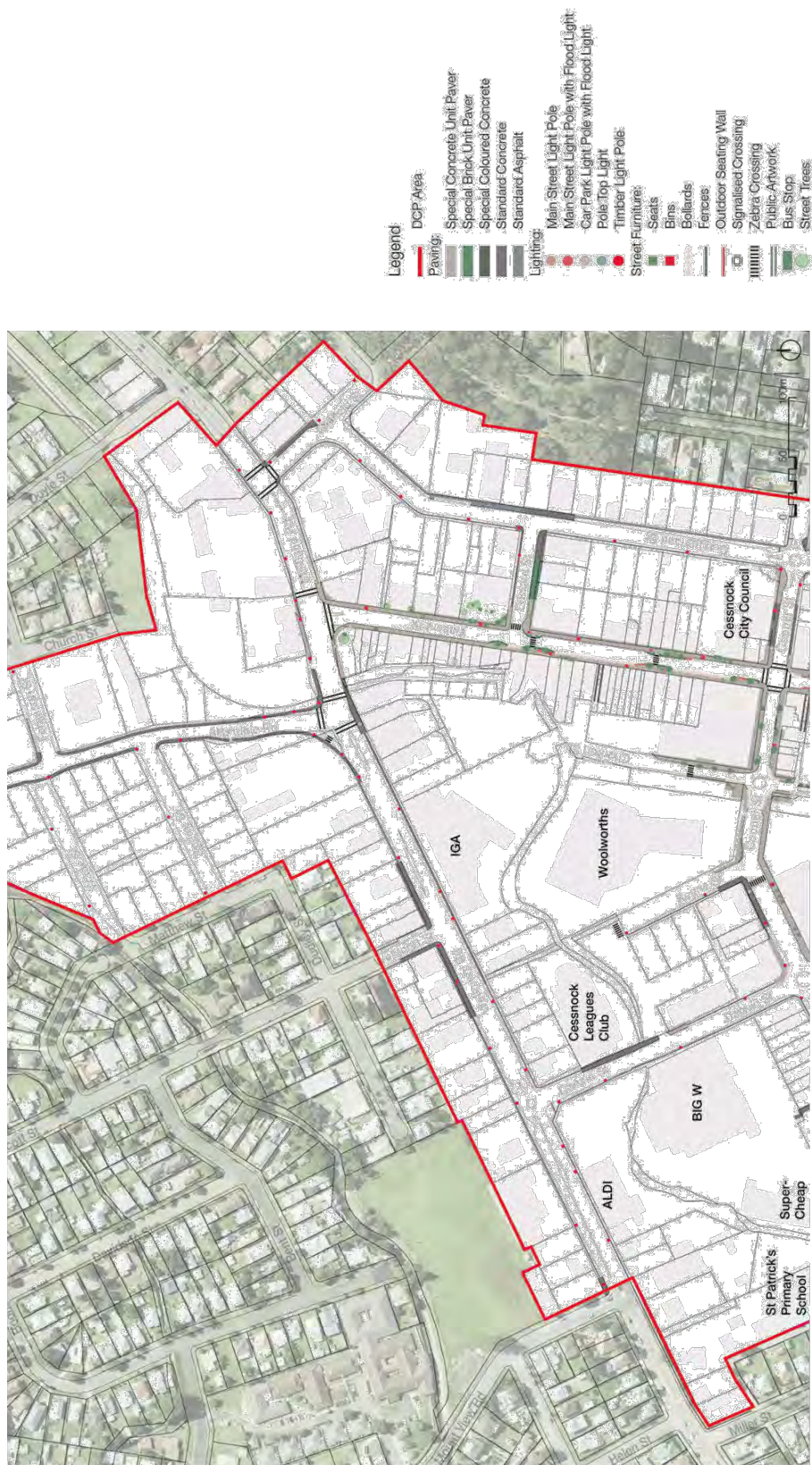


Figure 50: Public Domain Infrastructure Audit Plan - North

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6.0 Public Domain Infrastructure Audit

6.3 Audit of Middle of Precinct

Again, beyond Vincent Street, there is limited existing public domain infrastructure provided in the cross streets, with Cooper Street, and parts of Hall Street and Snape Street/Aberdare Road the exceptions. Many CBD streets to the west only have street lights, and often have no footpaths, trees or furniture. CPAC Plaza is well designed and performs well as a focus of activity pre and post-performance.

A low level of amenity is afforded users, businesses and residents of many streets, including Cooper Street, Hall Street and Snape Street/Aberdare Road, and Cumberland Street, Charlton Street, Darwin Street, and North and South Avenues. The TAFE site lacks a viable and connected footpath system.

To the west of Vincent Street adhoc open car parking areas and drainage canal infrastructure dominate, resulting in a confusing, undefined and unsafe public domain experience. Two laneways lead west to Coles supermarket via a confused 'back of house' public parking carpark. The public realm is poorly defined and car dominated. Orientation is confusing and safe passage for pedestrians not clear.

Street tree planting in the middle of the Precinct (except for Vincent Street) is often non-existent, sparse or adhoc in nature – most streets have very few trees, and are therefore exposed, glary and hot in summer.

Streetscape	Infrastructure Type	Audit of Existing Infrastructure (15 August 2016)	Upgrade Opportunity
Vincent Street - Middle of Precinct - Designated PPV	Paving	Special 'Main Street' paving - tapered concrete unit pavers with three colour banding and edge details, cobble paving (public domain) and special 'Main Street' paving (private domain) at most crossing points, tactile indicator pavers, special 'Main Street' paving at CPAC Plaza.	Carry special paving down laneway to continuity, investigate resurfacing of WILD opportunities.
	Lighting	Underground power supply, standard tapering steel 'greenneck' light poles along both sides, floodlights at extra crossings.	Investigate use of LED luminaires for efficiency, long life, low maintenance, integrate with landscape lighting (light poles, lighting of landscape and car parking areas) to improve safety. Introduce feature lighting for key heritage buildings.
	Street Furniture	Street/finer bench seats and litter bins provided along both sides of street, continuous steel bollards along west side of Vincent Street, between Cooper Street and Hall Street, street tree planting and planters, 'Hill of Fame' plaques in planters, temporary private car constraints, utility boxes, PO boxes, telephone booths, RMS signage.	Introduce recycling programme. Introduce directional signage to TAFE, CPAC, supermarkets and parking areas.
	Outdoor Seating Areas	Formal separate concrete block walls with stainless steel concrete seating and metal pedestrian friendly linear extra crossings.	Review locations for footpath seating to ensure alignment with safe foot crossings.
Cooper Street	Signal Crossings	Signalised intersections at Cooper Street (separate) type crossing, Hall Street (separate) type crossing and Snape Street.	RMS to review signal timing if bypass process.
	Extra Crossings	Mid-blocks between Cooper Street and Hall Street, Hall Street and Aberdare Road.	Investigate potential for raised pedestrian crossings.
	Public Artwork	Artwork between Cooper Street and Hall Street, Hall Street and Aberdare Road.	Support a series of public art installations to improve safety.
	Street Trees	Located in small groups along 'Main Street' (3 total).	Review landscaping and plant street trees in Vincent Street between Hall Street and Snape Street.
Cooper Street	Paving	Generally 'special' 'Main Street' paving along both sides, street section of standard concrete paving between Vincent Street and Cumberland Street.	Maintain existing footpaths, upgrade standard concrete paving to special 'Main Street' paving.
	Lighting	Underground power supply, standard tapering steel 'greenneck' light poles along both sides, feature pole top lights along south side of Cooper Street between Charlton Street and Vincent Street.	Investigate use of LED luminaires for efficiency, long life, low maintenance, integrate feature lighting bollards into light pole design.
	Street Furniture	Street/finer bench seats and litter bins provided along both sides of street, continuous steel bollards along west side of Cooper Street, between Charlton Street and Vincent Street, street tree planting and planters, 'Hill of Fame' plaques in planters, temporary private car constraints, utility boxes, PO boxes, telephone booths, RMS signage.	Introduce recycling programme. Introduce directional signage to supermarkets and parking areas.
	Outdoor Seating Areas	Grape leaves patterned steel bollards located at south side of Cooper Street between Charlton Street and Vincent Street.	Review footpath widening at north side of Cooper Street between Charlton Street and Vincent Street to provide more outdoor seating areas.
North Avenue	Signal Crossings	Signalised intersections at Vincent Street (separate) type crossing.	RMS to review timing at intersection (see above) and if bypass process.
	Extra Crossings	Mid-blocks between Vincent Street and Cooper Street, Vincent Street and Aberdare Road.	Investigate potential for raised pedestrian crossings at Charlton Street to replace roundabout.
	Paving	Generally standard concrete footpaths on both sides, special 'Main Street' paving at Darwin Street intersection and continuous south side to Cooper Street.	Maintain existing footpaths, upgrade street section of standard concrete paving close to Snape Street intersection to special 'Main Street' paving.
	Lighting	Standard tapering steel 'greenneck' light poles (4 total).	Review lighting levels with utility.
Hall Street	Street Furniture	Street/finer bench seats and litter bins provided along south side of North Avenue between Darwin Street and Kenna Street. Street bollards located at two corners of roundabout intersection.	Provides townships entry sign at west 'gateway' to Centre i.e. at South Avenue corner park. Introduce directional signage to supermarkets and parking areas.
	Extra Crossings	Kenna Street intersection.	Investigate potential for raised pedestrian crossings.
	Street Trees	Located in small groups along North Avenue (4 total).	Review landscaping of TAFE grounds and west 'gateway' park (South Avenue intersection), plant street trees on North Avenue.
	Paving	Generally standard concrete footpaths on both sides, special 'Main Street' paving at Vincent Street intersection.	Upgrade paving between Charlton Street and Cumberland Street to special 'Main Street' paving.
Snape Street + Aberdare Road	Lighting	Standard tapering steel 'greenneck' light poles (1 total).	Investigate undergrounding of power supply and upgrade to street light poles.
	Street Trees	Standard tapering steel 'greenneck' light poles (1 total).	Plant trees on both sides to consolidate avenue.
	Paving	Standard concrete paving along both sides of Aberdare Road, special 'Main Street' paving at Vincent Street.	Upgrade paving between Charlton Street and Cumberland Street to special 'Main Street' paving.
	Lighting	Standard tapering steel 'greenneck' light poles (3 total).	Extend existing planting on both sides to reinforce avenue.
South Avenue	Street Trees	Located along both sides of Aberdare Road and north side of Snape Street (15 total).	Investigate undergrounding of power supply and upgrade to street light poles.
	Paving	No footpath.	Provides standard concrete footpath.
	Lighting	Standard tapering steel 'greenneck' light poles (4 total).	Review lighting levels with utility.
	Street Trees	Standard tapering steel 'greenneck' light poles (4 total).	Plant trees on both sides to create avenue.
Bridge Street	Paving	Standard tapering steel 'greenneck' light poles (1 total).	Provides standard concrete footpath.
	Lighting	Standard tapering steel 'greenneck' light poles (1 total).	Review lighting levels with utility.
	Street Trees	Standard tapering steel 'greenneck' light poles (1 total).	Plant trees on both sides to create avenue.
	Paving	Standard concrete paving along both sides of Bridge Street, special 'Main Street' paving at the North Avenue intersection.	Maintain existing paving, provide standard concrete paving for entire length of street.
Charlton Street - Middle	Lighting	Standard tapering steel 'greenneck' light poles (3 total).	Review lighting levels with utility.
	Street Trees	Standard tapering steel 'greenneck' light poles (3 total).	Plant trees on both sides to create avenue.
	Paving	Standard concrete paving along both sides of Charlton Street, special 'Main Street' paving at the North Avenue intersection.	Provides full width footpaths on both sides, for entire length of street.
	Lighting	Standard tapering steel 'greenneck' light poles (3 total).	Maintain existing paving, provide standard concrete paving for entire length of street.
Cumberland Street - South	Street Furniture	Street/finer bench seats and litter bins provided along west side of Cumberland Street, street tree planting and planters, 'Hill of Fame' plaques in planters, temporary private car constraints, utility boxes, PO boxes, telephone booths, RMS signage.	Investigate use of LED luminaires for efficiency, long life, and low maintenance.
	Lighting	Standard tapering steel 'greenneck' light poles (1 total).	Provides full width footpaths for entire length of street.
	Street Trees	Standard tapering steel 'greenneck' light poles (1 total).	Plant trees on both sides to create avenue.
	Paving	Standard concrete paving along both sides of Cumberland Street, special 'Main Street' paving at the North Avenue intersection.	Maintain existing paving, provide standard concrete paving for entire length of street.

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6.0 Public Domain Infrastructure Audit



Figure 51: Public Domain Infrastructure Audit Plan - McCabe

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6.0 Public Domain Infrastructure Audit

6.4 Audit of South of Precinct

The upgraded section of Vincent Street tailors off at the Shape Street/Abardere Road intersection. To the south and west of the 'Main Street' retail spine, there is limited public domain infrastructure. A low level of amenity is afforded users, businesses and residents of precinct streets, including Vincent Street (south), Charlton Street (south), Cessnock Street, Railway Street, Edgeworth Street and Gordon Avenue.

To the west of Vincent Street large open car parking areas associated with the Bunnings warehouse development, dominate, resulting in an undefined and exposed public domain experience. The public realm is poorly defined and car dominated. Orientation is confusing and safe passage for pedestrians not clear.

Street tree planting is often non-existent, sparse or ad hoc in nature – with some residential streets having impressive avenue planting, whilst others and large car parking areas with almost no trees, are hot and exposed environments.

Streetscape	Infrastructure Type	Audit of Existing Infrastructure (15 August 2016)	Upgrade Opportunity
Vincent Street - South	Paving	Generally standard concrete footpaths on both sides, special 'Main Street' paving at Abardere Road intersection.	Extend full width footpaths south to Edgeworth Street intersection.
	Lighting	Standard timber light poles along north side (3 total); lapwing steel 'houseback' light pole (6 total).	Investigate use of LED luminaire for efficiency, long life, low maintenance, integrate feature/leaflet banners (no art pole design).
	Signage	Signage located at Shape Street/Abardere Road.	Plan to review signage timing if Bypass proceeds.
	Zone Crossing	Zone crossing located at Shape Street/Abardere Road.	Review signage for zone crossing.
	Blue Stops	Located close to Abardere Road intersection (2 total).	Provide improved details at bus stops where there are no awnings to encourage use of public transport.
	Street Trees	None.	Plant trees on both sides to create avenue. Provide landscaping for south gateway. (Assess/Implement)
Charlton Street - South	Paving	No footpath.	Provide standard concrete footpath.
	Lighting	Standard timber light poles (2 total).	Review lighting levels with utility.
	Street Trees	None.	Plant trees on both sides.
Cessnock Street	Paving	Short length of standard concrete paving at the north east of intersection.	Provide standard concrete footpath.
	Lighting	Standard timber light poles (4 total); lapwing steel 'houseback' light pole (2 total).	Review lighting levels with utility.
	Street Trees	None.	Plant trees on both sides.
Railway Street	Paving	No footpath.	Provide standard concrete footpath.
	Lighting	Standard timber light poles (1 total).	Review lighting levels with utility.
	Street Trees	Street trees on the north side, no trees on the south side.	Plant trees on both sides to consolidate avenue.
Edgeworth Street	Paving	Short length of standard concrete paving at intersection.	Provide standard concrete footpath.
	Lighting	Standard timber light poles (2 total).	Review lighting levels with utility.
	Street Trees	Very low street trees.	Plant trees on both sides.
Gordon Avenue	Paving	Generally standard concrete footpaths along north side and short section of south side.	Complete standard concrete footpath on both sides.
	Lighting	Standard timber light poles (2 total).	Review lighting levels with utility.
	Street Trees	Good continuous tree planting in street median island. Groups of trees on the south side, limited trees on the north side.	Plant additional trees on both sides.

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6.0 Public Domain Infrastructure Audit



Figure 6.0: Public Domain Infrastructure Audit Plan - South



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7.0 Community Engagement Outcomes

7.1 Introduction

A high level of community engagement was undertaken in the initial phase of the project involving:

- On line surveys;
- Intercept surveys;
- Interactive online mapping;
- Interactive large format mapping in the library;
- Two community workshops; and
- Staff workshops.

7.2 Key Outcomes

Feedback to date includes:

- Facilities/places for young people
- Don't see need for town centre housing – 50%
- Increased and formalised parking
- Increased tourism opportunities & accommodation – 66%
- Like the new precinct (yoghurt land/snap n' treat)
- More clothes/fashion retail and retail in general – 50%
- Improved amenities (toilets, bins, bubblebers)
- Improved pedestrian accessibility between shopping centres



Figure 53: Workshop - Issue Mapping



Figure 54: Staff Workshop



Figure 55: Interactive Large Format Mapping

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Figure 65: Vincent Street, Cessnock

March 2017 | Cessnock Commercial Precinct Project

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8.0 Vision and Objectives

<p>8.1 Theme</p> <p>Three themes have been investigated as means to realise the project vision:</p>	<p>Market / Town Square</p> <p>A meeting place to bring locals and visitors together, celebrating local wines and organic agricultural production, enhancing local cultural experience and identity.</p>	<p>Al-fresco Dining</p> <p>The street as an outdoor dining place to foster activity and a healthy, active lifestyle. Rejuvenate existing heritage buildings by adding a street dining experience. Design pedestrian friendly laneways with active frontages and art elements improving connectors and creating places linking destinations.</p>	<p>Urban Park</p> <p>Celebrating nature and the regions mining heritage. A place to re-charge, to enjoy the sun, the natural environment, attend engaging events and activities.</p>
<p><i>The Cessnock Commercial Precinct is 'The Gateway to the Hunter Valley Wine Country' that celebrates the cultural landscape of the region and provides a quality environment where the community is actively encouraged to participate in community and civic life.</i></p>			



Figure 62: Town Market



Figure 63: Town Market



Figure 64: Active Frontage



Figure 65: Active Frontage



Figure 66: Laneway



Figure 67: Urban Park



Figure 68: Open Canal Landscape

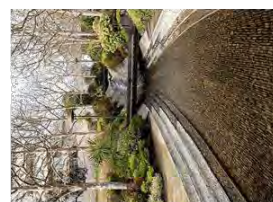


Figure 69: Open Canal Landscape

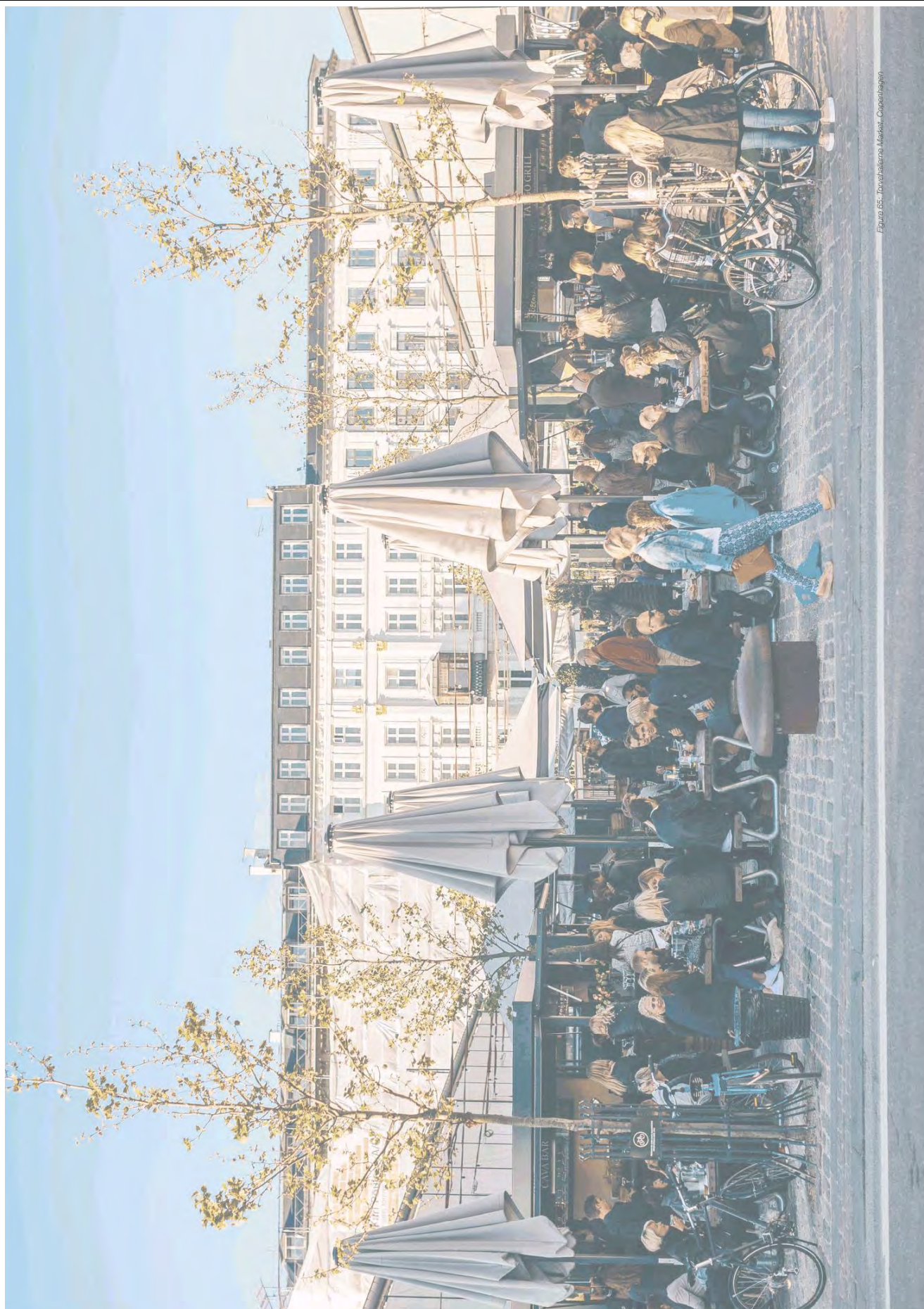


Figure 65: Townhallena Market, Copenhagen

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9.0 Indicative Layout Plan

9.1 Introduction

The Indicative Layout Plan is informed by the CBD Masterplan and the Supply and Demand Analysis undertaken by the AEC Group, and is coordinated with the Urban Form Analysis that was developed in parallel.

The Plan illustrates a 2041 vision and 'master plan' for the future land use of the Commercial Precinct. It has informed the preparation of the Development Control Plan and established the city structure, which sets the framework for the Public Domain Plan.

9.2 Urban Design Issues

There are a number of challenging urban planning issues, mapped in Figure 66, that will need to be addressed, in order to realise the CBD Masterplan 'vision', for an 'Attractive, Thriving and Welcoming' Cessnock Commercial Precinct.

The study identified the following key issues:

- Significant on-grade car parking areas, particularly in the west of the centre, undermine the provision of good urban amenity, and there is a lack of lively and attractive streets that feel safe to use.
- Significant lengths of blank walls and exposed 'back-of-house' functions dominate the experience of the west of the Precinct.
- Large 'super-blocks' in the west of the Precinct offer poor 'through-connectivity' and form barriers in the urban fabric.
- The emerging business park in the south of the precinct lacks an identity, is difficult to access and is poorly integrated with the streets and life of the CBD.
- A network of large scale drainage canals form barriers, are often unsightly and undermine the forming of regular city streets.
- There is a lack of useable open space within a convenient walk of the CBD.
- There is no one public place within the CBD that could be called the 'heart' of the CBD.
- The 'Main Street' retail experience is disconnected from the shopping centre retail experience.

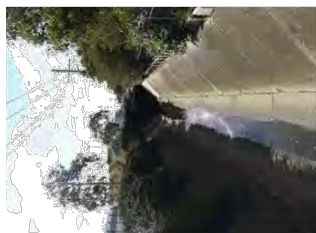


Figure 67: Existing Open Canal



Figure 68: Existing Blank Wall Street Frontages



Figure 69: Existing TAFE Park

- Car Park
- Business Park
- Blank Walls
- Super Blocks
- Drainage - Open Channels
- Drainage - Underground Channels
- DGP Area

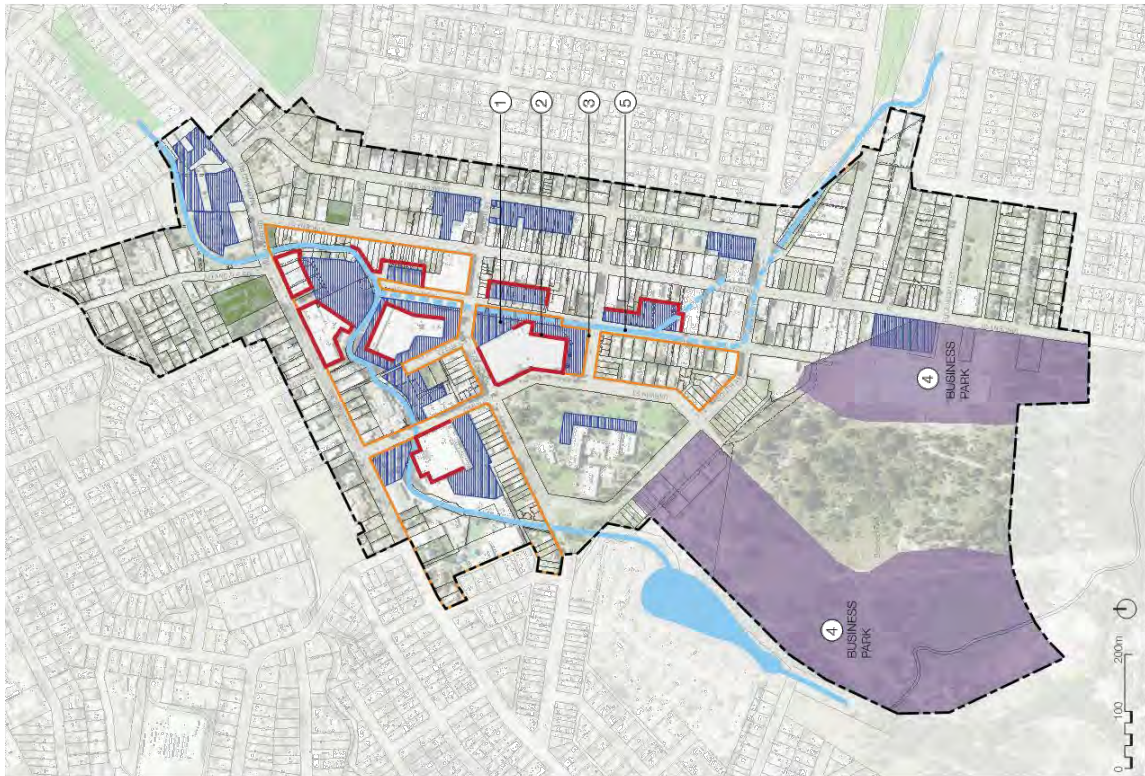


Figure 66: Urban Design Issues

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9.0 Indicative Layout Plan

9.3 Opportunities

The urban design study identified a series of inter-related urban planning opportunities which are illustrated in Figure 70. Some strategies have subsequently been modified, following more detailed analysis, development in the urban design strategy for the centre, and stakeholder consultation, however most of the fundamental urban planning strategies have been taken forward to inform subsequent stages of the project.

Urban planning opportunities identified for the Commercial Precinct, include:

- Extend the 'Main Street' urban character, west, to link up with the shopping centre developments and TAFE site.
- Create a central, urban, public place in the CBD, suitable for markets and as a social focus, a place for people to gather, a meeting place and destination for visitors.
- Complete the CBD urban street and block structure and improve overall amenity for people using the centre, improving the street level experience and developing new parks and plaza spaces.
- Provide new pedestrian links and 'green' corridors along waterways.
- Tailor land uses and urban form to address the proposed south CBD bypass.
- Reinforce the Precinct identity through featuring the three main CBD gateways.
- Improve signage and wayfinding at key locations to guide visitors to explore more of Cessnock.
- Provide a new draw-card visitor centre which is a landmark destination which attracts visitors from Sydney and regional NSW.

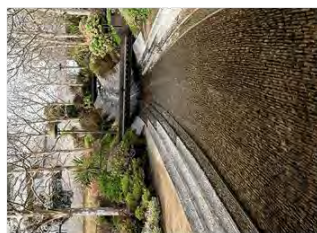


Figure 71: Open Canal Green Link



Figure 72: Food Market



Figure 73: Urban Park

- Retail Core
- Potential Urban City Blocks
- Existing Open Space
- Potential New Plaza
- Potential Market
- Potential New Green Space
- Gateway/City Landmarks
- Existing Main Street
- Potential New Retail Street
- Key Pedestrian Links
- Green Connections
- Waterway
- DCP Area

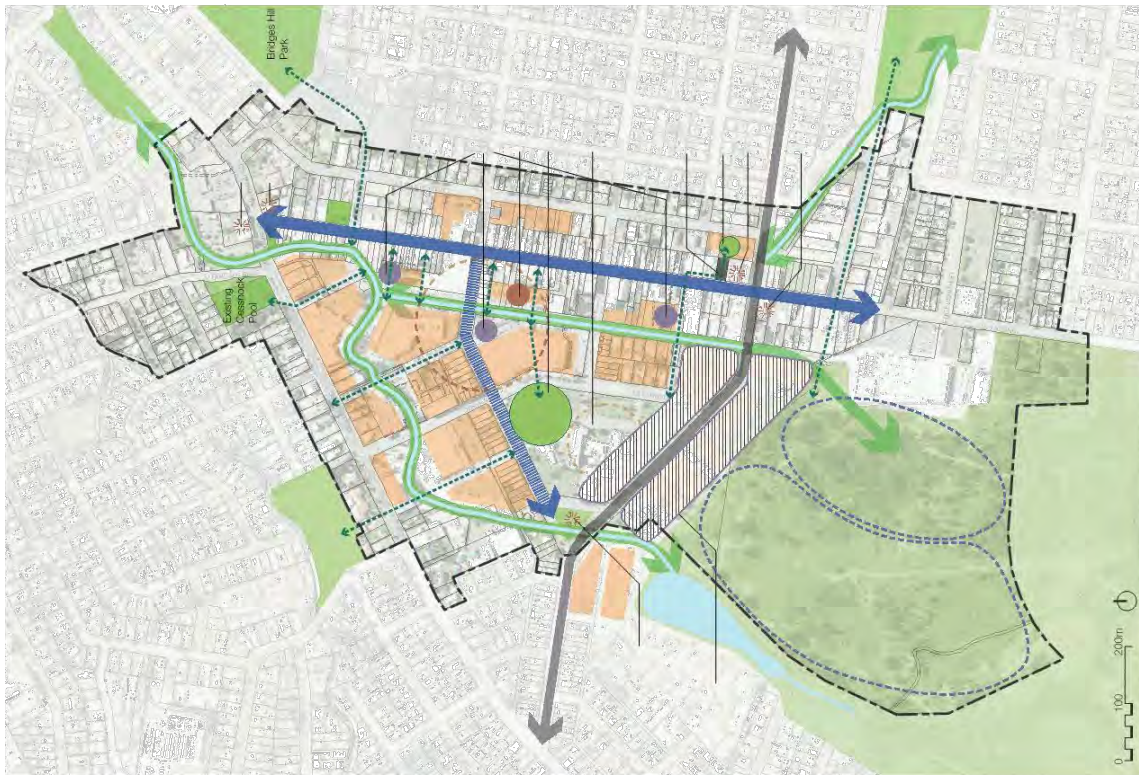


Figure 70: Urban Design Opportunity

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9.0 Indicative Layout Plan

9.4 Thematic Precincts

Cessnock Commercial Precinct will comprise a series of inter-related themed sub-precincts, each with their own distinct function and character. Collectively there will be synergies as sub-precincts overlap and complement each other.

The thematic precincts include:

- Mixed use zone
- Supermarket retail + club precinct
- Main road frontage businesses
- School precinct
- Medium density residential neighbourhood
- Light industrial/distribution
- Big box retail and business park
- Environmental preservation landscape

Within the Precinct there are four distinct activity 'hubs' that provide a focus for:

- Cultural/arts activities
- Civic functions
- Entertainment
- Potential 'Tourist/cultural destination

Urban structure is provided by the 'Main Street' spine of Vincent Street, and by the three 'gateways' to the centre, marking the arrival and departure experience at the west, north and south periphery. The potential bypass route in the south of the Precinct will also potentially drive some land uses in the centre.

An outdoor dining strip is identified along Vincent Street, extending north from Hall Street, and then linking west along Cooper Street, culminating in the proposed Town Square that will become a new focus for socialising and for a program of activities. This sub-precinct is envisaged as pedestrian friendly, lively and with an urban form that is 'fine grained' and highly permeable in terms of pedestrian access.

A synergy is envisaged through integrating the Vincent Street, 'Main Street' retail experience with the two major shopping centres, and also through enrolling the entertainment and civic hubs situated along this important cross axis.



Figure 76: Shopping Centre Plaza in Ryde



Figure 78: Outdoor Dining

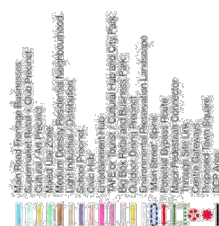


Figure 74: Thematic Precinct Plan

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9.0 Indicative Layout Plan

9.5 Indicative Layout Plan

The Plan provides an overall strategic level framework for the future urban planning of Cessnock Commercial Precinct. A holistic and integrated urban planning approach has been taken, drawing upon Council's expertise and detailed knowledge of Cessnock, a comprehensive process of stakeholder consultation and on the Consultant Team's analysis, and urban and landscape planning and design expertise.

The primary urban design 'moves' are illustrated, including the proposed land uses, location for public and community facilities (some with options), the proposed street hierarchy, major green spaces and pedestrian links, CBD gateways, overall drainage and regional infrastructure, public car parking sites and the major people places in the public domain are established.

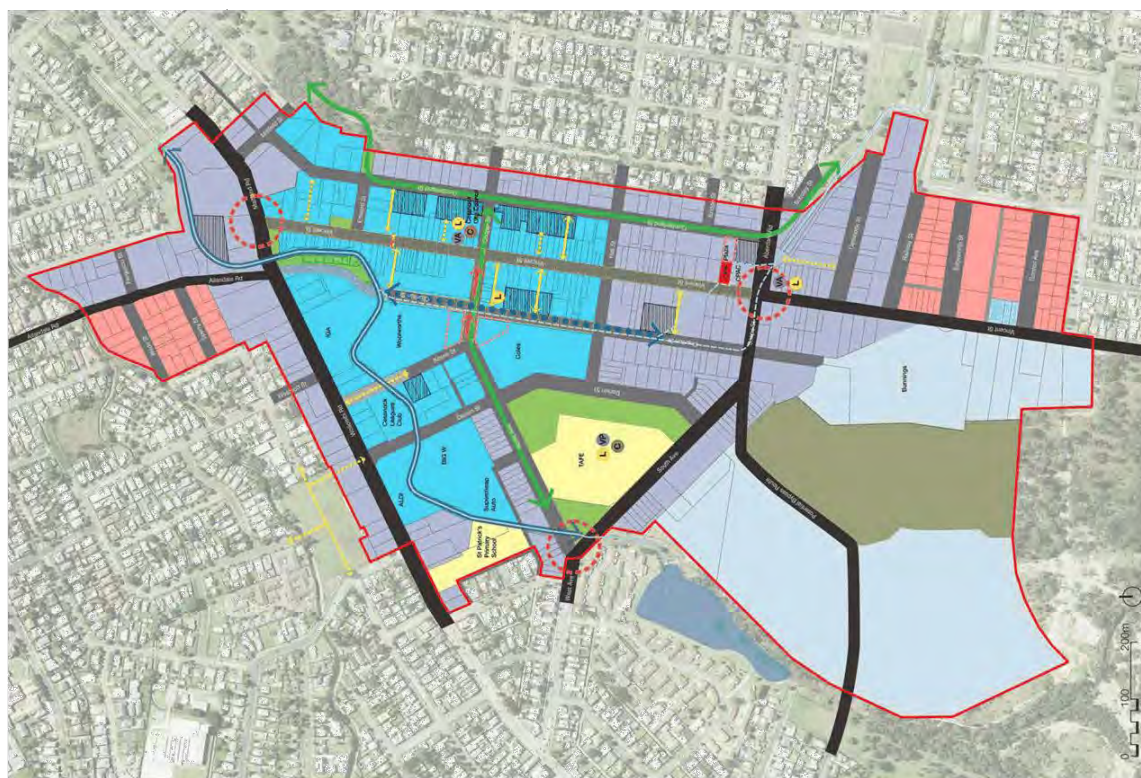
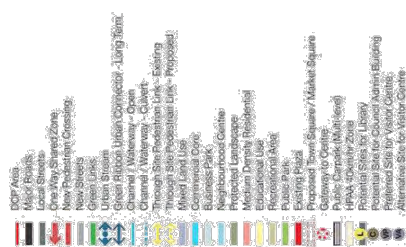


Figure 77: Indicative Layout Plan

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9.0 Indicative Layout Plan

9.6 Fostering Street Life + Activity

Well planned and designed urban centres have streets and public places that have good urban scale, are easy to access and move around, they feel safe, and most importantly are lively and interesting social destinations.

An important contributor to street life, are the activated frontages of businesses that line the CBD 'Main Street'. Requiring new businesses along the key streets of Cessnock to build up to the street frontage and to provide active uses at their ground floor level, will support a lively streetscape experience. This in turn will create an attractive destination for locals and visitors to the centre and support local business owners. The key principles include:

- Promote lively, active streets in the Commercial Centre;
- Ensure that development contributes to the quality, activity, safety and amenity of streets and the public domain;
- Provide for a comfortable pedestrian environment through the provision of awnings; and
- Contribute to the vitality of streets by maximising entries and display windows to retail uses and minimising blank walls.



Figure 78: Active Street Frontage in Paris



Figure 80: Active Street Frontage in Inner West Sydney

- Active Frontage - Required
- Active Frontage - Preferred
- Active Frontage - Encouraged
- Square / Plaza
- Existing Canal
- DCP Area

Notes: Awnings are required for all active frontages

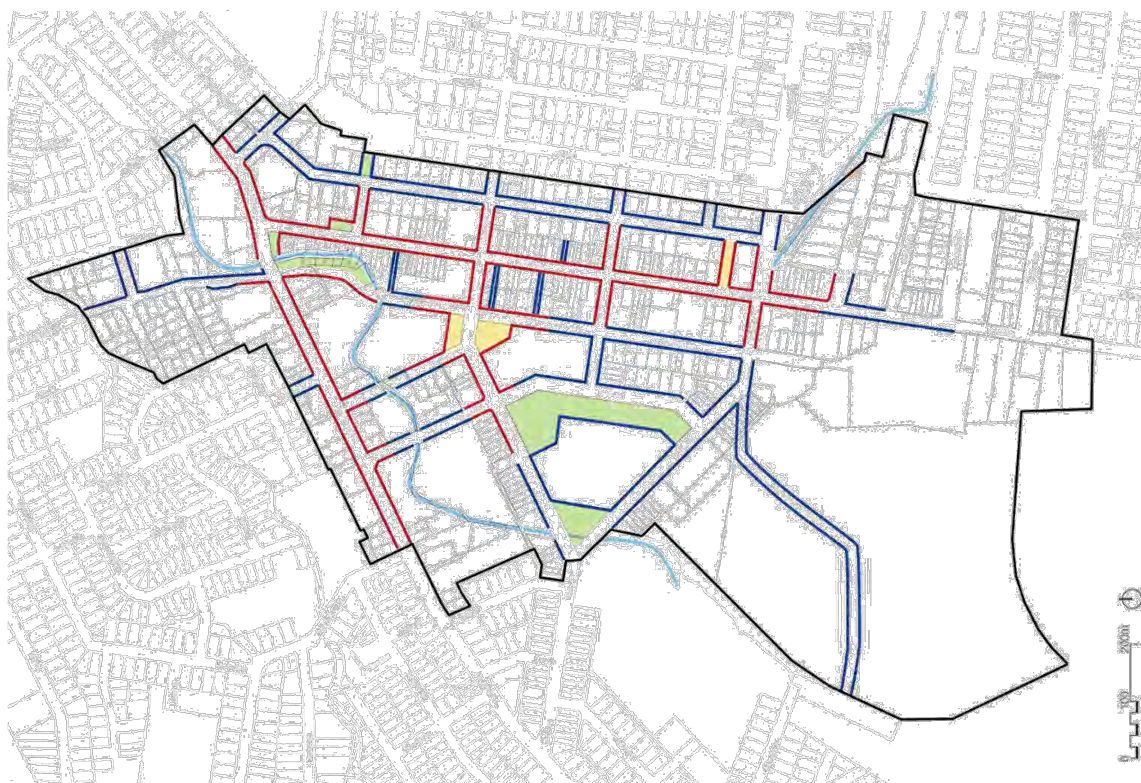


Figure 79: Active Frontage Plan

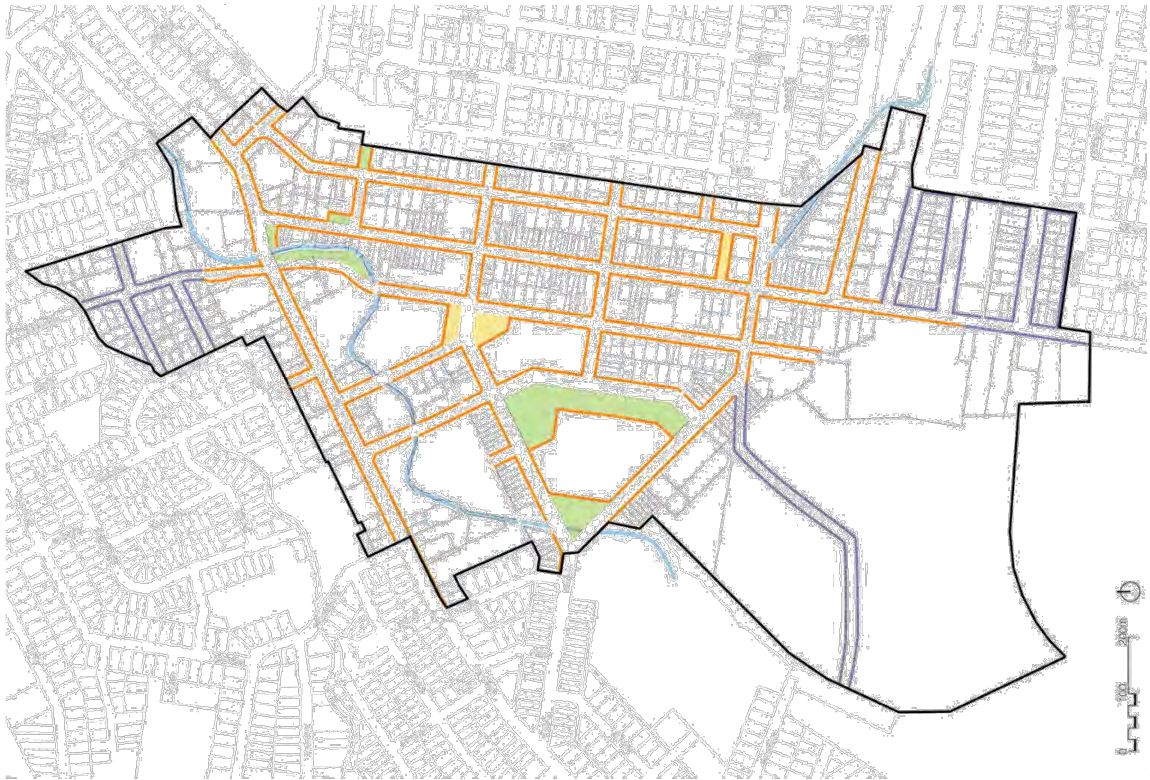


Figure 81: Setback Plan

CM⁺

9.0 Indicative Layout Plan

9.7 Ensuring Good Human Scale

Good urban scale is a major factor in determining how we experience urban centres. A commercial precinct, such as Cessnock, has good urban scale when the streets, plazas and parks which comprise the public domain, are in a comfortable proportioned with regard to the built form. This can be tested by analysing the streets, parks and plazas in cross section.

Heritage buildings often set a precedent in this regard, establishing classic architectural details such as parapets, facade modulation, detailing and street awnings that both define the streetscape whilst also establishing good human scale.

A contemporary architectural approach is encouraged. New buildings should complement the existing heritage buildings in the 'Main Street' in scale by building to the street frontage and by consolidating the existing street wall height - setting back above this height, to respect the parapets of the traditional street frontage.

The key setback requirements are illustrated in the adjoining figures. In summary:

- To effectively define the streets and blocks of the CBD, generally a zero setback is required for the majority of the Commercial Precinct; and
- A 5.0m minimum front setback is required for the north and south residential areas, to allow for front gardens that enhance a low scale residential street; and
- In the Business Park in the southwest of the Precinct.



Figure 82: Mixed Use Development Integrating a Heritage Building Facade



Figure 83: Mixed Use Development Integrating a Existing Heritage Building Facade

- 0m Setback (at street wall)
- 5.0m Setback
- 5.0m Setback (at street wall)
- Square / Plaza
- Existing Canals
- DCP Area

CM⁺

9.0 Indicative Layout Plan

9.8 Height of Building

An increase in building height is proposed for Cessnock Commercial Precinct. Building heights in the CBD will:

- Reinforce the future urban form and character of the Precinct;
- Maintain good scale – in the street cross section;
- Respect the scale of existing heritage buildings;
- Avoid undue overshadowing of city streets and parks;
- Focus retail and street activity in the north of the Centre; and
- Build-in flexibility and reflect the projected future demand for floor space.

The recommended building height is illustrated in the adjoining Height of Building Map and can be summarised as:

- 21m (6 storeys) for the city block defined by Charlton Street, Cooper Street, Cumberland Street and Edward Street;
- 15m (4 storeys) for the 'Commercial Core'; and
- 12m (3 storeys) for the remainder of the Precinct - for education, Business Park and residential areas.



Figure 85: Precinct - Mixed Use Development Along Main Street in Murrumbidgee



Figure 86: Precinct - Mixed Use Development Along Main Street in Murrumbidgee

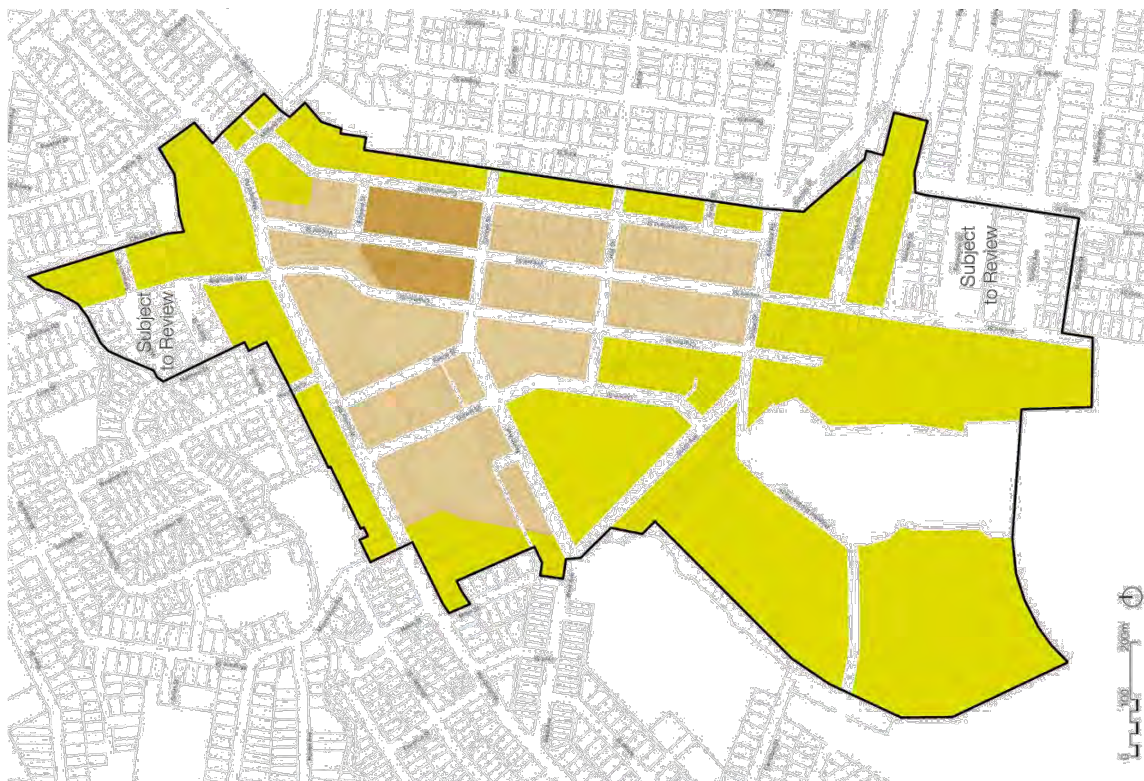


Figure 84: Height of Building Map

CM⁺

9.0 Indicative Layout Plan

9.9 Active Transport Strategy

The Active Transport Strategy (ATS) prioritises pedestrian and cyclist access within the CBD and establishes a coordinated plan for the staged improvement and construction of new facilities as the CBD develops, and funding is available. The ATS is consistent with the Cessnock Bicycle Strategy (2016) and identifies specific opportunities for bicycle parking and for a repair 'hub' at the centre of the CBD.

The key principles of the ATS are to:

- Prioritise pedestrian and bicycle access, connectivity and safety;
- Ensure that any new public pedestrian and cycling links are located on nominated sites;
- Promote cycling through the mandatory provision of bike storage for nominated land uses, and
- Promote cycling through the strategic placement of bike storage facilities in the public domain.



Figure 98: On Road Cyclist



Figure 99: Off Road Cyclist

- Cycleways**
- Existing Regional On Road
 - Existing District On Road
 - Existing Local On Road
 - Proposed Regional On Road
 - Proposed District On Road
 - Proposed Local On Road
 - Proposed Regional Off Road
 - Proposed District Off Road
 - Proposed Local Off Road
 - Bicycle Parking
- Pedestrian Links**
- Major On Foot
 - Through Site Link
 - DGP Area

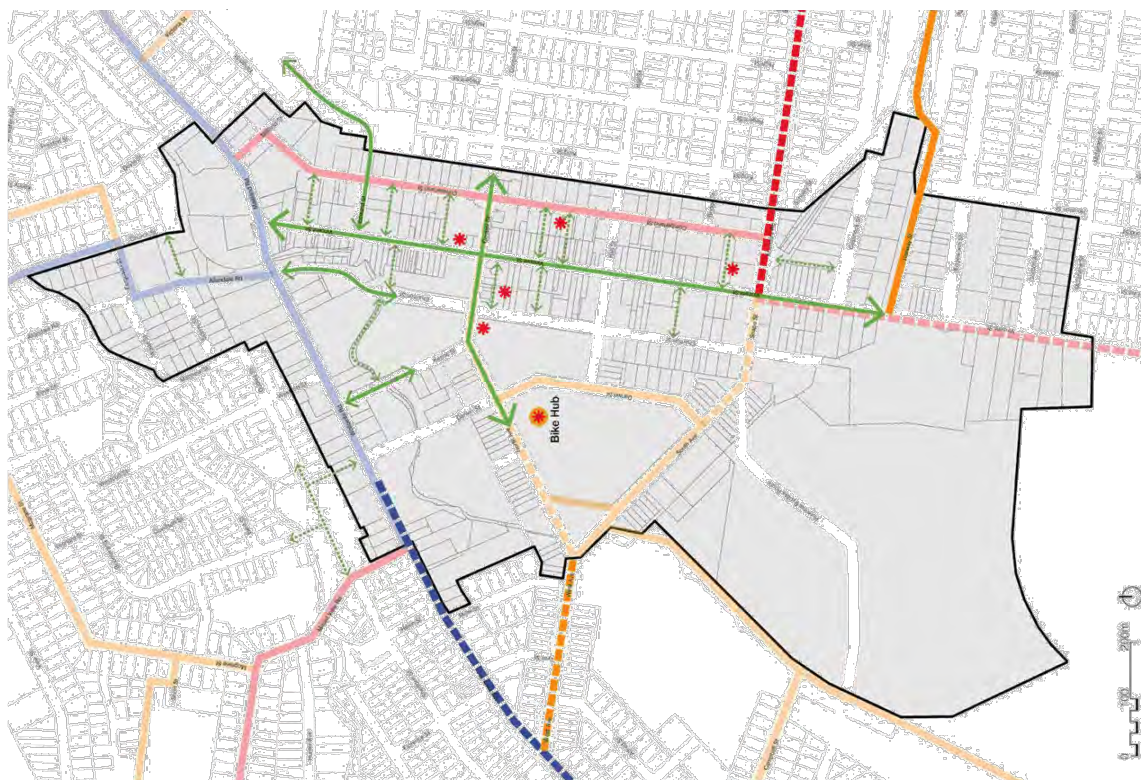


Figure 97: Active Transport Strategy

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9.0 Indicative Layout Plan

The Plan encourages the use of public transport (bus services) in the Commercial Precinct. A permeable urban form, including regular and safe through-site links, will enhance connectivity through the Commercial Precinct, so that residents, workers and visitors have access to safe and convenient public transport.

The following strategies are proposed to encourage public transport use:

- Encourage public transport (bus) use through lobbying State Government for improved frequency of services;
- Provide a high level of amenity by providing quality bus shelters that provide good weather protection (where no awnings);
- Provide seating that suits all age groups and levels of accessibility;
- Provide adequate lighting to enhance personal safety;
- Provide comprehensive transport information signage;
- Incorporate easy to read maps of Cessnock which illustrate key destinations; and
- Incorporate provisions for the sight impaired.



Figure 90: Public Transport Network

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9.0 Indicative Layout Plan

9.12 Opportunity Sites

The study has identified a number of key sites within the Commercial Precinct which due to their location, size and function, have significant potential to positively shape the future of Cessnock. The adjoining table and map, identifies the location, site area and significance of each site within the Precinct. An assessment was then made as to the urban development opportunity that each site presents, and the Development Objective sought.

The strategies employed include:

- Provide development incentives to enable design excellence and sustainable design for new development;
- Identify redevelopment opportunities and development objectives for Catalyst Sites;
- Reduce the isolated nature of the major shopping centre areas; improve the appearance and reduce the environmental impacts of public car parking;
- Identify options for the redevelopment of the TAFE site as a regional cultural/educational centre and district-level park; and identify the long term renewal plan for the Commercial Precinct's major stormwater canal as an attractive landscaped linear green space and access way.

9.13. Catalyst Sites

Thirteen Catalyst Sites in Cessnock Commercial Precinct were identified:

1. South Avenue Properties
2. TAFE
3. Coles Supermarket
4. Cessnock Plaza (Woolworths)
5. Cessnock Baths Site
6. Former Cinema + Carpark
7. Vincent Street Carpark Site
8. Cessnock City Council
9. Public Carpark
10. Mixed Use Site
11. Public Carpark
12. Vincent Street Site
13. Hall Street Site



Figure 62: Site 3 - Coles Supermarket Site



Figure 63: Site 4 - Cessnock Plaza Site

DCP Area
Development Sites
Catalyst Sites
Open Space Opportunities



Figure 91: Catalyst Sites Plan

CM⁺

9.0 Indicative Layout Plan

Ref.	Catalyst Development Site	Ownership	Area (Ha)	Significance	Development Objectives
1	South Avenue Properties	Private	1.31	Properties along South Avenue have important frontages (visually prominent) to proposed future Bypass route.	<ul style="list-style-type: none"> To encourage future development promotes commercial, retail and business activity.
2	TAFE	Department of Technical + Further Education	3.16	A major site in the west of the Centre - if educational use is to continue, a facility of this scale brings employment, retail demand and a vibrant student demographic to the Centre.	<ul style="list-style-type: none"> To encourage the use of the TAFE site as an education hub, or in the event that TAFE Cessnock ceases its operations, an adaptive re-use of the TAFE site that has a civic function.
3	Colas Supermarket	Private + Council (Operational)	2.00	A large, and key site in the heart of the Centre and a major retail destination. A key site on the Cooper Street cross axis link to 'Main Street' and with an important frontage to TAFE Park.	<ul style="list-style-type: none"> To aid in the development of an integrated commercial centre. To promote the development of a town square for the neighbourhood, accommodating areas for gathering, seating and children's play.
4	Cessnock Plaza (Woolworths)	Private + Hunter Water Corporation	1.82	Also a large, and key site in the heart of the Centre and a major retail destination. A key site on the Cooper Street cross axis link to 'Main Street' and with important street frontages.	<ul style="list-style-type: none"> To aid in the development of an integrated commercial centre. To promote the development of a town square for the neighbourhood, accommodating areas for gathering, seating and children's play.
5	Cessnock Baths Site	Council (Community)	0.69	The long term plan is for Cessnock Baths to be relocated to a new site outside the Centre, and, as a result, this prominent corner site, at the northern 'gateway' to Cessnock Commercial Centre, would become available for other uses.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design. To encourage future development that promotes a suitable mix of land uses.
6	Former Cinema + Carpark	Private	0.60	A prominent 'Main Street' corner property with significant frontage to Cooper Street - an important west-east connector street. The former cinema provided one of the few night time entertainment venues in the CBD.	<ul style="list-style-type: none"> To encourage the ongoing use of the site for entertainment purposes.
7	Vincent Street Car Park Site	Council (Operational) + Private	0.33	Consolidated, current carpark site in the heart of the CBD, with both Vincent Street and Cumberland Street frontages. Elevated with views west over the valley.	<ul style="list-style-type: none"> Future development of the site is to achieve high quality architectural outcome that promotes retail, commercial and residential uses.
8	Cessnock City Council	Council (Operational)	0.62	Current Cessnock City Council, Council Chamber, offices, public address and associated car parking. A key consolidated site within the CBD with Vincent Street, Cooper Street and Cumberland Street frontages.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design.
9	Public Carpark	Council (Operational) + Private	0.59	Consolidated Commercial Centre site, currently used for public car park. Significant CBD corner/street frontages.	<ul style="list-style-type: none"> Ensure future development promotes activity on the street and within the public domain.
10	Mixed Use and Public Carpark Site	Cessnock Leagues Club	0.54	Large consolidated site, including on site parking and green space.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design and public domain outcome.
11	Public Carpark	Council (Operational)	0.16	Consolidated Commercial Centre site currently used for public car park. Significant CBD corner/street frontages.	<ul style="list-style-type: none"> Future development is to encourage active street frontages.
12	Vincent Street Site	Council (Community)	0.41	Consolidated, prominent 'Main Street' corner site, currently three-quarters vacant.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design and public domain outcome.
13	Hill Street Site	Council (Operational)	0.07	Prominent Centre corner site, currently vacant.	<ul style="list-style-type: none"> To ensure any future development of the site achieves high quality architectural design and public domain outcome. To provide for active street frontages to promote activity on the street and within the public domain.
14	Potential Library Site and Public Carpark	Council (Operational)	0.40	Consolidated Commercial Centre site currently used for public car park.	<ul style="list-style-type: none"> Future development is to encourage active street frontages.

Table 4: Catalyst Sites



Figure 9.4: Open Space Opportunity Sites Plan

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9.0 Indicative Layout Plan

9.14 Open Space Opportunity Sites

The study also identified Open Space Opportunity Sites in the Commercial Precinct. The adjoining table and map, identifies the location, site area and significance of each, within the Precinct. An assessment was then made as to the open space/public realm opportunity that each site presents and the Development Objective sought.

Seven Open Space Opportunity Sites in Cessnock Commercial Precinct were identified:

- A. TAFE Park Site
- B. Stormwater Canal (north)
- C. Vincent Street 'North Gateway' Park
- D. HJ Sternback Lions Park
- E. Cooper Street and Shopping Centre Carparks
- F. CPAC Carpark (Plaza extension)
- G. Western Gateway Triangle



Figure 9.6: Site B - TAFE Site



Figure 9.6: Site G - Stormwater Canal (north)

DCP Area
Development Sites
Open Space Opportunities

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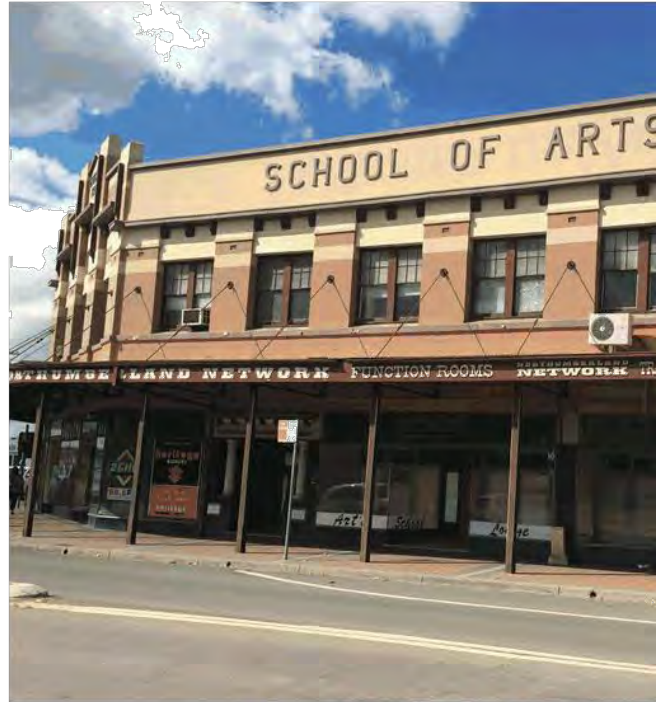
9.0 Indicative Layout Plan

Ref.	Open Space Opportunity	Ownership	Area (Ha) / Length (m)	Significance	Development Objectives
A	TAFE Site	Department of Technical and Further Education (TAFE)	1.51	A large green space at the 'heart' of the CBD that is currently underutilised, poorly defined, and lacks usable spaces and places to enjoy on a day to day basis (e.g. currently consists of a war memorial, gazebo, exposed lawn areas, scattered trees and parking areas).	<ul style="list-style-type: none"> To provide a distinct recreational park for the community, accommodating areas for gathering, picnicking, children's play and relaxation.
B	Stormwater Canal (North)	(Hunter Water Corporation)	883m (length)	A major concrete stormwater canal, of significant width, depth and length, weaves through the urban fabric of five city blocks.	<ul style="list-style-type: none"> Upgrade and incorporate WSLD of the existing stormwater canal.
C	Vincent Street 'North Gateway' Park	Council (Community)	0.07	Prominent corner site at the north gateway to the Commercial Centre.	<ul style="list-style-type: none"> Enhance the existing park signifying its gateway location.
D	HJ Sternbeck Lions park	Private	0.08	Publicly accessible, medium sized park located on a key corner of the 'Main Street'.	<ul style="list-style-type: none"> Upgrade the existing park and open to surrounding public domain.
E	Cooper Street and Shopping Centre Carparks	Private	0.48	Underdeveloped space at the 'heart' of the CBD, with connections from key shopping centres to the 'Main Street', currently only used for carparking.	<ul style="list-style-type: none"> To promote the development of a town square for the neighbourhood, accommodating areas for gathering, seating and children's play. Develop Cessnock Market Strategy.
F	CPAC Carpark	TBC	0.08	Important through-block public domain, pedestrian and visual connection from Vincent Street (existing plaza) through to Cumberland Street, adjoining CPAC.	<ul style="list-style-type: none"> Encourage the expansion of the existing CPAC plaza through to Cumberland Street.
G	Western Gateway Triangle	TBC	0.37	Prominent corner, triangular site at the west gateway to the Commercial Centre.	<ul style="list-style-type: none"> Deliver a gateway park, which enhancing streetscapes.

Table 5: Open Space Opportunity Sites



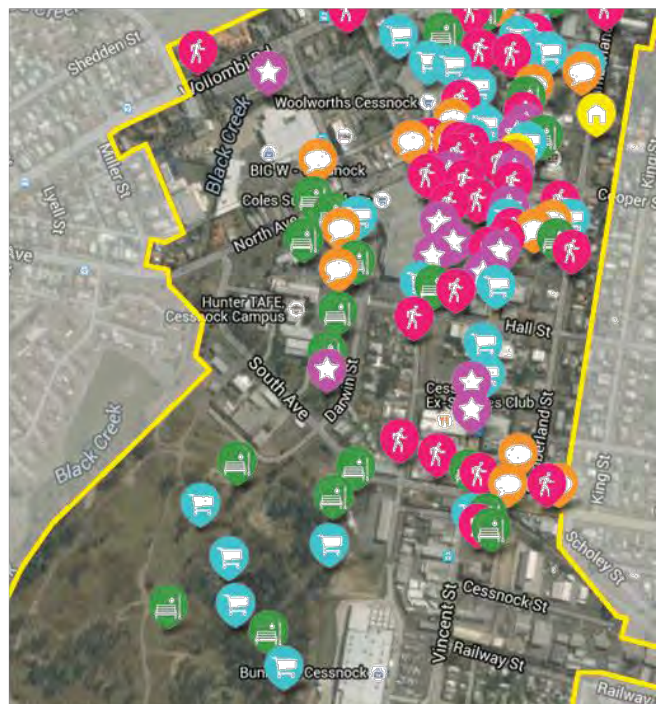
Figure 97: Town Square, Cessnock Hill



CESSNOCK CITY COUNCIL

Cessnock Commercial Precinct Public Domain Plan and DCP Engagement Outcomes Report

July 2016



Creating and building community

CESSNOCK CITY COUNCIL

ENGAGEMENT REPORT: PUBLIC DOMAIN PLAN AND DCP

**Report Title: Public Domain Plan and DCP
Engagement Report**

Client: Cessnock City Council

Version: Final

Date: 28 July 2016



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Cessnock Commercial Precinct Public Domain Plan and DCP Engagement Outcomes Report | 28 July 2016 |

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CESSNOCK CITY COUNCIL

ENGAGEMENT REPORT: PUBLIC DOMAIN PLAN AND DCP

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CESSNOCK CITY COUNCIL

ENGAGEMENT REPORT: PUBLIC DOMAIN PLAN AND DCP

Executive summary

Background

Cessnock City Council was awarded funding from the NSW Department of Planning and Environment to develop a Public Domain Plan and Development Control Plan (DCP) for the Cessnock Commercial Precinct (CCP). Council engaged Architects and Urban Designers CM+ to deliver the plans. A central part of the design process was engagement with the Cessnock community. This report provides the outcomes of community engagement completed in June and July 2016.

Community engagement completed

Approximately 263 people participated in the engagement process which included: online and intercept surveys with local business and residents (87 completed); an online and interactive community mapping tool and large maps in Council's Customer Service Centre and Library (135 comments); community workshop (29 attendees); and a Cessnock Council staff workshop (12 attendees). Engagement was promoted through a dedicated webpage on Council's website, Council's Facebook page, a project postcard, notifications in Council's e-newsletter, and communications directly with key stakeholders.

Identified priorities

A number of priorities emerged across all consultation types and themes. These priorities are for:

- Improved and more connected parking
- Improved parks and playgrounds and places for children, families and young people to play and gather
- Improved safety at night
- A more active and vibrant town centre including increased tourist accommodation and services
- Improved pedestrian access and connectivity, and
- Improved and increased amenities including toilets and bins.

Key findings by theme

Land Use

Priorities identified in relation to future land use include increased tourist and visitor accommodation and more diverse retail. In relation to housing the survey indicated a preference toward either no housing or medium density housing in the commercial precinct. However, the community map and community workshops indicated that some stakeholders consider there is a benefit to increased housing in the CCP.

A high proportion of business owners surveyed indicated that tourist and visitor accommodation and services would improve trade. A majority of residents surveyed thought there should be more retail, of the people who thought that there was sufficient retail, a concern was filling currently vacant shops. The

CESSNOCK CITY COUNCIL

ENGAGEMENT REPORT: PUBLIC DOMAIN PLAN AND DCP

main suggested improvement to the overall shopping experience in the survey was more variety/more shops.

Building height

Across all consultation types, there were only a few comments regarding building height and the views of respondents differed. In the community workshop, "good current height" was given as a strength of the CCP, while "can't go high" was given as a weakness. "Keeping the country feel – not too high" but "increasing the height limit to 3 or 5 storeys" was also objectives suggested.

Character

Feedback regarding the character of the CCP focused on preserving the heritage character (particularly on Vincent St) and providing more activity and vibrancy at night including wine bars, outdoor eating, live music and events. In the staff workshop there was a recurring theme of preserving and promoting heritage, both mining and Indigenous.

Access and transport

Parking and pedestrian and cyclist access and connectivity were the main priorities identified in access and transport. Parking was identified as an issue across all consultation methods and resident and business stakeholders with the main priority more connected and upgraded carparking.

Improved pedestrian and cyclist access and connectivity was considered a priority particularly in relation to the layout of the shopping plaza (in particular Charlton Street Shopping Centre) and connectivity between shopping areas. While "walkability and pedestrian crossings" were identified as one of the strengths of the CCP, the need to improve walkability was a recurring issue in comments. Closing Cooper Street was a commonly suggested improvement.

Public domain

Key issues regarding the public domain were the provision of facilities (particularly bins and toilets); providing parks with recreation facilities including children's playgrounds; improving safety; providing something for young people to do; and improving the drainage channels. The staff workshop highlighted the need for more places for people to gather and "stay" in the CCP. Possible improvements to parks and plazas noted through the community map were: places for people to gather and sit and spend time (e.g. parklets); improvements/centralising of parks and promoting their use through infrastructure (chairs, tables, shade) and outdoor events as well as activating TAFE Park through outdoor events (performances/concerts).

Safety was a key concern identified through the survey, particularly at night in behind the mainstreet. Finally, the community workshops and staff workshop identified a need to improve the look and use of the drainage channels.

1. Introduction

1.1. Background

This report presents the findings from the community engagement delivered to inform the development of the Cessnock Town Centre Public Domain Plan and Development Control Plan (DCP). Cessnock City Council was awarded funding from the NSW Department of Planning and Environment to develop a Public Domain Plan and Development Control Plan (DCP) for the Cessnock Commercial Precinct (CCP). The Public Domain Plan and DCP compliments the Lower Hunter Regional Strategy's vision of Cessnock Commercial Precinct as a major regional centre and completes actions identified in Cessnock City Council's Cessnock CBD Masterplan 2012 (Arup).

Council engaged a team of consultants under Architects and Urban Designers, CM+, to develop a Public Domain Plan and DCP to revitalise the streets, parks, and public spaces within the Cessnock Commercial Precinct. The plans will address zoning, heights, character, and accessibility within the precinct. The public domain is defined as the publicly owned spaces, including streets and places such as parks, plazas, and squares. It can be thought of as the shared public spaces between buildings. The public domain is also considered to include publicly accessible privately owned spaces including arcades, building forecourts and other semi-public spaces.

Figure 1 Cessnock commercial precinct



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1.2. Consultation approach

A broad range of consultation methods were used to facilitate engagement from a wide range of age groups, genders, services, resident and business representatives, and workers. This consultation built upon consultation completed to inform the Cessnock CBD Masterplan (2012). Engagement delivered included the Cessnock Commercial Precinct page on Council's website; a survey (completed online and through intercept surveys with residents and business); 2 community workshops; a staff workshop; and an online community map accessible through the project webpage (see image below).

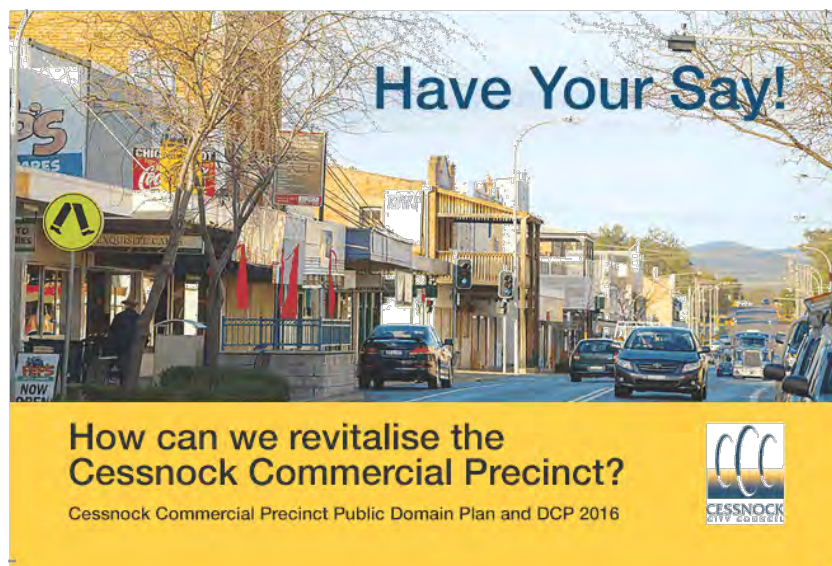
Figure 2 Image of interactive community map showing comments



Consultation opportunities were promoted widely through:

- The Cessnock Commercial Precinct webpage
- Council's Facebook page
- A Project Postcard (see image below)
- A letter to key stakeholders, and
- Notification in Council's business e-newsletter.

Figure 3 Project postcard promoting consultation opportunities



1.3. Summary of consultation completed

Consultation completed for this project is outlined below.

Consultation method	Details	Results
Online survey	<p>The online survey was live from 10 June 2016 to 13 July 2016.</p> <p>It was promoted through the project page on Council's website, postcards delivered to Council facilities and businesses in the CCP, social media posts and an advertisement in the Cessnock Business e-newsletter.</p> <p>Hard copy surveys were available with the community map at Cessnock Library.</p>	<p>87 surveys completed.</p> <ul style="list-style-type: none"> 46 respondents (52.9%) lived in the Cessnock LGA 37 respondents (42.5%) owned/managed a business in the CCP
Intercept surveys and business doorknocking	<p>Intercept surveys with residents and visitors conducted at the Funky Junk markets on 18 June 2016.</p> <p>Business surveys conducted on 17 June and 28 June 2016.</p>	

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Consultation method	Details	Results
Community map	<p>The interactive community mapping tool was used as an engagement tool to identify key places where improvement is required and to collect place based ideas.</p> <p>Comments on the Community Map were also collected through intercept surveying at the Funky Junk markets on 18 June 2016, and at the Hunter Valley Wine Country Business Networking Event on Tuesday 28 June.</p> <p>A floor map was displayed in Cessnock City Council administration building and Cessnock Library for public comments and these comments were added to the interactive community map.</p>	135 comments and 46 'likes' received
Community workshops	<p>2 community workshops were held to provide opportunities for residents and businesses to provide qualitative input into the development of the plans. The first workshop held in the evening (28 June 2016) had 1 participant and a 2nd business workshop held in the morning (12 July 2016) attracted 28 participants.</p> <p>The workshops were promoted through the project page on Council website, postcards delivered to Council facilities and businesses in the CCP, social media posts and advertisement in the Cessnock Business e-newsletter.</p> <p>A large floor map of the CCP was used as an engagement tool to identify issues and opportunities with the CCP (see image below).</p>	29 attendees across both community workshops
Staff workshop	A workshop with Council staff from across Council's departments was held on 28 June 2016.	12 attendees

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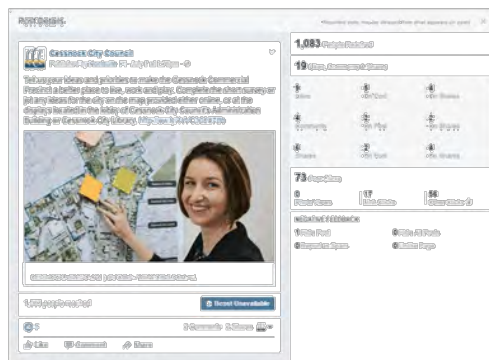
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Council staff adding their comments to the floor map at the staff workshop



Our survey team at the Funky Junk Markets surveying residents and visitors



The printed community map in Council's Library and promoted on Council's Facebook page



A comment on the interactive community map on the project page

2. Summary of key findings

This section provides a summary of the key findings across all engagement types.

Land use

Priorities identified in relation to future land use include increased tourist and visitor accommodation and more diverse retail. The survey indicated a preference toward no housing (39.3%) or medium density housing (36.1%) in the commercial precinct. However, the community map and community workshops indicated that some stakeholders consider there is a benefit to increased housing in the CCP. When asked specifically about affordable housing, 50.8% of survey respondents indicated support for affordable housing in the CCP.

58.8% of business owners survey indicated that tourist and visitor accommodation and services would improve trade. 52.4% of residents surveyed thought there should be more retail (but 42.9% thought there was sufficient retail). Of the people who thought that there was sufficient retail, a concern was filling currently vacant shops (4 people, 14.8%). The main suggested improvement to the overall shopping experience in the survey was more variety/more shops (8 people, 34.8%).

Building height

Across all consultation types, there were only a few comments regarding building height and the views of respondents differed. In the community workshop, "good current height" was given as a strength of the CCP, while "can't go high" was given as a weakness. "Keeping the country feel – not too high" but "increasing the height limit to 3 or 5 storeys" were also objectives suggested.

Character

Feedback regarding the character of the CCP focused on preserving the heritage character (particularly on Vincent St) and providing more activity and vibrancy including at night. In the staff workshop there was a recurring theme of preserving and promoting heritage, both mining and Indigenous. *"More activity and vibrancy in the town centre with cafes, wine bars, outdoor eating, live music and events"* was seen as important with 42.5% of survey respondents identifying this as a priority.

Access and transport

Parking and pedestrian and cyclist access and connectivity were the main priorities identified in access and transport.

Parking was identified as an issue across all consultation methods. 44.8% of survey respondents indicated that *"upgraded carparks and improved links to them"* was a top priority. For business owners/managers,

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upgraded carparks was the top priority (22 people, 59.5%), and "more parking" was the top improvement identified to improve trading (22 people, 64.7%).

Improved pedestrian and cyclist access and connectivity was considered a priority particularly in relation to the layout of the shopping plaza and connectivity between shopping areas. While "walkability and pedestrian crossings" were identified as one of the strengths of the CCP (41.5% of survey respondents thought this was working well), the need to improve walkability was a recurring issue in comments. Closing Cooper Street was an improvement suggested by 8 people (16.3%).

Public domain

Key issues regarding the public domain were the provision of facilities (particularly bins and toilets); providing parks with recreation facilities including children's playgrounds; improving safety; providing something for young people to do; and improving the drainage channels.

34.5% of survey and 61.8% of business owners respondents identified "more facilities (bubblers, toilets, seats, shaded areas)" as a high priority. In the community map, "toilets" was the fifth most significant comment topic and the second most "liked" comment topic (5.2% of comments, 13% of likes).

The staff workshop highlighted the need for more places for people to gather and "stay" in the CCP. A lack of open space and a lack of tables and toilets was identified as an issue. Possible improvements to parks and plazas noted through the community map were: places for people to gather and sit and spend time (e.g. parklets); improvements/centralising of parks and promoting their use through infrastructure (chairs, tables, shade) and outdoor events as well as activating TAFE Park through outdoor events (performances/concerts).

Safety was a key concern identified through the survey. 29% of respondents stated that this was a priority to be addressed in the CCP. This included concerns about lighting at night (17 people, 27.4%). The needs for positive activities for young people to do was identified as a potential improvement for the public domain (6 people, 12.2%) and was the second most significant topic on the community map (7.4% +13% of likes).

Finally, the community workshops and staff workshop identified a need to improve the look and use of the drainage channels. 14.9% of survey respondents identified "covering the drainage channels and improving and greening the canals" as a top priority for improvement.

3. Online survey outcomes

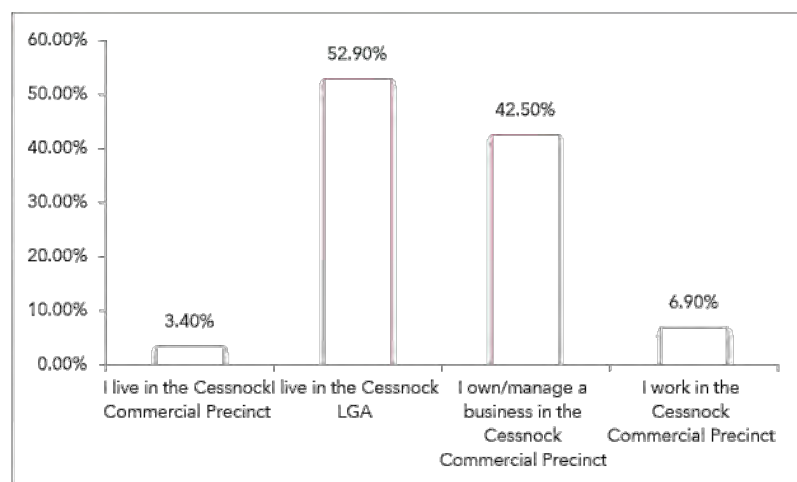
A total of 87 residents and businesses completed the online survey. This section provides a summary of the survey outcomes including comments.

3.1. Respondents' profile

There was a fairly even distribution between residents and business owners who completed the survey:

- 46 respondents (52.9%) lived in the Cessnock LGA
- 37 respondents (42.5%) owned/managed a business in the CCP, and
- 4 respondents (4.6%) were visitors to Cessnock

Figure 4 Survey respondent profile (source: CCP survey July 2016)



Place of residence

- 37 people or 50.7% of respondents lived in Cessnock
- 7 people or 9.6% lived in Bellbird, and
- People who did not live in the Cessnock LGA came from Sydney, Newcastle, the Central Coast and East Maitland.

Gender

- Slightly more females than males completed the survey (39 females, 52%; 36 males, 48%).

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Age

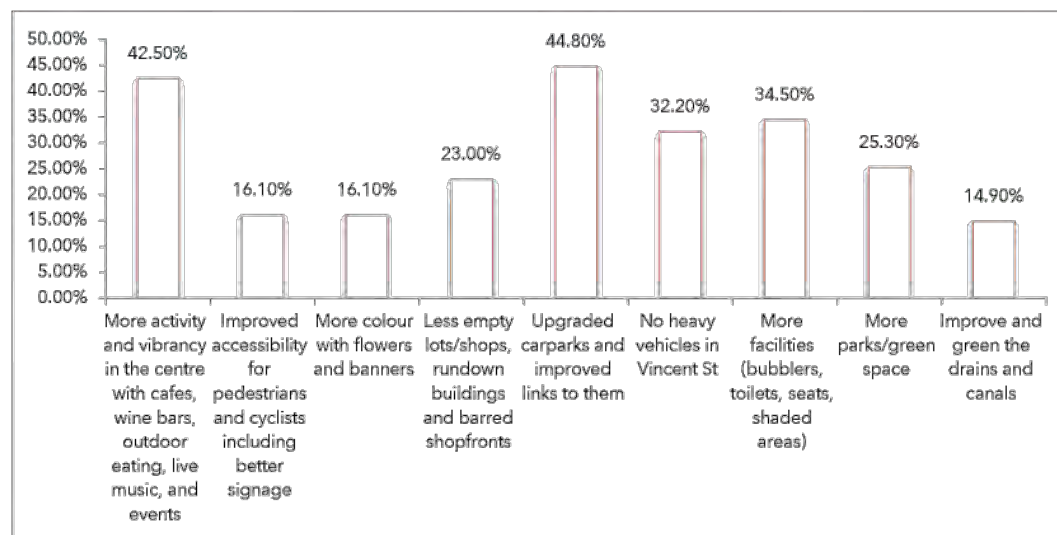
- The majority of respondents were aged 35 to 44 (21 people, 28.4%) and 45 to 54 (14 people, 18.9%)
- 12 young people completed the survey (3 people aged 17 or younger, 4%; 9 people aged 18 to 24, 12.2%) (compared to Cessnock LGA population: 9.6% aged 12 to 17, 8.5% aged 18 to 24)
- 9 older people completed the survey (8 people aged 65 to 74, 10.8%; 1 person aged 75+, 1.4%) (compared to Cessnock LGA population: 13.9% aged over 65), and
- Of male respondents, most were aged 35 to 44 (13 people, 37.1%). There was a more even spread of females across the age groups.

3.2. Priorities for the Cessnock commercial precinct

In 2012, Council engaged Arup to deliver a Masterplan for the Cessnock CBD. A number of priorities were identified as part of the Masterplanning process. As part of this 2016 survey, the previously identified priorities were tested for their continued relevance. The 2016 survey showed that all priorities identified through previous engagement are still well supported by respondents. The top priorities that respondents selected from the previous engagement were:

- Upgraded carparks and improved links to them (39 respondents, 44.8%)
- More activity and vibrancy in the town centre with cafes, wine bars, outdoor eating, live music and events (37 respondents, 42.5%)
- More facilities (bubblers, toilets, seats, shaded areas) (30 respondents, 34.5%), and
- No heavy vehicles in Vincent St (28 respondents, 32.2%).

Figure 5 Priorities for commercial precinct (source: CCP survey July 2016)



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There were some differences between genders:

- More parks/green space was a higher priority for females (14 females, 35.9%) compared to males (5 males, 13.9%), as was more facilities (18 females, 46.2%) compared to 10 males, 27.8%), and
- More activity and vibrancy was a higher priority for males (18 males, 50%) compared to females (15 females, 38.5%), as was no heavy vehicles in Vincent St (13 males, 36.1%) compared to females (9 females, 23.1%).

Other priorities for improvements given by respondents were:

- Pedestrian access including closing Cooper St (8 people, 16.3%)
"Cooper St closure would be good"
- Lighting and security concerns including cameras in the CCP (8 people, 16.3%)
"Security cameras to prevent crime"
- Business type changes including attracting larger specialty retailers, and there being too many cafes and employment agencies (8 people, 16.3%)
"Employment agency, op shop, empty shop and repeat"
"A lot of the government buildings take up empty spaces - the first thing people see as they drive through Cessnock. It gives it a bad reputation"
"Different sorts of shops, instead of a new café or eatery opening up every week!"
- Improve location of retail including "one big shopping centre" or encouraging externally facing businesses (6 people, 12.2%)
"Outward facing shopping centres instead of inwards facing 1970's design like the new Coles!!!"
- Something for young people to do (6 people, 12.2%)
"Something different like a games arcade"
"Keep kids out of trouble"
- Provide activities which encourage people to stay including events, night time activations, community stages (5 people, 10.2%)
"Exciting night time lighting and explicit street life entertainment"
"Community spaces e.g. things like performance spaces, permanent staging"
"We need all-weather entertainment!"

3.3. What's working well in the commercial precinct?

Respondents thought that "walkability and pedestrian crossings" was working well in the CCP (22 people, 41.5%). Other things that respondents thought were working well were:

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- Lighting at night (13 people, 24.5%)
- Parks and plazas (12 people, 22.6%) (however, respondents may have interpreted "plazas" as the three major shopping plazas rather than public spaces)
- Parking (8 people, 15.1%)
- Seating (8 people, 15.1%)
- Signage and wayfinding (8 people, 15.1%)
- Other (14 people, 26.4%)
 - Cafes (5 people, 9.4%)
 - Less heavy traffic on Vincent St (4 people, 7.5%)

Comments included:

"Festivals in main street are good, bring a lot of people in e.g. postie bike festival"

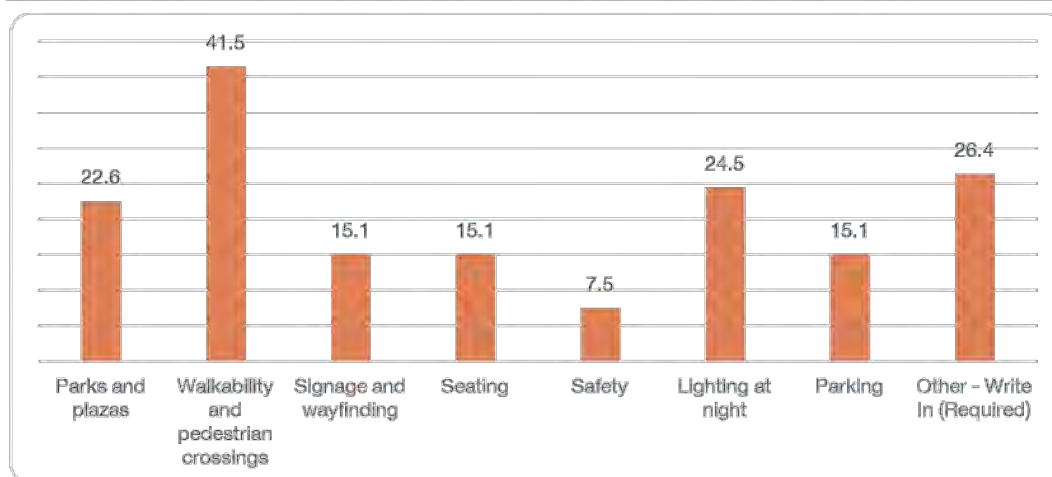
"It's pumping. Shopping plazas and parks are great"

"The snap fitness plaza is nice" "Yoghurtland has been good"

"Parks and walkways on Vincent St are well done. They are lacking everywhere else"

"Laneway projects creating appetite for street ambience may flower if quality increases sufficiently to give "the right" businesses confidence to enter the fray"

Figure 6 What is working well in the commercial precinct? (source: CCP survey July 2016)



3.4. What's not working well in the commercial precinct?

Parking was the key issue that people felt was not working well in the CCP (38 people, 61.3%).

"Parking at Woolworths and Coles/Target shopping centres is an absolute nightmare"

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"No places for couriers"

"Parking is difficult to find for visitors due to lack of signage"

This was followed by:

- Safety (18 people, 29%)

"[After the store closes] we run from here to the car park. [People] used to sit along the strip for the free Wifi"

"At night we are the only place open. I'm needing to keep multiple workers in shop for safety. The carpark is really dangerous, because of no lighting, people staying and drinking and drugs"

"Gangs of scooter kids steal from store"

"Vandalism has gone down but still present. Kids hang out for free Wifi"

- Lighting at night (17 people, 27.4%)

"I'm the only store with a street light"

"We start at 4:30am, there's not enough lights"

- Parks and plazas (16 people, 25.8%)

"We need a central civic precinct in the TAFE grounds, outdoor gym, play equipment, bbqs and full picnic facilities"

Comments included:

"I get stopped in the streets and asked directions because visitors don't know where to go."

"Footpaths are too often used by hooligans on skateboards and bikes etc"

"The roads are bad to ride on a bicycle"

"There are no public bathrooms anywhere in Vincent St"

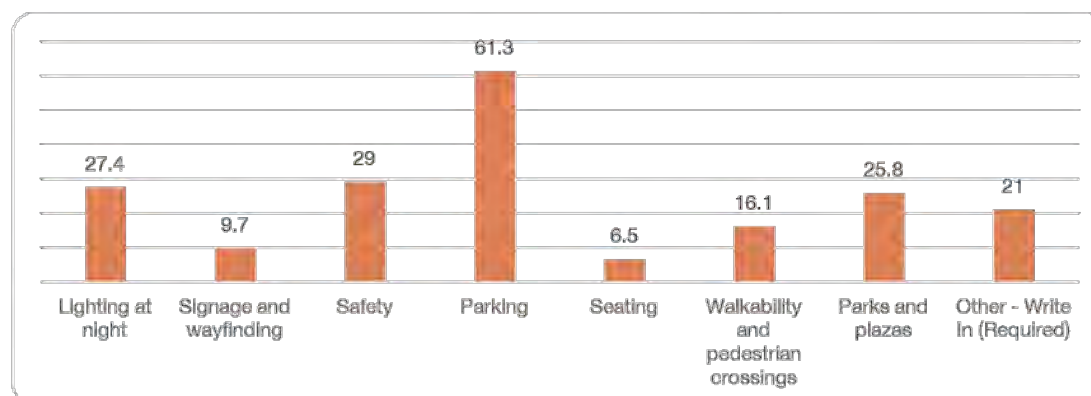
"Have to cross three major roads to go from Coles to Big W, it's hard with kids and prams"

"Too spread out for older people"

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Figure 7 What's not working well in the commercial precinct? (source: CCP survey July 2016)



3.5. Housing in the commercial precinct

Types of housing in the CCP

In relation to the housing preferences for the CCP, the most common response was "No housing" (24 people or 39.3%), followed closely by "Medium density residential apartment buildings" (22 people or 36.1%).

Comments on this question provided more detail:

- There were concerns that housing in the CBD would attract social and security issues (7 people, 17.5%)

"No dodgy public housing/boarding houses that will only attract undesirables"

"You have to look at the quality of the people you're appealing to, e.g. that they're not selling drugs, ice"

- Some people didn't think that the CBD was the place for housing - that housing and commercial uses should be separate (6 people, 15%)

"Good to differentiate between commercial and housing, be able to go home and away from commerce"

- Others felt that there was just not a need for housing in the CCP (3 people, 7.5%)

"It depends whether people would live there. It's not that hard to just come in to the centre."

- Others felt that it was an opportunity for more affordable housing including housing for older people, smaller apartments for singles (3 people, 7.5%)

"More community housing for my family"

There were some differences between genders:

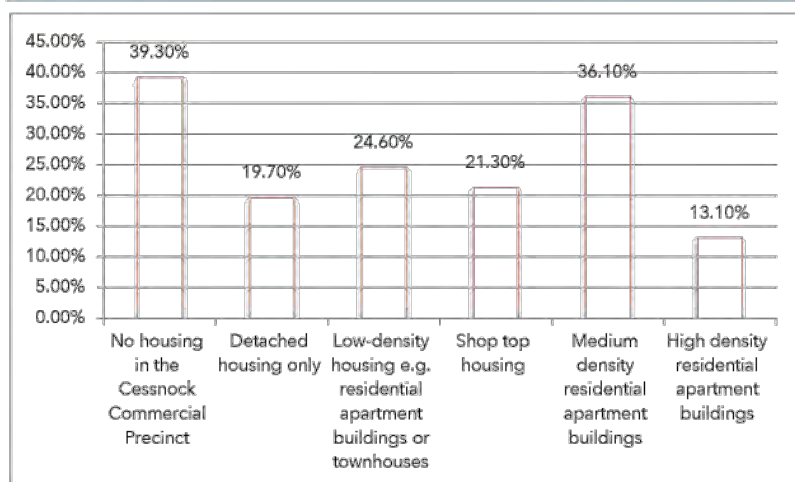
- Females in general did not want to see housing in the CCP (17 people, 54.8%)

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- Males in general selected "medium density residential apartments (13 people, 46.4%)

Figure 8 What type of housing would you like to see in the commercial precinct? (source: CCP survey July 2016)?



Affordable housing in the commercial precinct

A similar number of people thought that there should/shouldn't be more affordable housing in the CCP (more affordable housing: 31 people, 49.2%; no more affordable housing: 32 people, 50.8%).

- Nine people (14.2%) were concerned about the "types of people" that affordable housing might bring to the CCP (a stigma associated with the notion of "affordable housing").

"Housing commission people don't care about their houses"

"Will create social problems out the front of businesses"

"You have to have employed people in the housing"

- Eight people (12.7%) said that there was a general need for affordable housing.

"Not enough decent affordable housing for people that need it"

"There is always a need for affordable housing"

"Difficult for young people to enter private rental market"

- Five people (7.9%) said that particular types of housing that would be affordable were needed e.g. townhouses, apartments for older single people.

"Townhouses and units will be relatively affordable"

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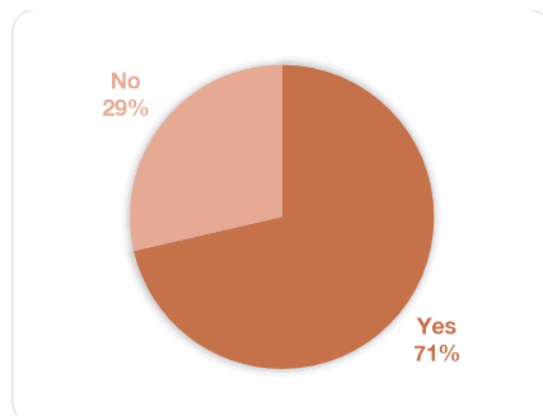
"Personal experience – I am currently homeless. There are no one bedroom flats. It's not the price but the type of place"

All respondents who work in the CCP (4 respondents, 100%), did not think that there was a need to increase the affordable housing in the CCP.

3.6. Tourist and visitor accommodation and services

Fifty people or 71.4% of respondents thought that there was a need for increased tourist and visitor accommodation and services in the CCP. 20 people or 28.6% disagreed.

Figure 9 Is there a need for increased tourist and visitor accommodation and services in the commercial precinct? (source: CCP survey July 2016)



- 13 people (18.6%) thought that there was a need for accommodation in the CCP in general.

"Don't have much here - little motels and stuff", "I get a lot of tourists asking where they can get accommodation"

- 10 people (14.3%) thought that there was a need for a tourist information centre in the CCP or on the approach to the town.

"Need for tourism info centre in the heart of town. We have to answer a lot of questions."

"The tourist info centre moved, it's outside of town now. We need one as an entry, direct people to coffee shops etc"

- 7 people (10%) thought that tourism could bring benefits to Cessnock including more jobs, economic benefits, and more people around.

"Definitely would help out businesses", "I think it will make the area more lively, more people around"

- 6 people (8.6%) thought that there was a need for activities and services that attract tourists, such as restaurants and cafes opening on weekends, events like the Stomp! festival, and facilities like toilets.

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"Restaurants in town keeps people wanting to come back to the area after they go to events. Toilets are good for if buses stop."

"Should have "crazy days" again, as it worked really well for us There was a really good food and wine festival which helped business Let's have a few days like that"

"Information centre, markets, using vacant spot for interesting fun events that bring tourism. Making Cessnock a stop along the way"

- 5 people (7.1%) thought that there was a need for accommodation in the CCP to cater to the major festivals and events.

"More concerts and not enough accommodation for the people"

- 3 people (4.3%) thought that there was a need for more signage in Cessnock e.g. to cafes, toilets and parking.

"Talking to many tourists, they comment that due to lack of signage they have no idea what cafes, restaurants and shops are in Cessnock & can't see easy parking so they keep driving."

However:

- 6 people (8.6%) thought that there was enough accommodation in the CCP
- 5 people (7.1%) thought that there would not be demand for accommodation in Cessnock (and that people would rather stay out in the vineyards)

"Everyone comes for the vineyards so stay there"

"Tourists want a view of grass and kangaroos and that's not here"

- 3 people (4.3%) thought there was a need for improved quality of existing accommodation.

"Need a much better hotel, have the Hunter nearby though. Maybe just spruce up the ones that are here"

"Use procedures to keep people accountable e.g. inspections"

All respondents who work in the CCP (5 respondents, 100%), and all respondents who live in the CCP (2 respondents, 100%) thought that there is a need for increased visitor and tourist accommodation and services in the CCP.

3.7. Retail businesses in the commercial precinct

52.4% of respondents (or 33 people) thought that there should be more retail in the CCP and 42.9% (27 people) thought that there was sufficient retail.

Of the people who thought that there was sufficient retail, a key concern was filling currently vacant shops (4 people, 14.8%). Across all respondents, common concerns and wishes were:

- Fill empty shops (10 people, 15.9%)
- White goods/furniture/electrical store (6 people, 9.5%)

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- Too many cafes (4 people, 6.3%)
- Need more variety, speciality stores (4 people, 6.3%)
- Rents are too high so it is hard for new retail to survive (3 people, 4.8%), and
- The layout of existing retail is poor (3 people, 4.8%).

3.8. Business survey

Through door knocking, local businesses were asked questions relating to their business activities and priorities for the CCP. 37 respondents owned/managed a business in the CCP including

- 9 specialty retail businesses (28.1%)
- 8 service retail businesses (25%)
- 7 clubs/cafes/restaurants (21.9%)
- 4 charity stores/pawn shops/variety stores/small personal loan services (12.5%)
- 2 food stores (6.3%)
- 1 accommodation service (3.1%), and
- 1 church (3.1%).

While 13 businesses preferred not to answer, there were:

- 8 respondents with an annual turnover of over \$500,000
- 4 respondents with an annual turnover of \$200,000 to \$500,000
- 2 respondents with an annual turnover of \$150,000 to \$200,000
- 3 respondents with an annual turnover of \$100,000 to \$150,000, and
- 3 respondents with an annual turnover of \$50,000 to \$100,000.

Most businesses had 1-2 full time employees:

- 12 respondents (32.4%) had 2 full time employees, and
- 10 respondents (27%) had 1 full time employee.

There was a number of long term business operators (11 businesses or 31.4% operating in the CCP for more than 20 years) as well as more recent businesses (10 businesses or 28.6% operating in the CCP for 1 to 5 years).

Most businesses planned to continue operating as currently (29 businesses, 82.9%).

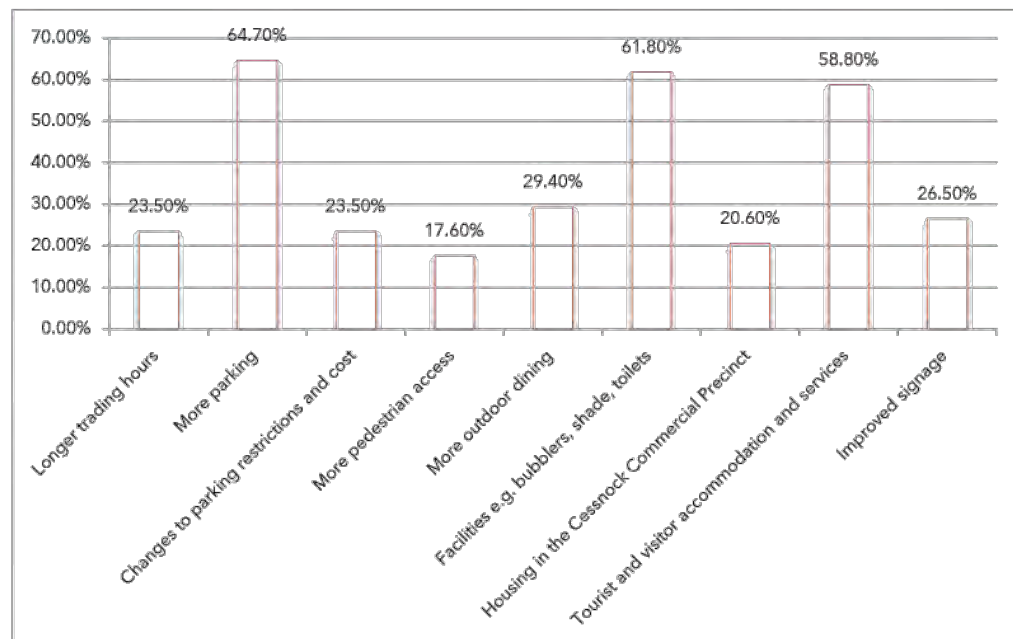
Most businesses rated their current trading as better/improving compared to the past two years (14 businesses, 48.3%). However, 10 businesses (34.5%) rated their current trading as worse/down on the past two years. 4 businesses (13.8%) rated their current trading as the same as the past two years.

3.8.1. Improvements to the commercial precinct

The top things that business owners/managers thought would improve trading were:

- More parking (22 people, 64.7%)
 - 3 comments talked about more parking spaces
"Elderly people need parking", "An hour restriction is fine but need more"
- Facilities such as bubblers, shade, toilets (21 people, 61.8%)
 - 5 comments suggested having toilets
"People come in and ask to use ours", "Put toilets next to the gallery", "Public toilets and bins in the car park"
 - 3 comments suggested bins
- Tourist and visitor accommodation and services (20 people, 58.8%)
 - 3 comments suggested a tourism information service in the CCP.
"Share the tourism more. Promote Cessnock CBD, tourist information centre, buses"

Figure 10 What activities could improve trading? (source: CCP survey July 2016)



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3.8.2. Businesses/services missing from the commercial precinct

Business owners thought that the businesses/services missing from the CCP were:

- Fashion stores including plus size stores and small fashion retailers (7 people, 31.8%)
- Electrical/white goods/furniture stores (6 people, 27.3%)
- Food options including fine dining, sushi (5 people, 22.5%)
- Businesses to attract tourists e.g. markets, boutiques (5 people, 22.7%)
- Kmart (3 people, 13.6%), and
- Activities for young people (2 people, 9.1%).

Comments included:

"Hospitality services e.g. wine bars. Anything that can link vineyard industry to local industry."

3.8.3. Businesses with turnover over \$500,000

Eight businesses reported annual turnover over \$500,000. Three of these businesses (60%) were specialty retail businesses e.g. pharmacies, one was a club/café/restaurant, and one was a service retail business.

Key trends from this group

- The top priorities were:
 - Upgraded carpark and improved links to them (6 businesses, 75%)
 - Improved accessibility for pedestrians and cyclists including better signage (3 businesses, 37.5%)
 - More facilities e.g. bubblers, toilets, seats, shaded areas (3 businesses, 37.5%)
- Things that these business owners/managers thought were not working well in the Cessnock precinct were:
 - Parking (4 businesses, 80%)
 - Walkability and pedestrian crossings (3 businesses, 60%)
 - Parks and plazas (3 businesses, 60%)
- 4 of these businesses (66.7%) thought that there should be no housing in the CCP, each for different reasons (concerns about the "type of people" in the housing, there is enough housing nearby, commercial and residential should be separate)
- 5 of these businesses (71.4%) thought that there should not be increased affordable housing in the CCP
- 6 of these businesses (75%) thought that there should be increased tourist and visitor and accommodation services in the CCP
- 5 of these businesses (71.4%) thought that there should be more retail in the CCP
- Things that could be improved in the CCP to improve trading were:
 - More parking (7 businesses, 87.5%)

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- Tourist and visitor accommodation and services (6 businesses, 75%)
- Facilities (e.g. bubblers, shade, toilets) (5 businesses, 62.5%)
- 3 businesses thought that clothes retailers were missing in the CCP (42.9%)

3.9. Non-business survey

This section looks at the responses to survey questions for people who live in, work in or are visitors to the Cessnock, but do not own businesses.

3.9.1. Visiting patterns

The majority of respondents visit the CCP two to three times per week (19 people, 55.9%). Ten people (29.4%) visit the CCP every day.

People stay one to two hours (15 people, 45.5%) or thirty minutes to an hour (10 people, 30.3%) (25 people, 75.8% staying less than two hours).

In terms of how much people spent in a single visit to the CCP:

- 11 people (28.9%) spent \$100 to \$199
- 8 people (21.2%) spent \$50 to \$99
- 8 people (21.1%) spent \$20 to \$49
- 6 people (15.8%) spent \$0 to \$19, and
- 4 people (10.5%) spent \$200 to \$499.

Young people spent less money in the CCP, with all young respondents spending \$0 to \$49 on an average visit.

3.9.2. Shops and services used

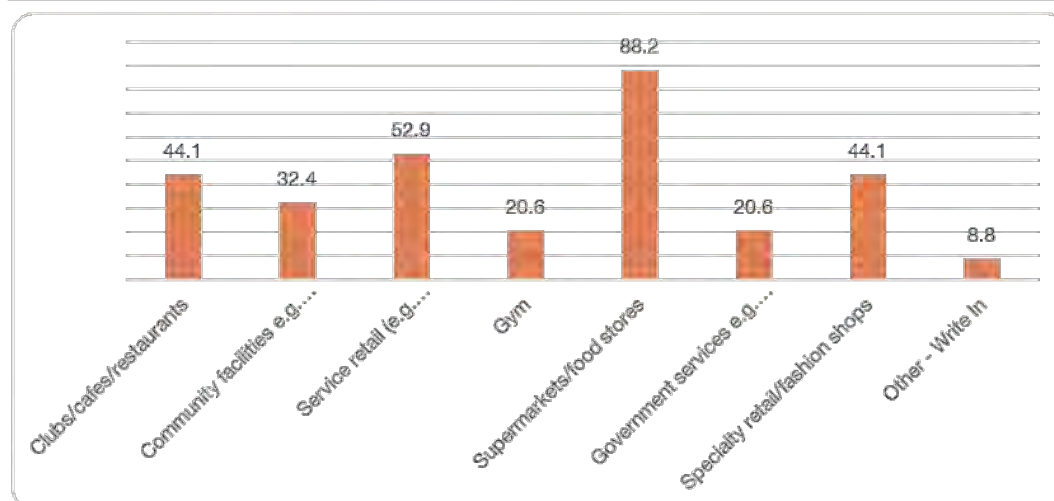
The most popular shops and services used were:

- Supermarkets/food stores (30 people, 88.2%)
- Service retail (e.g. hairdresser, banks, pharmacy) (18 people, 52.9%)
- Specialty retail/fashion shops (15 people, 44.1%), and
- Clubs/cafes/restaurants (15 people, 44.1%).

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Figure 11 When you visit the commercial precinct, which shops and services do you generally use?
(source: CCP survey July 2016)



3.9.3. Reasons for choosing the commercial precinct

The main reason that respondents chose the CCP for shopping and to access services over other town centres was because it is the closest centre to their home (30 people, 85.3%).



3.9.4. Satisfaction with shopping

The majority of people were neutral or satisfied with the overall shopping experience in the CCP:

- 42.1% (16 people) were satisfied
- 23.7% (9 people) were neither satisfied or dissatisfied

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Only 2 people (5.3%) were very satisfied, and 10 people (26.3%) were unsatisfied or very unsatisfied.

Reasons for dissatisfaction included:

- Lack of variety (5 comments)

"There's not much of a range compared to Maitland."

- Layout (3 comments)

"The two separate plazas is a pain. Particularly when you have kids with you."

"Way too spread out. Can't get what you want in one area."

- Parking (2 comments)

"Very dissatisfied with their parking available to the public"

- Accessibility (2 comments)

"More pedestrian access to be able to walk between shops easier"

3.10. What could be improved?

The main suggested improvements were:

- More variety/more shops (8 people, 34.8%)
- Parking (7 people, 30.4%)

Other suggestions were:

- Bigger name shops (3 people, 13%)
- Toilets (3 people, 13%)
- Pedestrian access (3 people, 13%)
- Activities other than shopping (2 people, 8.7%)
- Playground (2 people, 8.7%)
- Outdoor dining (2 people, 8.7%), and
- Parks and green space (2 people, 8.7%)

3.11. Missing employment/shops

The most common thing that people said was missing in the CCP was particular types of cafes/restaurants/fast food options, and variety in the food options (8 people, 32%), followed by:

- Furniture/Electrical/White goods stores e.g. Harvey Norman (7 people, 28%)
- Kmart (4 people, 16%)
- JB HiFi (4 people, 16%)

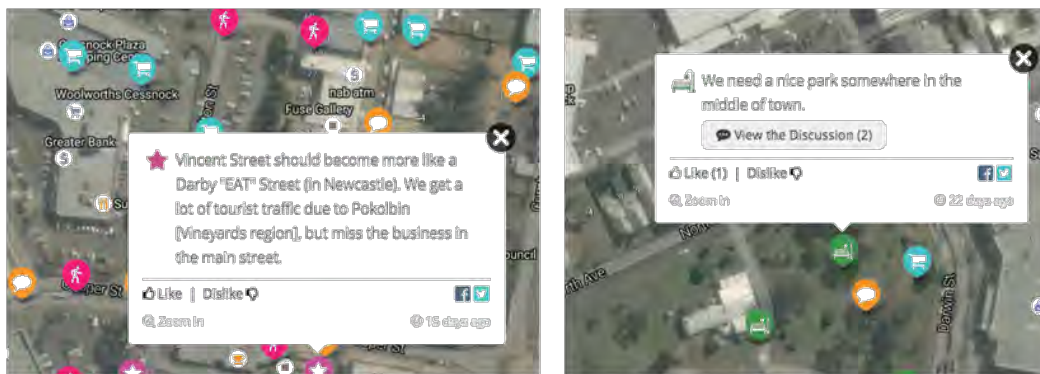
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- Fresh food stores (3 people, 12%)
- Music store/venues (3 people, 12%)
- Cinema (3 people, 12%), and
- Homewares (3 people, 12%).

4. Community map outcomes

This section provides a summary of the outcomes from the interactive community map. The interactive community map was accessible through the Cessnock Commercial Precinct webpage on Council's website. It enabled the community to leave comments that are location based on an interactive map, and for other community members to indicate their support or otherwise for the proposed idea.



Comments by community members added to the community map suggesting improvements to the commercial precinct

4.1. Summary of comments received

Respondents were invited to contribute their ideas for the CCP, choosing from six different comment types (Land Use, Building Height, Access and Transport, Character, Public Domain, Other Comments).

In total, 135 comments and 46 likes were received. The most popular comment types were:

- Access and Transport (39 comments, 17 likes)
- Land Use (30 comments , 9 likes)
- Public Domain (28 comments, 13 likes)
- Other Comments (19 comments, 7 likes)
- Character (17 comments, 0 likes), and
- Building Height (2 comments, 0 likes).

4.2. Comment topics

These comments were analysed and categorised into 19 topic areas based on common themes throughout the responses. Each topic was ranked based on the number of comments and likes it received and the top five topics of interest were identified, as shown in Table 1.

Table 1 Key comment topics from community map (source: CCP interactive commercial map July 2016)

Rank	Topic	Percentage of Total Comments	Percentage of Total Likes	Significance Score ¹
1	Parks and playgrounds	11.9	15.2	27.1
2	Activities for young people	7.4	13.0	20.4
3	Pedestrian and cyclist safety and connectivity	10.4	8.7	19.1
4	More shops	9.6	8.7	18.3
5	Toilets	5.2	13.0	18.2

The most comments received for any one topic were those relating to Parks and Playgrounds (using the Public Domain and Other Comments comment types). An example of such a comment is: *"We need a nice park somewhere in the middle of town."* This comment was located at TAFE Park and received two comments in agreement and one like.

Requests for more Activities for Young People were the second most popular and came from the comment types of Land Use, Public Domain or Other Comments, an example being: *"We need things for the youth to do that aren't going to cost families an arm and a leg."* This comment was tagged at the corner of Cooper St and Charlton St regarding the suggestion of a new cinema and received one like.

The topic of Pedestrian and Cyclist Safety and Connectivity received the third most comments, from the Access and Transport type. An example is: *"There is poor pedestrian and cycling connectivity,"* which was

¹ The significance score was calculated from the sum of the percentage of total comments and the percentage of total likes in order to rank the topics of most importance.

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referring to the area near the corner of Charlton St and Cooper St and received one supporting comment and one like.

The fourth most popular topic was requesting More Shops, from the Land Use, Character and Other Comments comment types. One example is: "More shops, more cafes, more restaurants on Vincent St with extended opening hours. I work 9-5, Mon- Fri so everything is shut when I need to go shopping," which was posted on Vincent St near the Hall St intersection.

The topic with the fifth most interest was Toilets, which fell under Public Domain. One comment asked for "Toilets in the park if people want to have concerts and stuff" and this was tagged at TAFE Park, receiving 2 comments in agreement and likes.

4.3. Key locations

Clusters of comments were also identified on the interactive community map and the key topics at these specific locations were identified.

Vincent St

Vincent St was identified as the most popular site, having received 32.6% of the total number of comments (43 comments). The key issue here was to Preserve and Develop the Character of Vincent St, making up 18.6% of the comments at this site (9 comments). This was a complex topic as there were nine comments in total and seven of these called for some form of development of the street, while two suggested preserving its heritage. One commenter suggested: "Vincent Street should become more like a Darby "EAT" Street (in Newcastle). We get a lot of tourist traffic due to Pokolbin [Vineyards region], but miss the business in the main street." Other topics on Vincent St, in order of popularity, were:

- More Shops, with 11.6% of comments for this site (5 comments)
- Toilets (4 comments, 9.3%), and
- Traffic and Road Maintenance (4 comments, 9.3%).

Charlton St shopping centres

The area around Charlton St shopping centres was the second most important site, with 23.7% of total comments (32 comments). Here the key issues were, in order of significance:

- Pedestrian and Cyclist Safety and Connectivity (8 comments, 25.0%)
- Improve and Centralise Parking (5 comments, 15.6%)
- More shops (4 comments, 12.5%)
- Activities for Young People (4 comments, 12.5%).

There were links between comments calling to improve parking, unify the shopping centres, and pedestrian safety and access:

"Fix up the crazy illogical shopping & parking & create a Town Square."

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"There's only one walkway between the two shopping centres. It's a puzzle to get around."

TAFE Park

TAFE Park was the third most significant site, with 10.4% of the overall comments (14 comments). The key issues here were:

- Parks and Playgrounds (9 comments, 64.3%)

"We need more green spaces throughout the city."

- Toilets (2 comments, 14.3%)

Bunnings site

The fourth most significant identified cluster was in the large open park area behind Bunnings, which made up 7.4% of overall comments. Here the key topics were:

- Activities for Young People/Families (4 comments, 40%)
- Parks and Playgrounds (3 comments, 30%), and
- More Shops (2 comments, 20.0%).

This site is seen as having significant potential for development, with one comment proposing: "Skating rink / Trampoline Park / Cinema / Family stuff that is not expensive so families in the area don't have to travel."

Cumberland St

Cumberland St had eight comments (5.9% of all comments). The key issues were (all with 2 comments, 25%):

- Improve and Centralise Parking
- Pedestrian and Cyclist Safety and Connectivity
- Housing with Views

"Cumberland Street is a good street for housing. It has nice views."

IGA carpark

The IGA carpark also had eight comments (5.9% of all comments). The major topics were (all with 2 comments, 25%):

- Improve and Centralise Parking
- Traffic and Road Maintenance

"Road markings not great & there is a mini raised roundabout which isn't clearly marked & I scraped the under part of my car."

5. Staff workshop outcomes

A workshop was conducted with Council staff on Tuesday 28 June. Workshop outcomes are provided below.



5.1. What are areas for potential improvements or rejuvenation?

Staff identified areas of the CCP with potential for improvement. Comments are categorised here by land use, building height, character, access and transport, and public domain.

Land Use

A major theme for improving utilisation of the CBD was creating a connection between the supermarkets and Big W for a more centralised shopping centre, potentially by closing traffic on Cooper St to create a pedestrian mall between Darwin and Vincent St. It was also suggested that improved housing quality and affordability would encourage development and visitors. It was suggested that use of Bridges Hill for recreation and fitness trails could be promoted through improved connection to the CBD. The creation of a community and cultural centre for tourists on the corner of Vincent and Snape St was also suggested.

Building height

There was limited mention of building height. There was one positive comment about the CCP as it is currently: *"Open, not closed in buildings and height."*

Character

A major desire was to improve outdoor spaces and encourage people to stay in the CBD. There was also a desire to increase night activity in a similar way to Newcastle by attracting visitors and promoting outdoor trading, small businesses and food trucks. Preserving and promoting heritage, both mining and

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indigenous, was also important. A suggested improvement was recuperating the past heritage listing. But there was also mention of the benefits that might result from relocating the pool site.

Access and transport

Parking was a significant theme, with calls for covered parking, more vegetation to make parked cars less visible and encouraging a more practical and safer layout. There were calls for development of parking at the cinema on Cumberland Carpark and redevelopment of the IGA Carpark on Charlton St. Pedestrian access was also an important theme, with suggestions to reduce street parking, widen footpaths, reduce traffic and create greener spaces to increase foot traffic and make it safer and more pleasant for pedestrians. Improving accessibility was seen as important for seniors and vulnerable individuals was also seen as well as improving cycling infrastructure and promoting access to mountain bike trails.

Public domain

Possible improvements identified were:

- Improve/centralise parks and promote use through infrastructure (chairs, tables, shade) and outdoor events
- Activating TAFE Park with outdoor events (Performances/concerts)
- Cover and redevelop the drainage canals
- Improve laneways and promote usage

5.2. What should be included in the DCP?

In groups, participants discussed the strengths and weaknesses of the CCP and what should be included in the DCP around each theme.

Access and transport

The CCP was seen as a positive space for pedestrians but there were issues with safety at crossings, public transport and cycling infrastructure. It was suggested that promoting laneway networks, improving cycling infrastructure, reducing parking visibility, reducing traffic and promoting shared access to the CCP were important focus areas.

Character and public domain

The CCP was seen as open with lots of parking but some of this was seen as unsafe. There was a lack of spaces to "stay" rather than "in and out" services, as well as no open space and a lack of tables and toilets. Suggestions were made to encourage outdoor trading by making allowances for outdoor activities in public places to encourage activity. It was also suggested that the project team should focus on places for people to gather and sit and spend time (e.g. parklets), focus on pedestrian activities and more "staying activities".

6. Community workshop outcomes

6.1. Community workshop 1

While the first community workshop was promoted widely (see section 1), only one resident attended. Feedback included that the Creative Arts Centre was seen as a positive cultural centre, and that the increased prison population was leading to unemployment in the area. Lions Park was seen as an area for improvement through infrastructure such as tables and chairs and the Council was seen as restricting night events and limiting potential for innovative events.

6.2. Community workshop 2

6.2.1. Key issues and opportunities

27 business representatives attended the second workshop. Each group of 5 or 6 participants identified their key issues and top opportunities.

Key issues

- Three of four groups identified "separate sub-precincts" as a key issue
- Three of four groups identified "significant on-grade parking preventing good urban amenity and active streets" as a key issue
- Two of four groups identified "drainage channels" as a key issue
- Other issues generated in the groups and identified as key issues were:
 - Pedestrian access/safety from car parks to retail spaces; and
 - Multi-level car parking.

Top opportunities

Three groups identified top opportunities:

- The top opportunity for all three groups was "create new urban streets, parks and plazas"
- "extend urban character of Vincent St west towards potential TAFE redevelopment and supermarkets" and "capitalise on opportunities arising from CBD bypass" were each identified by two groups as a second or third top opportunity
- Other opportunities generated in the groups and identified as top opportunities were:
 - Create unique character/perceptions – West End etc
 - Regional park hub TAFE park

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- Entertainment precinct North end Vincent St

6.2.2. Areas and ideas for improvements

Ideas for improvement to arcades, buildings and semi-public spaces included:

- Four comments regarding character/design improvements

"Improved design standards" and "Council subsidy for improvements"

- Two comments regarding blank walls

"Get rid of blank walls Wollombi Rd/Vincent St"

- Two comments regarding supermarkets

"Woollies carpark interface"

Ideas for improvements to streets, parks, plazas and squares included:

- Three groups suggests trees and beautification measures

"Roundabouts – beautify, plants, signage", "More artwork, quirky fixtures", "Entries to City precinct – use trees liquid amber, Jacaranda"

- Two groups suggested improvements to traffic flow

"Direction of traffic flow" and "Vincent St northbound only between Paul to Edward St. Control traffic to parking areas."

- Two groups suggested a need for a central park

"No town hall or central park – major public facility" and "Have a central place to build the CBD around (like a park in the TAFE grounds)"

- Two groups suggested improvements to signage and wayfinding

"Better signage, more uniform" and "Better defined walkways"

Areas with potential for improvement or rejuvenation were:

- Carparks in general (four groups)

"Cumberland St carparks behind PAC" and "Carpark/connections between shopping areas"

- IGA/Northern car park (three groups)

"Clean up, better access, undercover parking"

"Public space with rest/café etc – open up entertainment precinct"

- Wollombi Rd/Vincent St area (two groups)

- TAFE Park (three groups)

"TAFE site (with regional park)"

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"Civic Park @ TAFE precinct: duck pond? Green space? Art space? Redevelopment? Hotel/Motel?"

- Signage and wayfindings (three groups)

"More signage and direction"

- Green space with recreation facilities e.g. free exercise equipment, toilets, skate park, children's playground (three groups)

"Play area for children", "Walking paths, multi use trail, outdoor gym, create multi access points, skate park"

"More green space/parks/public toilets"

6.2.3. Strengths, weaknesses and actions

Each group identified the strengths and weaknesses of the CCP in the areas of land use, building height, character and public domain, and access and transport, and suggested objectives for the DCP in these areas (Table 2).

Table 2 Community workshop (business) strengths, weaknesses and actions

Topic	Strengths	Weaknesses	Objectives/Actions
Land Use		Lack of connection to wine country Carparking Accessibility Wasted space Business park undermines city	Concentrate city Signage/wayfinding Lower Council rates if buildings renovated
Building Height	Good current height	Can't go high	Increase height limit above 2 storeys (to 3 or 5 storeys) (2 groups) Keep the country feel - not too high Put residential behind the main street/above shops on main street
Character and Public Domain	Space to improve and beautify Heritage	Needs more charm, a theme Not very attractive Inconsistent standards Precincts and offerings not defined	Making Cessnock a unique and desirable destination Interactive facades Coal mining and pub theme Need country town feel but needs rejuvenating - green/colour → vibrancy

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Topic	Strengths	Weaknesses	Objectives/Actions
Access and Transport	<p>We have space to use</p> <p>We have infrastructure</p> <p>Pedestrian crossing @ Vincent and Cooper</p>	<p>Parking</p> <p>Traffic flow</p> <p>Trucks (too many)</p> <p>Connectivity and safety for pedestrians and cycling</p> <p>Need green space between main street and back of shops</p> <p>Need a loop transport e.g. Newcastle Tram</p> <p>No night time public transport</p> <p>Poor disabled access and access for prams</p>	<p>Need covered walkways between shops</p> <p>More parking and multistorey</p> <p>Block parts of Cooper St</p> <p>Subsidy at appropriate strategically located parking areas</p> <p>One-way Vincent Northbound - Directs traffic through town and to parking</p> <p>Free shuttle or hop on/hop off</p>



PLANNING PROPOSAL
Amendment to the
Cessnock Local Environmental Plan 2011

Cessnock Commercial Precinct

Version - 1.0

12 April 2017

Contact: Mr S J Christie
Strategic Land Use Planner

Telephone: 02 4993 4168

Email: scott.christie@cessnock.nsw.gov.au

Planning Proposal – Cessnock Commercial Precinct

File No. 18/2017/1/1

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PART 1: OBJECTIVES and OUTCOMES

The objectives of the proposed LEP are:

- To enable Cessnock City to grow as the strategic centre and major gateway to the vineyards district;
- To allow and encourage medium density mixed use development within the core of Cessnock City;
- To achieve a high standard of quality for future buildings within the city of Cessnock;
- To encourage best practice design at key designated sites strategically located within the city;
- To promote urban growth within the city of Cessnock that does not adversely impact its heritage character; and
- To make provision for a focal heart to the city of Cessnock through the introduction of a town square or plaza.

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PART 2: EXPLANATION of PROVISIONS

The objective of this Planning Proposal will be achieved by amending the Cessnock LEP 2011 in the following manner:

1. Rezone from:
 - B4 Mixed Use to B3 Commercial Core – Lot 22 DP 579269, Lot 4, 7 & 8 DP 38740, Lot 56 DP 614109, Lot 1 DP 112703, Lot 1 DP 721821, Lot 1 DP 979574, part of Lot 1 DP 1097308 and part of Lot 1 DP 340072;
 - RE1 Public Recreation to B4 Mixed Use – Lot 16 DP 48151 and Lot 261 DP 666805;
 - B3 Commercial Core to RE1 Public Recreation – Part of Lot 1 DP 450874, part of Lot 11 DP 613614, part of Lot 1 DP 1114018, part of Lot H DP 384894, part of Lot G DP 347929, part of Lot F DP 354733, part of Lot D DP 339311, part of Lot E DP 384895, part of Lot 2 DP 1114018, part of Lot 41 DP 617668, part of Lot 30 DP 1013326, part of Lot 2 DP 1013378, part of Lot 1 DP 1100097 and Lot 1 DP 1013378;
 - B4 Mixed Use to RE1 Public Recreation – part of Lot 1 DP 577260 and Lot 7009 DP 1030585.
2. Amend the Floor Space Ratio Map pertaining to land within the precinct from a Floor Space Ratio of 1.5:1 or no Floor Space Ratio to a Floor Space Ratio of 1.5:1, 2.0:1 and 2.5:1;
3. Amend the Height of Building Map pertaining to land within the precinct from a 12m or no Height of Building control to a Height of Building control of 12m, 15m, 18m and 21m;
4. Amend the Land Reserved for Acquisition Map to include Land Reserved for Acquisition within the precinct for the purposes of Classified Road (SP2) and Local Open Space (RE1);
5. Introduce a new clause to allow residential flat buildings in zone B3 Commercial Core;
6. Introduce a new clause to encourage design excellence on key sites identified within the Cessnock Commercial Precinct; and
7. Introduce Key Sites mapping in association with the introduction of the design excellence clause.

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PART 3: JUSTIFICATION

In accordance with the Department of Planning and Environment's "Guide to Preparing Planning Proposals", this section provides a response to the following issues:

- Section A: Need for Proposal;
- Section B: Relationship to Strategic Planning Framework;
- Section C: Environmental, Social and Economic Impact; and
- Section D: State and Commonwealth Interests

Section A: Need for Proposal

1 Resulting from a Strategic Study or Report

The planning proposal is the result of strategic investigations into Cessnock City's role as regionally significant strategic centre.

The strategic studies and reports leading to this planning proposal are:

- Hunter Regional Plan;
- Cessnock CBD Masterplan; and
- Cessnock Commercial Precinct Background Report.

Hunter Regional Plan

The Hunter Regional Plan identifies Cessnock City as a regionally significant strategic centre seeking concentrated growth in a mix of uses that will support economic and population growth.

More specifically, the Hunter Regional Plan prioritises the following for the Cessnock strategic centre:

- the investigation of opportunities to leverage the heritage character of the centre, and the growth of wine tourism in Pokolbin; and
- implementation of the Cessnock CBD masterplan.

Cessnock CBD Masterplan

Council adopted the Cessnock CBD Masterplan on 12 December 2012. The Masterplan outlines a strategy towards revitalising the Cessnock CBD through improved liveability, aesthetics and investment opportunities. It recommends urban infrastructure improvements such as key road realignments, improved pedestrian links and optimised public car parking, along with economic promoting initiatives such as higher densities along Vincent Street, and the promotion of weekend markets and events to draw new visitors to the CBD.

Cessnock Commercial Precinct Background Report

The Cessnock Commercial Precinct Background Report underpins a Development Control Plan and Public Domain Plan for the Cessnock Commercial Precinct that implements actions identified in the Cessnock CBD Masterplan. The report recommends detailed improvements to the Cessnock Commercial Precinct, including height of building controls, building setbacks, street activation and catalyst site land uses. These recommendations were informed by a review of development options for the precinct based on development supply, demand and feasibility.

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2 Planning Proposal as best way to achieve to objectives

The proposed zone, height, floor space ratio and local provisions are considered to be the most effective way of reinforcing Cessnock City's role as a major regional centre, allowing for feasible growth and density improvements that will not adversely impact the heritage character of the precinct.

The planning proposal will be complimented by a precinct specific Development Control Plan, Public Domain Plan and Section 94 Plan, all together providing a comprehensive suite of development guidelines and strategies to reinforce Cessnock as a major regional centre.

Residential flat buildings are proposed to be added to the list of permissible with consent land uses within Zone B3 Commercial Core (refer Appendix 2). This inclusion will be accompanied by a provision to prevent any residential flat building developments from being developed in isolation of an additional non-residential land use.

An incentive provision has been drafted to encourage catalyst developments at strategic locations (key sites) within the precinct. Accompanied by reduced car parking requirements in the Development Control Plan, the proposed floor space ratio and height of building incentives in the proposed design excellence clause (Appendix 2) would enable developers to achieve higher residential yields and reduced car parking requirements where they are willing to deliver a design that achieves design excellence, including a 5 Star Green Star rating.

It is noted that other Council's use a similar Design Excellence clause in their LEP, typically in conjunction with two other clauses for floor space ratio and / or height of building bonuses. Rather than introducing three new clauses to the Cessnock LEP, the design excellence clause (Appendix 2) proposed by this planning proposal incorporates floor space ratio and height of building bonuses within the same clause that is referenced on the Floor Space Ratio and Height of Building maps. This method is considered to be more user friendly, minimising the chance for confusion.

The key sites referenced in the design excellence clause have been selected on the basis that they present one or more of the following characteristics:

- They are strategically located to provide specific built form or public domain improvements; and / or
- They are larger and / or vacant sites that could feasibly be developed as a high quality mixed use development; and / or
- They can feasibly accommodate an increased yield or density in a way that does not undermine the character of the precinct; and / or
- They present opportunities as a marquee gateway development to the city centre

Potential improvements for the key sites include publicly accessible through site links, active frontages, centrally located height or a sensitive response to heritage items.

The design excellence clause requires development applications to be reviewed by a Design Review Panel containing experts in architecture and urban design, who will assess the quality of a proposed development and provide recommendations in relation to urban design, architecture and landscape architecture.

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Section B: Relationship to Strategic Planning Framework

3 Consistency with Objectives and Actions within Regional Strategies

Hunter Regional Plan

This planning proposal will contribute to Cessnock City's role as a strategic centre, providing for concentrated growth in a mix of uses that will support economic and population growth.

More specifically, the planning proposal will implement a series of actions identified in the Cessnock CBD masterplan and seek to leverage from the heritage character of the centre, and the growth of wine tourism in Pokolbin.

4 Consistency with Council's Community Strategic Plan or other Local Strategic Plan

Community Strategic Plan - Our People, Our Place, Our Future

Cessnock City Council's Community Strategic Plan (Our People, Our Place, Our Future), was developed by Council following extensive local community consultation. This planning proposal is consistent with the following objectives of the plan:

Objective 2.1 - Diversifying local business options:

- Our local government area is attractive and supportive of business.
- We have a diversity of business and industries across the local government area.
- Our planning controls provide for adequate industrial and commercial land.

Objective 2.2 - Achieving more sustainable employment opportunities:

- We have learning opportunities for people of all ages.
- We have employment opportunities in the local government area.

Objective 2.3 - Increasing tourism opportunities and visitation in the area:

- We have a range of diverse visitor experiences across the entire local government area.
- Our local government area is attractive to visitors.

Objective 3.2 - Better utilisation of existing open space:

- Our open spaces are distributed where people live.
- We have green corridors connecting our open space areas.
- Our open spaces have suitable amenities and plenty of shade.

Objective 4.1 – Better Transport Links

- We have access to a range of public and community transport within the local government area.
- We have access to a range of public and community transport beyond the local government area.

Objective 4.2 – Improving the Road Network

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- We have a high quality road network.

Cessnock City Wide Settlement Strategy (2010)

Following the release of the Lower Hunter Regional Strategy in 2006, the Cessnock City Wide Settlement Strategy 2003 was reviewed to coordinate with the Lower Hunter Regional Strategy forming what is now known as the Cessnock City Wide Settlement Strategy 2010.

The Cessnock City Wide Settlement Strategy identifies the city of Cessnock as the major regional centre for the LGA. This Planning proposal supports the Cessnock City Wide Settlement Strategy's vision for Cessnock.

5 Consistency with State Environmental Planning Policies

There are no existing or draft State Environmental Planning Policies (SEPPs) that prohibit or restrict the proposed development as outlined in this Planning Proposal. An assessment of relevant SEPPs against the planning proposal is provided in the table below.

Table 1: Relevant State Environmental Planning Policies

SEPP	Relevance	Consistency and Implications
SEPP 1 - Development Standards	The SEPP makes development standards more flexible. It allows councils to approve a development proposal that does not comply with a set standard where this can be shown to be unreasonable or unnecessary.	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 14 – Coastal Wetlands	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 15 - Rural Land Sharing Communities	The SEPP provides for multiple occupancy development, with council consent, in rural and non-urban zones, subject to a list of criteria in the policy.	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 19 – Bushland in Urban Areas	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 21 - Caravan Parks	The SEPP provides for development for caravan parks.	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 26 – Littoral Rainforests	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 29 – Western Sydney Recreation Area	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 30 - Intensive Agriculture	The SEPP provides considerations for consent for intensive agriculture.	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 32 - Urban Consolidation (Redevelopment of Urban Land)	The SEPP makes provision for the re-development of urban land suitable for multi-unit housing and related development.	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 33 -	The SEPP provides	Nothing in this Planning Proposal

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SEPP	Relevance	Consistency and Implications
Hazardous & Offensive Development	considerations for consent for hazardous & offensive development.	impacts upon the operation of this SEPP.
SEPP 36 - Manufactured Homes Estates	The SEPP makes provision to encourage manufactured homes estates through permitting this use where caravan parks are permitted and allowing subdivision.	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 39 – Spit Island Bird Habitat	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 44 - Koala Habitat Protection	This SEPP applies to land across NSW that is greater than 1 hectare and is not a National Park or Forestry Reserve. The SEPP encourages the conservation and management of natural vegetation areas that provide habitat for koalas to ensure permanent free-living populations will be maintained over their present range.	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 47 – Moore Park Showground	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 50 - Canal Estate Development	The SEPP bans new canal estates from the date of gazettal, to ensure coastal and aquatic environments are not affected by these developments.	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 52 – Farm Dams and Other works in Land and Water Management Plan Areas	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 55 - Remediation of Land	This SEPP applies to land across NSW and states that land must not be developed if it is unsuitable for a proposed use because of contamination	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 59 – Central Western Sydney Regional Open Space and Residential	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP 62 - Sustainable Aquaculture	The SEPP relates to development for aquaculture and to development arising from the rezoning of land and is of relevance for site specific rezoning proposals.	Nothing in this Planning Proposal impacts upon the operation of this SEPP.
SEPP 64 - Advertising and Signage	The SEPP aims to ensure that outdoor advertising is compatible with the desired amenity and visual character of an area,	Nothing in this Planning Proposal impacts upon the operation of this SEPP.

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SEPP	Relevance	Consistency and Implications
	provides effective communication in suitable locations and is of high quality design and finish.	
SEPP 65 - Design Quality of Residential Development	The SEPP relates to residential flat development across the state through the application of a series of design principles. Provides for the establishment of Design Review Panels to provide independent expert advice to councils on the merit of residential flat development.	This planning proposal is consistent with the aims and provisions of this SEPP. In addition, the planning proposal will introduce design excellence to key sites within the Cessnock Commercial Precinct that will act as catalysts for high quality design in residential flat buildings. Approvals for development on key sites will require review and approval from a design review panel involving 3 or more experts in architecture, urban design or landscape architecture.
SEPP 70 – Affordable Rental Housing (Revised Schemes)	The SEPP provides for an increase in the supply and diversity of affordable rental and social housing in NSW.	Nothing in this Planning Proposal affects the aims and provisions of this SEPP.
SEPP 71 – Coastal Protection	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP Affordable Rental Housing 2009	The aims of this Policy are as follows: (a) to provide a consistent planning regime for the provision of affordable rental housing, (b) to facilitate the effective delivery of new affordable rental housing by providing incentives by way of expanded zoning permissibility, floor space ratio bonuses and non-discretionary development standards, (c) to facilitate the retention and mitigate the loss of existing affordable rental housing, (d) to employ a balanced approach between obligations for retaining and mitigating the loss of existing affordable rental housing, and incentives for the development of new affordable rental housing, (e) to facilitate an expanded role for not-for-profit-providers of affordable rental housing, (f) to support local business	Nothing in this Planning Proposal affects the aims and provisions of this SEPP.

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SEPP	Relevance	Consistency and Implications
	centres by providing affordable rental housing for workers close to places of work, (g) to facilitate the development of housing for the homeless and other disadvantaged people who may require support services, including group homes and supportive accommodation.	
SEPP Building Sustainability Index: BASIX 2004	The SEPP provides for the implementation of BASIX throughout the State.	This planning proposal is consistent with the aims and provisions of this SEPP.
SEPP Exempt and Complying Development Codes 2008	The SEPP provides exempt and complying development codes that have State-wide application, identifying, in the General Exempt Development Code, types of development that are of minimal environmental impact that may be carried out without the need for development consent; and, in the General Housing Code, types of complying development that may be carried out in accordance with a complying development certificate.	Nothing in this Planning Proposal affects the aims and provisions of this SEPP.
SEPP Housing for Seniors or People with a Disability 2004	The SEPP aims to encourage provision of housing for seniors, including residential care facilities. The SEPP provides development standards.	Nothing in this Planning Proposal affects the aims and provisions of this SEPP.
SEPP Infrastructure 2007	The SEPP provides a consistent approach for infrastructure and the provision of services across NSW, and to support greater efficiency in the location of infrastructure and service facilities.	This planning proposal is consistent with the aims and provisions of this SEPP. Further, the planning proposal is supported by a public domain plan that provides a framework for delivering improvements to public domain infrastructure.
SEPP (Kosciuszko National Park – Alpine Resorts) 2007	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Kurnell Peninsula) 1989	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP Major Development 2005	The SEPP defines certain developments that are major projects to be assessed under Part 3A of the Environmental Planning and Assessment Act 1979 and determined by the	Nothing in this Planning Proposal affects the aims and provisions of this SEPP.

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SEPP	Relevance	Consistency and Implications
	Minister for Planning. It also provides planning provisions for State significant sites. In addition, the SEPP identifies the council consent authority functions that may be carried out by Joint Regional Planning Panels (JRPPs) and classes of regional development to be determined by JRPPs.	
SEPP Mining, Petroleum Production and Extractive Industries 2007	The SEPP aims to provide proper management of mineral, petroleum and extractive material resources and ESD.	Nothing in this Planning Proposal affects the aims and provisions of this SEPP.
SEPP Miscellaneous Consent Provisions 2007	The aims of this Policy are as follows: (a) to provide that the erection of temporary structures is permissible with consent across the State, (b) to ensure that suitable provision is made for ensuring the safety of persons using temporary structures, (c) to encourage the protection of the environment at the location, and in the vicinity, of temporary structures by specifying relevant matters for consideration, (d) to provide that development comprising the subdivision of land, the erection of a building or the demolition of a building, to the extent to which it does not already require development consent under another environmental planning instrument, cannot be carried out except with development consent.	Nothing in this Planning Proposal affects the aims and provisions of this SEPP.
SEPP Penrith Lakes Scheme 1989	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP Rural Lands 2008	The SEPP aims to facilitate economic use and development of rural lands, reduce land use conflicts and provides development principles.	Nothing in this Planning Proposal affects the aims and provisions of this SEPP.
SEPP 53 Transitional Provisions 2011	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>

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SEPP	Relevance	Consistency and Implications
SEPP State and Regional Development 2011	The SEPP aims to identify development and infrastructure that is State significant and confer functions on the Joint Regional Planning Panels (JRPPs) to determine development applications.	Nothing in this Planning Proposal affects the aims and provisions of this SEPP.
SEPP (Sydney Drinking Water Catchment 2011)	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP Sydney Region Growth Centres 2006	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Three Ports 2013	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Urban Renewal) 2010	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Western Sydney Employment Area) 2009	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>
SEPP (Western Sydney Parklands) 2009	<i>Not Applicable to LGA</i>	<i>Not Applicable to LGA</i>

6 Consistency with s.117 Ministerial Directions for Local Plan Making

An assessment of relevant s.117 Directions against the planning proposal is provided in the table below.

Table 2: Relevant s.117 Ministerial Directions

Ministerial Direction	Objective of Direction	Consistency and Implication
1. EMPLOYMENT AND RESOURCES		
a. Business and Industrial Zones	The objectives of this direction are to: (a) encourage employment growth in suitable locations, (b) protect employment land in business and industrial zones, and (c) support the viability of identified strategic centres.	This planning proposal has an inconsistency of minor significance to the objectives of this direction, proposing to rezone areas currently used for car parks and private open space from a business zone to RE1 Public Recreation creating a town square and public recreation area. The purpose of these areas being rezoned is to encourage pedestrian patronage within the commercial precinct of Cessnock. Overall, the planning proposal proposes to increase the availability and quality of built form

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Ministerial Direction	Objective of Direction	Consistency and Implication
		outcome on business zoned land within the Cessnock Commercial Precinct. Therefore, this planning proposal is considered to be consistent with the aims and provisions of this Ministerial Direction, encouraging employment growth and increasing the density of employment floor space in an identified strategic centre.
b. Rural Zones	The objective of this direction is to protect the agricultural production value of rural land.	Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
c. Mining, Petroleum Production and Extractive Industries	The objective of this direction is to ensure that the future extraction of State or regionally significant reserves coal, other minerals, petroleum and extractive materials are not compromised by inappropriate development.	. Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
d. Oyster Aquaculture	<i>The objectives of this direction are:</i> <i>(a) to ensure that Priority Oyster Aquaculture Areas and oyster aquaculture outside such an area are adequately considered when preparing a planning proposal,</i> <i>(b) to protect Priority Oyster Aquaculture Areas and oyster aquaculture outside such an area from land uses that may result in adverse impacts on water quality and consequently, on the health of oysters and oyster consumers.</i>	<i>Not Applicable to LGA</i>
e. Rural lands	The objectives of this direction are to: (a) protect the agricultural production value of rural land, (b) facilitate the orderly and economic development of rural lands for rural and related purposes.	Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
2. ENVIRONMENT AND HERITAGE		
a. Environmental	The objective of this direction is	Nothing in this Planning Proposal

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Ministerial Direction	Objective of Direction	Consistency and Implication
Protection Zones	to protect and conserve environmentally sensitive areas.	is inconsistent with the objectives of this direction.
b. Coastal Protection	<i>The objective of this direction is to implement the principles in the NSW Coastal Policy.</i>	<i>Not Applicable to LGA</i>
c. Heritage Conservation	The objective of this direction is to conserve items, areas, objects and places of environmental heritage significance and indigenous heritage significance.	Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
d. Recreation Vehicle Areas	The objective of this direction is to protect sensitive land or land with significant conservation values from adverse impacts from recreation vehicles.	Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
3. HOUSING, INFRASTRUCTURE AND URBAN DEVELOPMENT		
a. Residential Zones	The objectives of this direction are: (a) to encourage a variety and choice of housing types to provide for existing and future housing needs, (b) to make efficient use of existing infrastructure and services and ensure that new housing has appropriate access to infrastructure and services, and (c) to minimise the impact of residential development on the environment and resource lands.	This planning proposal is in support of the objectives of this Ministerial Direction. It encourages the development of residential flat buildings that will improve the availability of housing choice, while making efficient use of existing infrastructure and services.
b. Caravan parks and Manufactured Home Estates	The objectives of this direction are: (a) to provide for a variety of housing types, and (b) to provide opportunities for caravan parks and manufactured home estates.	The Planning Proposal does not seek to rezone land to provide for caravan parks or manufactured home estates, further there are no existing caravan parks within the study area. The Planning Proposal is considered to be consistent with this direction.
c. Home Occupations	The objective of this direction is to encourage the carrying out of low-impact small businesses in dwelling houses.	Home occupations are permitted without consent in the proposed B3 zone. Home occupations are not permitted on areas of land proposed to be zoned RE1. However, this land is currently

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Ministerial Direction	Objective of Direction	Consistency and Implication
		<p>used for car parking or recreational uses only. Overall, the planning proposal will provide a significant net increase in the floor space available for home occupation uses with increase densities in mixed use zones proposed by this planning proposal.</p> <p>The inconsistencies of this Planning Proposal with this direction are considered to be of minor significance, therefore the planning proposal is considered to be consistent with this direction.</p>
d. Integrating Land Use and Transport	<p>The objective of this direction is to ensure that urban structures, building forms, land use locations, development designs, subdivision and street layouts achieve the following planning objectives:</p> <p>(a) improving access to housing, jobs and services by walking, cycling and public transport, and</p> <p>(b) increasing the choice of available transport and reducing dependence on cars, and</p> <p>(c) reducing travel demand including the number of trips generated by development and the distances travelled, especially by car, and</p> <p>(d) supporting the efficient and viable operation of public transport services, and</p> <p>(e) providing for the efficient movement of freight.</p>	<p>This Planning Proposal supports the objectives of this direction, seeking to improve the transport network for vehicles, pedestrians and cyclists.</p> <p>This planning proposal aims to:</p> <ul style="list-style-type: none"> • Encourage pedestrian through site links at strategic locations within the city; • Enable a bypass route for vehicles travelling east/west and wishing to bypass Cessnock city; • Alleviate cumulative traffic impacts along Wollombi Road with the proposed city bypass route; • Reduce travel demand with the strategic location of higher urban densities in well serviced locations; • Reduce vehicle dependency by encouraging and prioritising pedestrian movement in the city; and • Improve the city's overall traffic network.
e. Development Near Licensed Aerodromes	<p>The objectives of this direction are:</p> <p>(a) to ensure the effective and safe operation of aerodromes, and</p> <p>(b) to ensure that their operation is not compromised by development that constitutes an obstruction, hazard or potential hazard to aircraft flying in the</p>	<p>Nothing in this Planning Proposal is inconsistent with the objectives of this direction.</p>

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Ministerial Direction	Objective of Direction	Consistency and Implication
	vicinity, and (c) to ensure development for residential purposes or human occupation, if situated on land within the Australian Noise Exposure Forecast (ANEF) contours of between 20 and 25, incorporates appropriate mitigation measures so that the development is not adversely affected by aircraft noise.	
f. Shooting Ranges	The objectives are: (a) to maintain appropriate levels of public safety and amenity when rezoning land adjacent to an existing shooting range, (b) to reduce land use conflict arising between existing shooting ranges and rezoning of adjacent land, (c) to identify issues that must be addressed when giving consideration to rezoning land adjacent to an existing shooting range.	Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
4. HAZARD AND RISK		
a. Acid Sulfate Soils	The objective of this direction is to avoid significant adverse environmental impacts from the use of land that has a probability of containing acid sulphate soils	Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
b. Mine Subsidence and Unstable Land	The objective of this direction is to prevent damage to life, property and the environment on land identified as unstable or potentially subject to mine subsidence.	The land is not within a proclaimed Mine Subsidence District. Consultation regarding mine subsidence and unstable land will be undertaken with the Mine Subsidence Board of NSW and Trade and Investment following the Gateway determination.
c. Flood Prone Land	The objectives of this direction are: (a) to ensure that development of flood prone land is consistent with the NSW Government's Flood Prone Land Policy and the principles of the Floodplain Development Manual 2005, and	This Planning Proposal is consistent with the objectives of this direction. A large portion of the land proposed to be rezoned from B3 or B4 to RE1 falls within the flood planning level, reducing the impacts of flood on that land.

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Ministerial Direction	Objective of Direction	Consistency and Implication
	(b) to ensure that the provisions of an LEP on flood prone land is commensurate with flood hazard and includes consideration of the potential flood impacts both on and off the subject land.	
d. Planning for Bushfire Protection	The objectives of this direction are: (a) to protect life, property and the environment from bush fire hazards, by discouraging the establishment of incompatible land uses in bush fire prone areas, and (b) to encourage sound management of bush fire prone areas.	Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
5. REGIONAL PLANNING		
a. Implementation of Regional Strategies	The objective of this direction is to give legal effect to the vision, land use strategy, policies, outcomes, and actions contained in regional strategies.	This Planning Proposal will reinforce Cessnock as Strategic Centre providing higher density commercial and residential development that supports but does not dominate or hinder the City from functioning as a higher order commercial / service centre.
b. Sydney Drinking Water Catchment	<i>The objective of this Direction is to protect water quality in the Sydney drinking water catchment.</i>	<i>Not Applicable to LGA</i>
c. Farmland of State and Regional Significance on the NSW Far North Coast	<i>The objectives of this direction are: (a) to ensure that the best agricultural land will be available for current and future generations to grow food and fibre, (b) to provide more certainty on the status of the best agricultural land, thereby assisting councils with their local strategic settlement planning, and (c) to reduce land use conflict arising between agricultural use and non-agricultural use of farmland as caused by urban encroachment into farming areas.</i>	<i>Not Applicable to LGA</i>
d. Commercial and Retail	<i>The objectives for managing commercial and retail</i>	<i>Not Applicable to LGA</i>

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Ministerial Direction	Objective of Direction	Consistency and Implication
<i>Development along the Pacific Highway, North Coast</i>	<i>development along the Pacific Highway are: (a) to protect the Pacific Highway's function, that is to operate as the North Coast's primary inter- and intra-regional road traffic route; (b) to prevent inappropriate development fronting the highway (c) to protect public expenditure invested in the Pacific Highway, (d) to protect and improve highway safety and highway efficiency, (e) to provide for the food, vehicle service and rest needs of travellers on the highway, and (f) to reinforce the role of retail and commercial development in town centres, where they can best serve the populations of the towns.</i>	
<i>e. Development in the vicinity of Ellalong, Paxton and Millfield (Cessnock LGA)</i>	<i>(Revoked 18 June 2010)</i>	<i>No longer applicable to the LGA.</i>
<i>f. Sydney to Canberra Corridor</i>	<i>(Revoked 10 July 2008. See amended Direction 5.1)</i>	<i>Not Applicable to LGA</i>
<i>g. Central Coast</i>	<i>(Revoked 10 July 2008. See amended Direction 5.1)</i>	<i>Not Applicable to LGA</i>
<i>h. Second Sydney Airport: Badgerys Creek</i>	<i>The objective of this direction is to avoid incompatible development in the vicinity of any future second Sydney Airport at Badgerys Creek.</i>	<i>Not Applicable to LGA</i>
<i>i. North West Rail Link Corridor Strategy</i>	<i>The objectives of this direction are to: (a) promote transit-oriented development and manage growth around the eight train stations of the North West Rail Link (NWRL) (b) ensure development within the NWRL corridor is consistent with the proposals set out in the NWRL Corridor Strategy</i>	<i>Not Applicable to LGA</i>

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Ministerial Direction	Objective of Direction	Consistency and Implication
	<i>and precinct Structure Plans.</i>	
6. LOCAL PLAN MAKING		
a. Approval and Referral Requirements	The objective of this direction is to ensure that LEP provisions encourage the efficient and appropriate assessment of development.	Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
b. Reserving Land for Public Purposes	The objectives of this direction are: (a) to facilitate the provision of public services and facilities by reserving land for public purposes, and (b) to facilitate the removal of reservations of land for public purposes where the land is no longer required for acquisition.	Part of this Planning Proposal seeks to rezone land for public purposes facilitating civic and recreational uses within the city. It also seeks to reserve land for future local and classified road purposes. Nothing in this Planning Proposal is inconsistent with the objectives of this direction.
c. Site Specific Provisions	The objective of this direction is to discourage unnecessarily restrictive site specific planning controls.	This Planning Proposal has inconsistencies of minor significance with this direction in that it introduces residential flat buildings as a permissible land use within B3 zoned land allowing residential flat building to be developed where they form a component of a mixed use development involving a permitted non-residential use. It also introduces site specific design excellence provisions to key sites within the Cessnock Commercial Precinct, necessary to incentivise catalyst developments in improving the quality and density of architecture and urban design in the precinct.
7. Metropolitan Planning		
a. Implementation of A Plan for Growing Sydney	<i>The objective of this direction is to give legal effect to the planning principles; directions; and priorities for subregions, strategic centres and transport gateways contained in A Plan for Growing Sydney.</i>	<i>Not Applicable to LGA</i>

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Section C: Environmental, Social and Economic Impact

7 Impact on Threatened Species

This Planning Proposal seeks to increase the density of development in urban areas already occupied with development and void of any significant vegetation. Consequently, the Planning Proposal would not result in the isolation, fragmentation or removal of any areas of habitat for significant or threatened species.

8 Environmental Impact

Traffic Network

There is a compilation of improvements proposed to the traffic network servicing Cessnock city and surrounds. These include the connection of:

- Wollombi Road to Aberdare Road south of the city centre;
- Allandale Road to Charlton Street; and
- Wollombi Road to Keen Street.

Some of the key benefits to these improvements outlined below.

Wollombi Road to Aberdare Road

It is proposed to connect Aberdare Road / South Avenue to James Street connecting the arterial road network of Aberdare Road and Wollombi Road. Benefits of this alignment include:

- Improved travel times for vehicles travelling to and / or from the east or west of Cessnock to bypass the city centre;
- Improved safety within the city centre by providing an alternate route for traffic using Cooper Street as a rat run; and
- Reduced pressure on Wollombi Road, north of James Street, in servicing future traffic travelling between the east and west of Cessnock city.

Allandale Road to Charlton Street

Benefits of this alignment include:

- Improved management of traffic travelling between the intersections of Allandale Road, Wollombi Road and Vincent Street. In particular, traffic travelling between Vincent Street and Allandale Road; and
- Significant improvements to the passive surveillance of areas behind buildings addressing Vincent Street, between Wollombi Road and Cooper Street.

Wollombi Road to Keen Street

This alignment involves a one way connection travelling south from Wollombi Road to the existing two way road of Keene Street. This alignment is required for bus routes when a shared zone is introduced to Cooper Street creating a town square in the centre of Cessnock.

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9 Social and Economic Impacts

Social and economic benefits of the Planning Proposal include:

- Short term construction employment for construction of the development;
- Provision of additional housing stock to meet demand;
- Providing housing within close proximity to the CBD, schools and infrastructure.

The Planning Proposal is not of a scale that will create any significant adverse social impacts.

Social Impact

The Cessnock Commercial Precinct is currently lacking a shady public place for locals or visitors to sit, linger, read or converse in a pleasant outdoor area. This planning proposal introduces a public town square in the heart of the Cessnock Commercial Precinct that will act as an anchor for pedestrians using the city and its services.

Other social benefits of the planning proposal include:

- Significant improvements to the passive surveillance of areas behind buildings addressing Vincent Street, between Wollombi Road and Cooper Street via the connection of Charlton Street to Allandale Road;
- Improved safety through passive surveillance via the introduction of additional residents within the commercial precinct; and
- Improved health benefits to existing and future residents of the precinct via the introduction of additional residents within walking distance of various services offered by the Cessnock Commercial Precinct.

Economic Impact

Retail or commercial centres are most successful when clustered in one location, creating a vibrant and active area. Currently, the Cessnock Commercial Precinct is stretched out along Vincent Street, with three major shopping centres to the north-west. This planning proposal will enable the construction of a public town square in an accessible location central to both Vincent Street and the three major shopping centres. The town square will enable people to linger and add vibrancy to the city, in turn generating additional demand for retail and adding to the vitality of the commercial precinct.

The Cessnock Commercial Precinct is currently oversupplied with employment floor space. While this planning proposal will allow for additional employment floor space within the precinct, the focus of this additional floor space is to provide opportunities to improve the quality of the floor space, and generate additional demand through increased resident population. Incentives to that affect are proposed through the proposed height of building controls, floor space ratio controls and a design excellence clause (Appendix 2).

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Section D: State and Commonwealth Interests

10 Adequate Public Infrastructure

While it is considered that there is suitable public infrastructure in the immediate vicinity of the site to support increased residential densities, consultation will be required with utility providers following the issue of a Gateway determination.

Augmentations will be required to existing infrastructure to ensure that future development is appropriately serviced.

11 Consultation with State and Commonwealth Authorities

Formal consultation with selected statutory agencies will be undertaken following, and as directed by, the Gateway determination. It is expected that this may include consultation with the following:

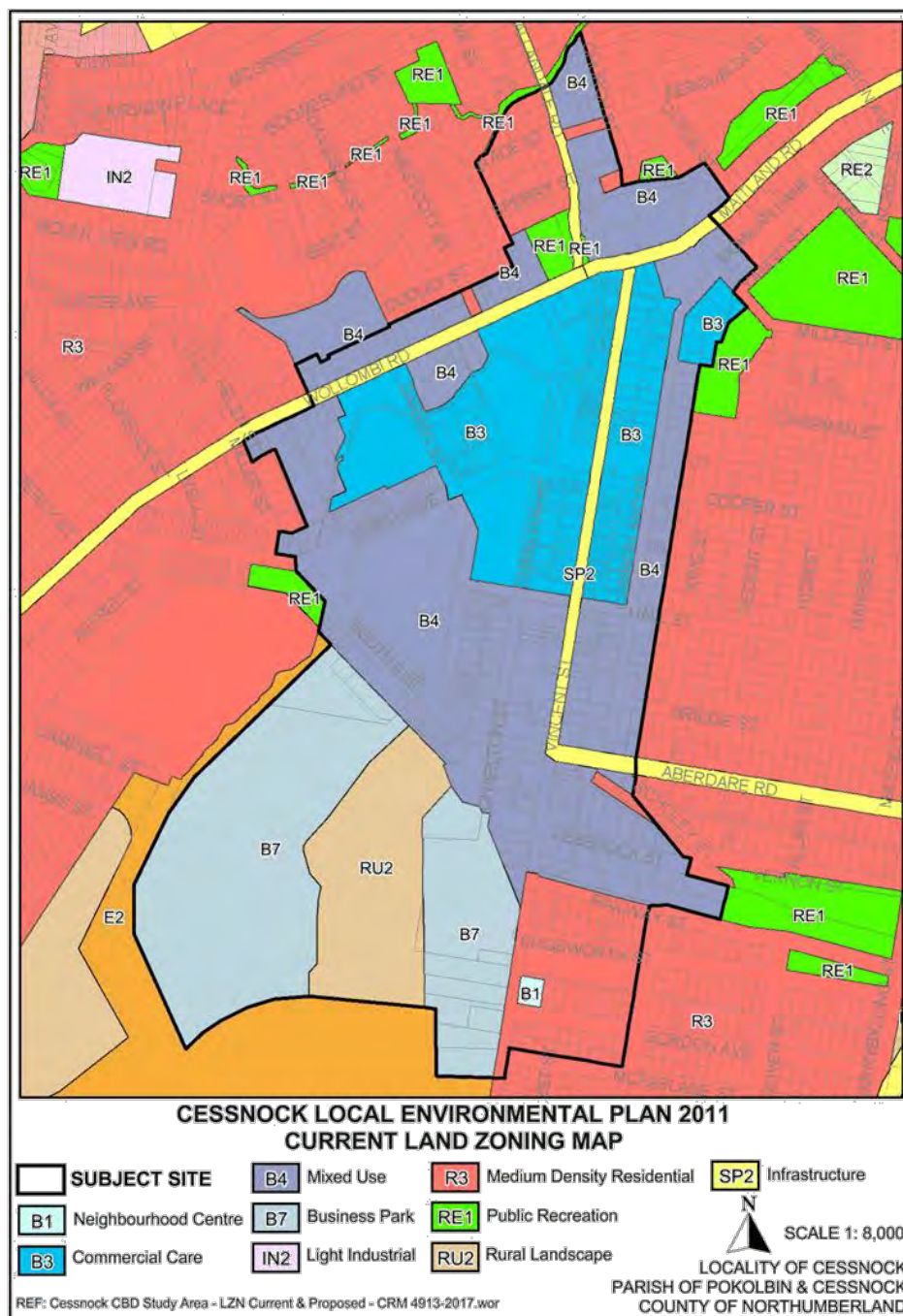
- Water & Sewerage Services
- Electricity Services
- Telecommunication Services
- Traffic Generation and Management

Initial consultations with RMS have taken place concerning the implementation of a shared zone along Cooper Street. Although the consultation has not been conclusive, this planning proposal reflects the outcomes of that consultation.

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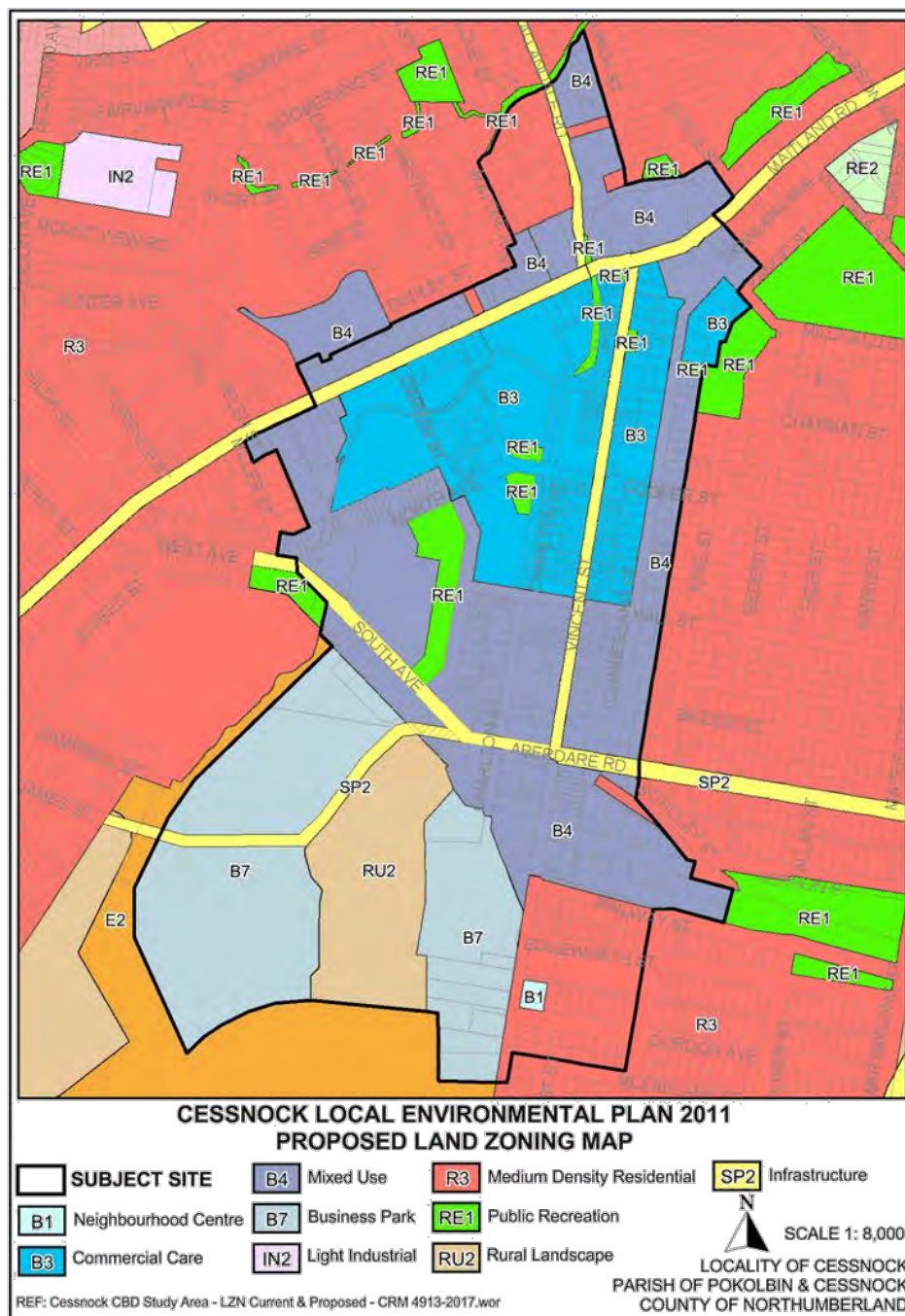
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PART 4: MAPPING



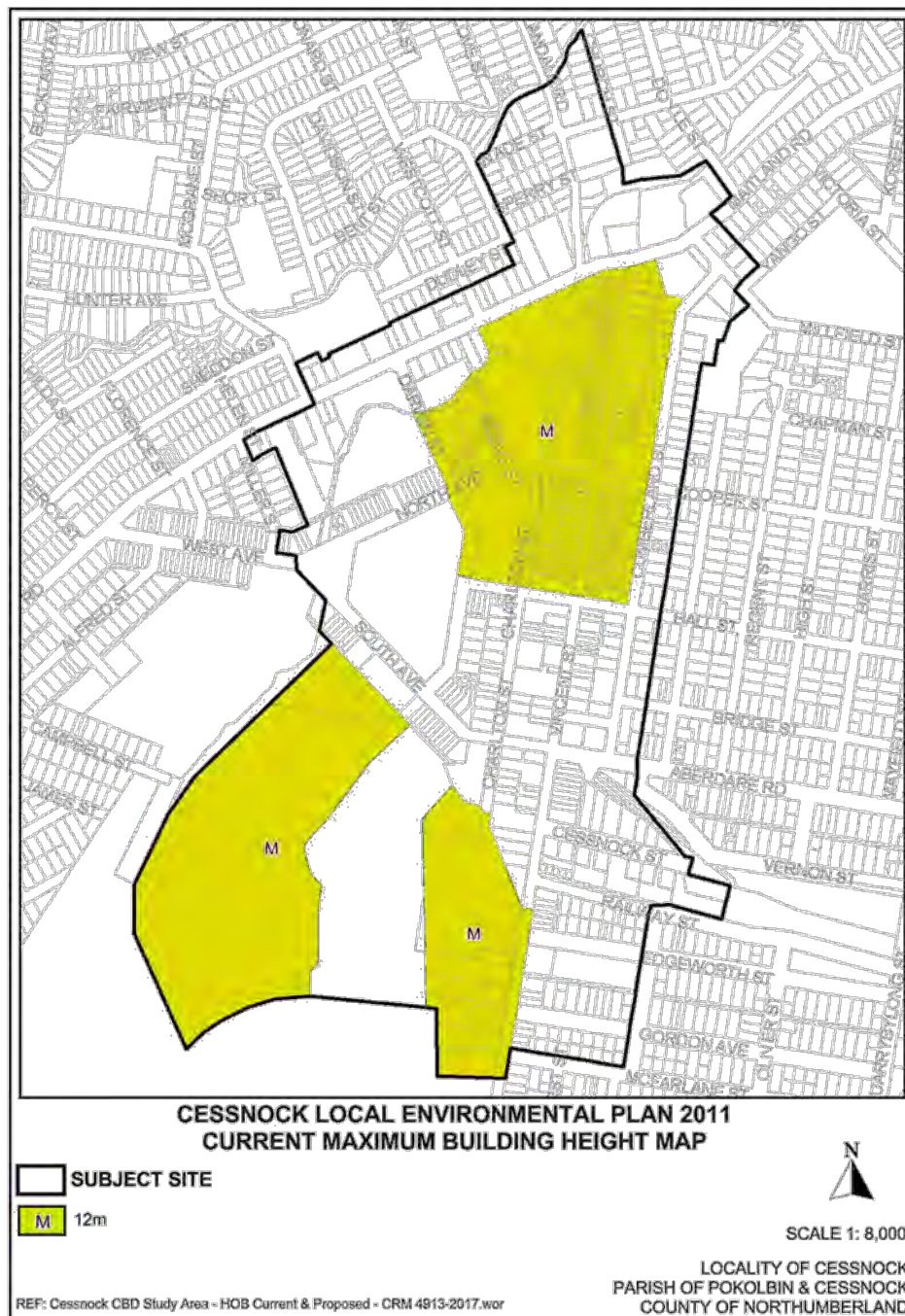
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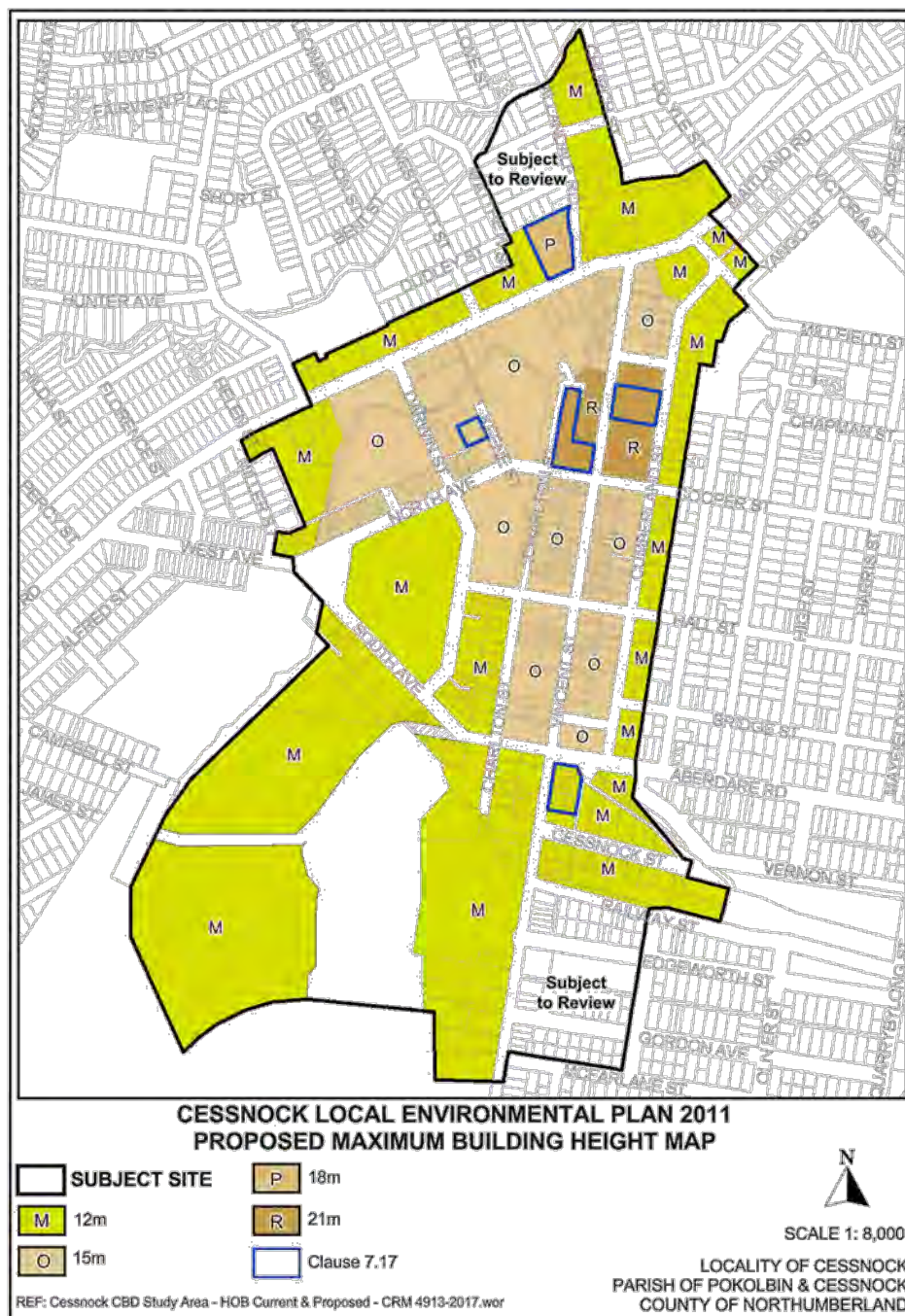
Planning Proposal – Cessnock Commercial Precinct

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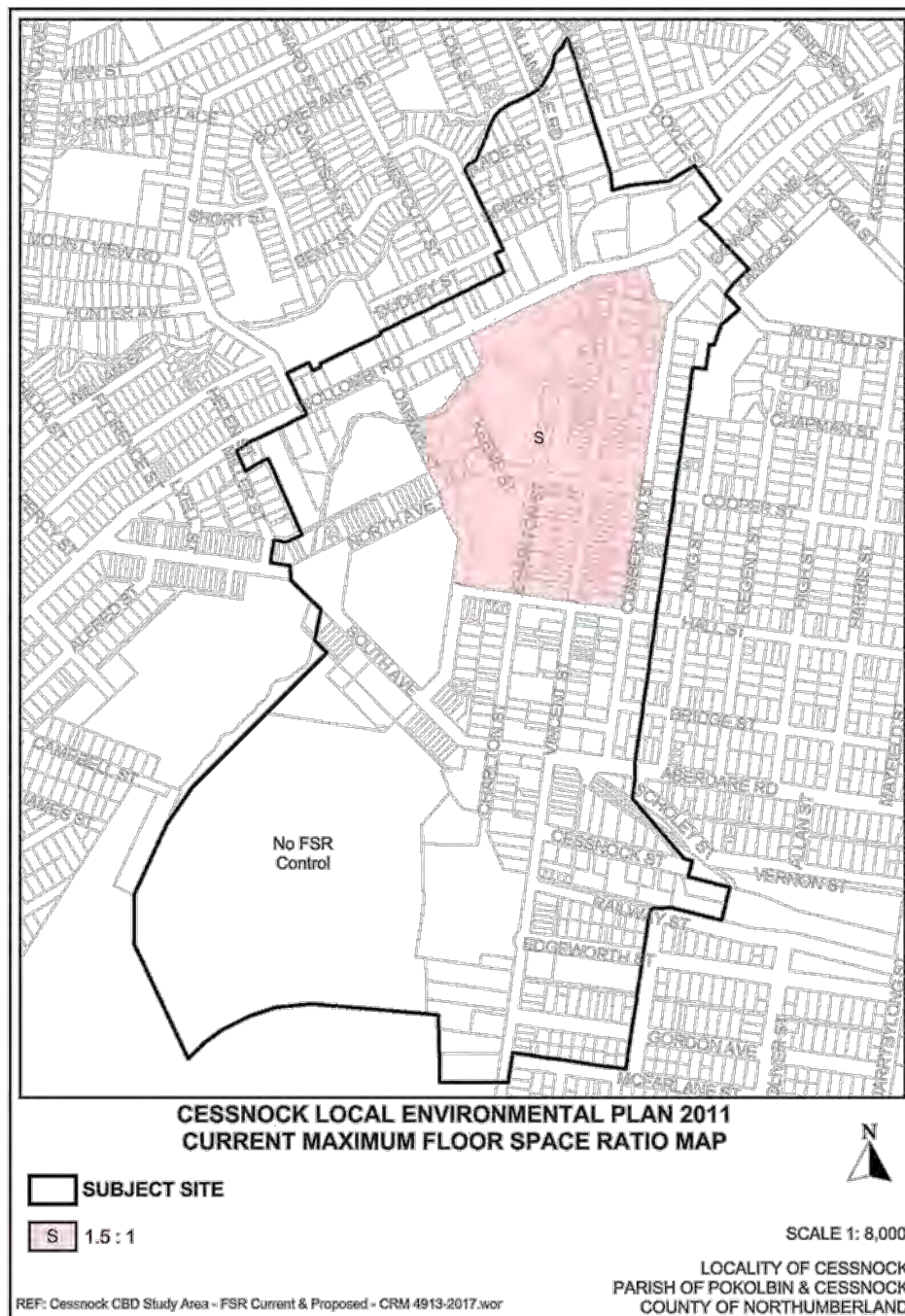
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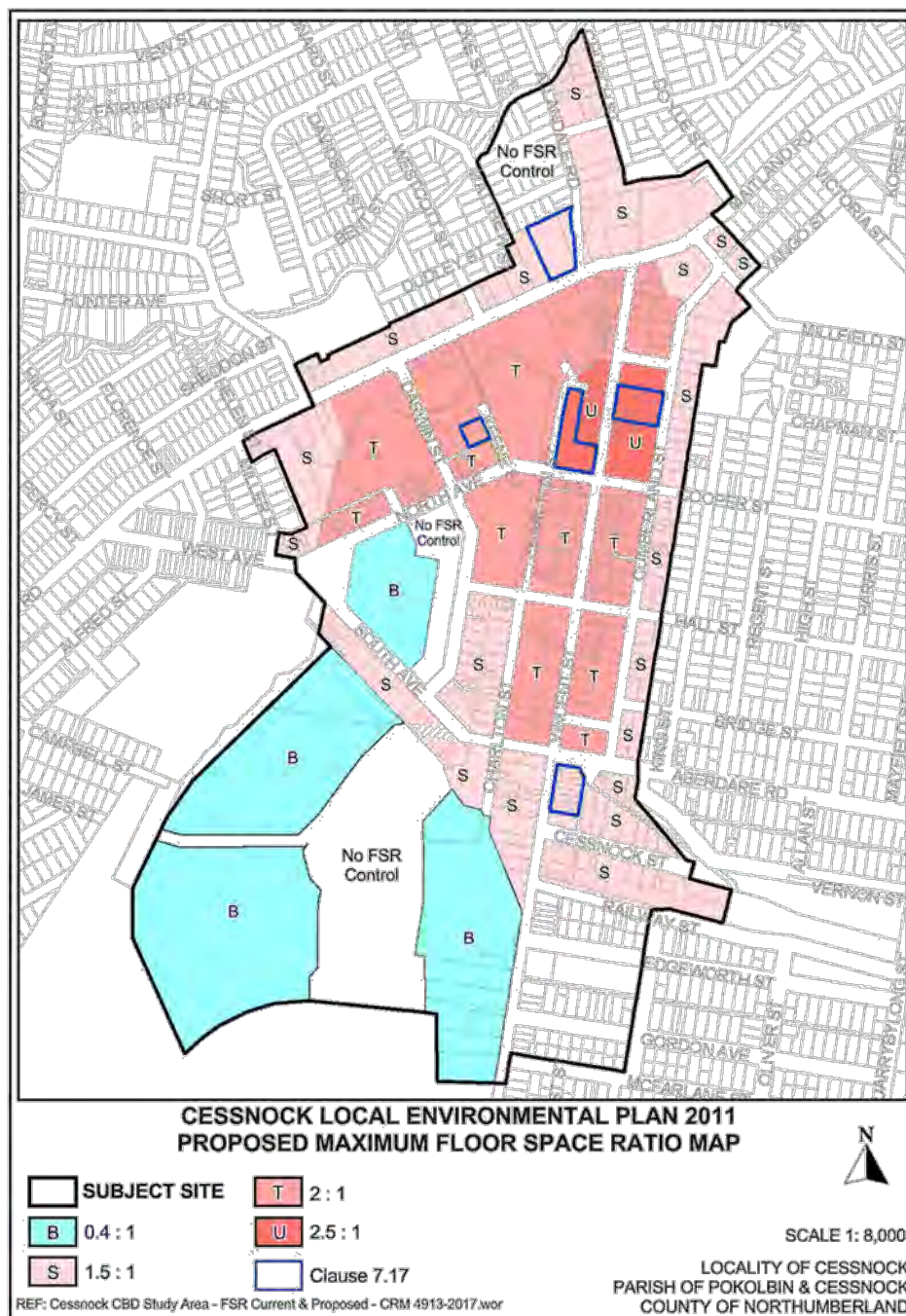
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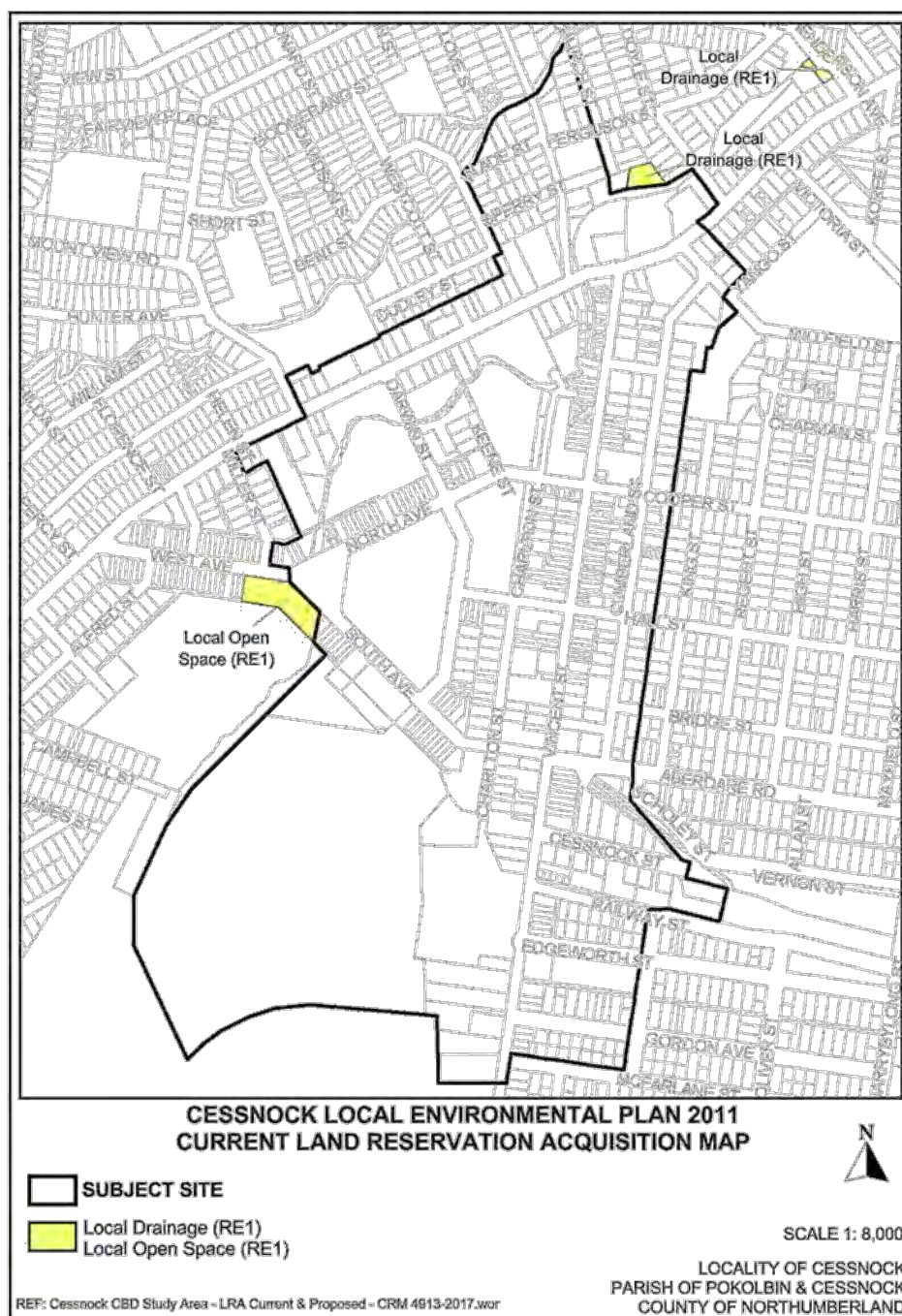
Planning Proposal – Cessnock Commercial Precinct

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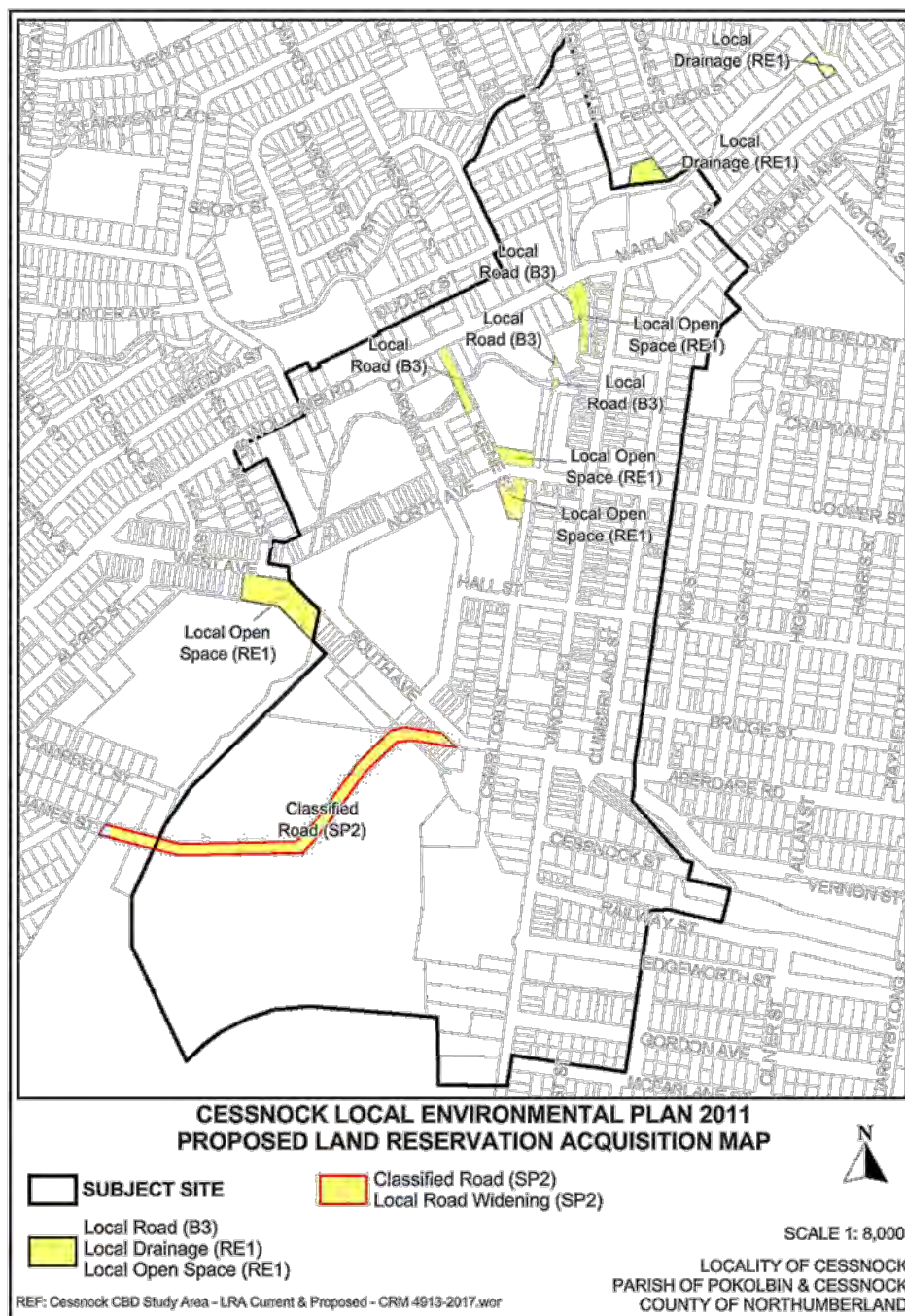
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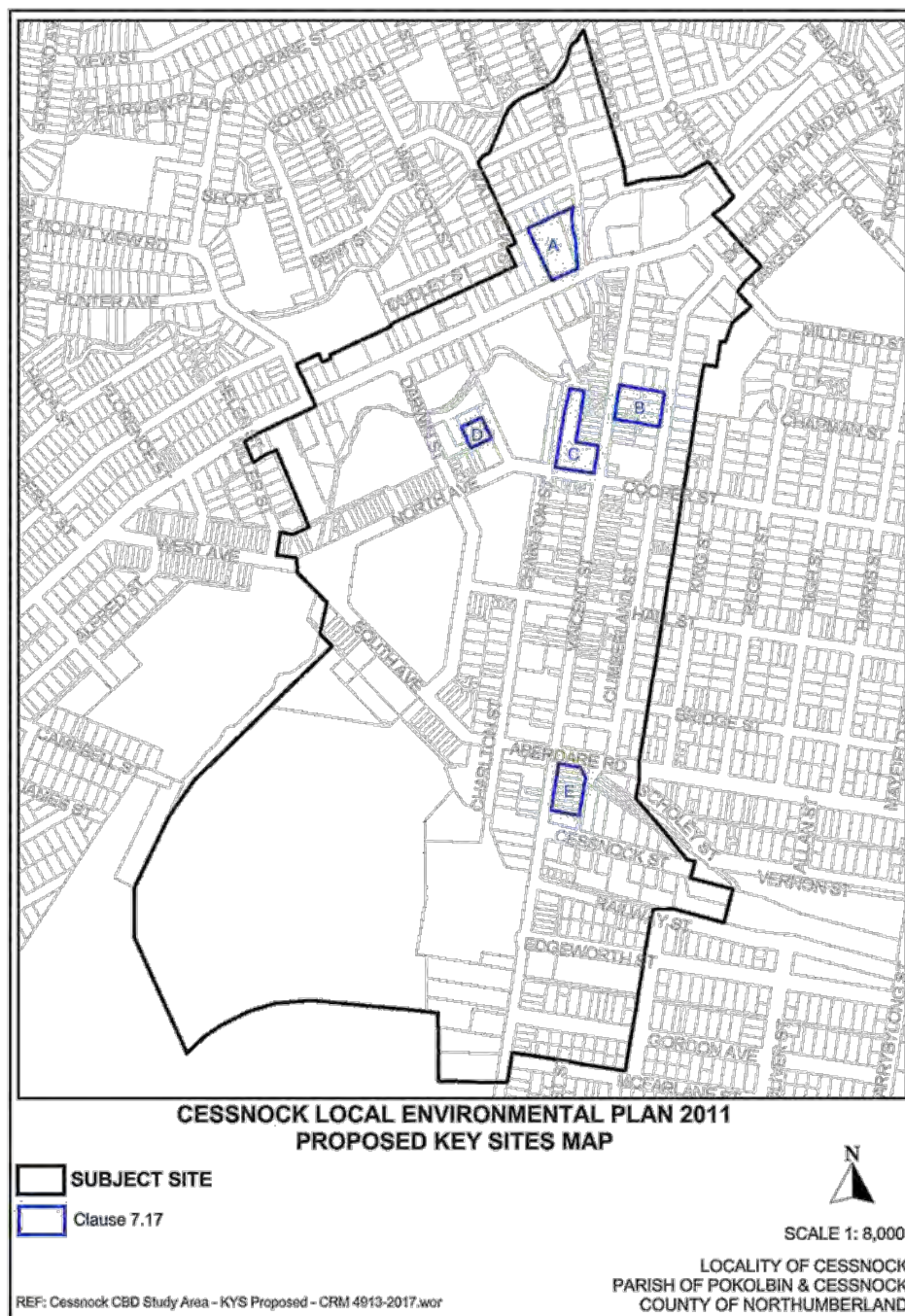
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PART 5: COMMUNITY CONSULTATION

This Planning Proposal forms a component of a larger Cessnock Commercial Precinct Project. The Cessnock Commercial Precinct Project is a continuation of the Cessnock CBD Masterplan, whereby significant consultation with the community was undertaken. Key components of the Masterplan, commensurate with the community's vision, have been brought forward into the Cessnock Commercial Precinct Project.

Further community consultation specific to the Cessnock Commercial Precinct Project was also undertaken in the form of:

- Online and intercept surveys with local businesses and residents (87 completed);
- Interactive mapping in the form of online mapping and a large printed map placed in the foyer of Council's Administration Building and Cessnock Library (135 comments);
- Community workshop (29 attendees); and
- Council staff workshop

Valuable information from the community was gathered during the consultation phases and used to inform this Planning Proposal. Key findings of the consultation are provided at **Appendix 3**.

It is expected that this Planning Proposal will be publicly exhibited for a minimum period of 28 days. The exhibition period would be advertised in local newspapers, notification letters will be sent to property owners within and adjoining the precinct to advise them of the exhibition, and the exhibition material will be made available at the following locations:

- Cessnock City Council's Administration Building
- Cessnock Public Library
- Kurri Kurri Public Library; and
- Cessnock City Council's Website at www.cessnock.nsw.gov.au

Initial consultations with RMS have taken place concerning the implementation of a shared zone along Cooper Street. Although the consultation has not been conclusive, the documentation subject of this Report reflects the outcomes of that consultation.

Planning Proposal – Cessnock Commercial Precinct

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PART 6: PROJECT TIMELINE

The Project Timeline will assist with tracking the progress of the Planning Proposal through the various stages of consultation and approval. It is estimated that the proposed amendment to the Cessnock Local Environmental Plan 2011 will be completed by September 2014.

PROJECT TIMELINE

	Apr 2017	Jun 2017	Aug 2017	Sep 2017	Oct 2017	Jan 2018	Mar 2018
STAGE 1 Submit to DoP&E – Gateway Panel consider Planning Proposal							
STAGE 2 Receive Gateway Determination							
STAGE 3 Preparation of documentation for Public Exhibition							
STAGE 4 Public Exhibition							
STAGE 5 Review/consideration of submission/s received							
STAGE 6 Report to Council							
STAGE 7 Make amendment or request amendment to be made by DoP&E							

Appendix 1: Council Report and Minutes

To be provided following Council resolution.

Appendix 2: Draft Local Environmental Plan Amendments

PART 7 ADDITIONAL LOCAL PROVISIONS (proposed)

Residential flat buildings in Zone B3 Commercial Core

1. *The objectives of this clause are as follows:*
 - (a) *To increase the supply of mixed use development in Zone B3 Commercial Core,*
 - (b) *To promote uses that attract pedestrian traffic along street frontages in Zone B3 Commercial Core,*
2. *Development consent must not be granted to a residential flat building on land in Zone B3 Commercial Core unless:*
 - (a) *it is a component of a mixed use development involving a permitted non-residential use,*
 - (b) *the consent authority is satisfied that the building will have an active street frontage after its erection or change of use.*
3. *Despite subclause (2), an active street frontage is not required for any part of a building that is used for any of the following:*
 - (c) *entrances and lobbies (including as part of mixed use development),*
 - (d) *access for fire services,*
 - (e) *vehicle access.*
4. *In this clause, a building has an **active street frontage** if all premises on the ground floor of the building facing the street are used for the purposes of business premises or retail premises*

Design excellence

1. *The objective of this clause is to ensure that development exhibits design excellence that contributes to the natural, cultural, visual and built character of the Cessnock Commercial Precinct.*
2. *This clause applies to development involving the erection of a new building or external alterations to an existing building on land identified as "Areas A, B, C, D and E" on the Key Sites Map.*
3. *Development consent must not be granted for development to which this clause applies unless:*
 - (a) *the consent authority considers that the development exhibits design excellence;*
 - (b) *the development within Area B, C or D achieves a minimum Floor Space Ratio that is 80% of the maximum Floor Space Ratio identified for the site on the Floor Space Ratio Map;*
 - (c) *the development achieves a 5 Star Green Star rating;*

- (d) the development is designed by a registered Architect;
- (e) the development in an area shown in Column 1 of the table to this subclause meets the specifications shown opposite the area in Column 2.

Column 1 <i>Area on the Key Sites Map</i>	Column 2 <i>Specifications relating to the Area</i>
Area A	<p>The proposed development architecturally presents as a major gateway to the city.</p> <p>The proposed development includes a suitable mix of both commercial and residential land uses.</p> <p>The proposed development is sympathetic or complimentary to the character of heritage items.</p> <p>The proposed development includes active landscaped street frontages in accordance with the Cessnock DCP</p>
Area B	<p>The proposed development includes a publicly accessible through site pedestrian link with a minimum width of 4 metres connecting Vincent Street to Cumberland Street and Council Car Park adjacent.</p> <p>The proposed development includes active frontages in accordance with the Cessnock DCP</p>
Area C	<p>The proposed development includes a publicly accessible through site pedestrian link, centrally positioned along the eastern façade, with a minimum width of 4 metres connecting Charlton Street to Vincent Street.</p> <p>The proposed development includes public entertainment uses such as a cinema.</p> <p>The proposed development includes active frontages in accordance with the Cessnock DCP.</p> <p>The proposed development provides a sensitive or complimentary response to heritage items in the vicinity of the site.</p>
Area D	<p>The proposed development is a mixed use development including residential flats.</p> <p>The proposed development includes active frontages in accordance with the Cessnock DCP.</p>

Area E	<p><i>The proposed development architecturally presents as a major gateway to the city.</i></p> <p><i>The proposed development includes a suitable mix of both commercial and residential land uses.</i></p> <p><i>The proposed development is sympathetic or complimentary to the character of heritage items.</i></p>
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4. *In considering whether the development exhibits design excellence, the consent authority must have regard to the following matters:*
 - (a) *whether a high standard of architectural design, materials and detailing appropriate to the building type and location will be achieved,*
 - (b) *whether the form and external appearance of the development will improve the quality and amenity of the public domain,*
 - (c) *whether the development detrimentally impacts on view corridors identified in the Cessnock Development Control Plan,*
 - (d) *how the development addresses the following matters:*
 - (i) *the suitability of the land for development,*
 - (ii) *existing and proposed uses and use mix,*
 - (iii) *heritage issues and streetscape constraints,*
 - (iv) *the relationship of the development with other development (existing or proposed) on the same site or on neighbouring sites in terms of separation, setbacks, amenity and urban form,*
 - (v) *bulk, massing and modulation of buildings,*
 - (vi) *street frontage heights,*
 - (vii) *environmental impacts such as sustainable design, overshadowing, wind and reflectivity,*
 - (viii) *the achievement of the principles of ecologically sustainable development,*
 - (ix) *pedestrian, cycle, vehicular and service access, circulation and requirements,*
 - (x) *the impact on, and any proposed improvements to, the public domain.*
5. *The consent authority may grant consent to the erection or alteration of a building to which this clause applies that has a floor space ratio of not more than 20% greater than that allowed by clause 4.4 or a height of not more than 3 metres greater than that allowed by clause 4.3, but only if the design of the building or alteration has been reviewed by a design review panel that determines the development exhibits design excellence.*
6. *In this clause **design review panel** has the same meaning as it has in the State Environmental Planning Policy No 65 – Design Quality of Residential Apartment Development.*

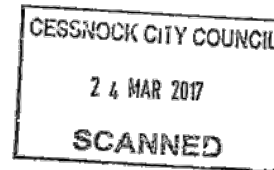
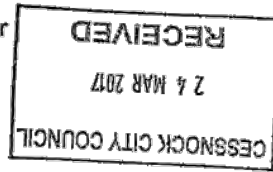
Appendix 3: Cessnock Commercial Precinct Public Domain Plan and DCP Engagement Outcomes Report

To be provided following Council Resolution



Australian Government
Department of the Environment and Energy

Mr Martin Johnson
Strategic Land Use Planning Manager
Cessnock City Council
PO Box 152
CESSNOCK NSW 2325



Dear Mr Johnson

National Heritage List nomination for the 2017-18 assessment period

Thank you for your nomination of Wollombi Public School for the National Heritage List. Some information from the nomination will be made publicly available through the Australian Heritage Database online at <http://www.environment.gov.au/cgi-bin/ahdb/search.pl>. A copy of this information is provided for your records.

The nomination (with all supporting material) will now be considered by the Australian Heritage Council in preparing its advice to the Hon Josh Frydenberg, Minister for the Environment and Energy on eligible nominations for the National Heritage List.

The Australian Heritage Council's advice to the Minister will cover each place nominated, with regard to the extent to which a place may have National Heritage values, the Council's views of the relative priority of all the nominated places, its resourcing and capacity, and any other matters which it deems relevant.

Once the Minister has considered all the nominations and the Australian Heritage Council's advice on them, he will select a final list of places for the Council to fully assess for the National Heritage List (forming its new assessment work plan). The Minister's selection of the final list of places to assess will be published on the Internet, generally around June or July. The webpage is: <http://www.environment.gov.au/heritage/places/priority-assessment>.

The Department will formally notify you of the Minister's decision. Meanwhile for any queries on the process, you are welcome to email heritage@environment.gov.au.

Yours sincerely

Caroline Brusse
Director
Heritage Strategies Section
Heritage Branch
Wildlife, Heritage and Marine Division

20 March 2017



Australian Heritage Database

Place Report - Detailed (Internal)

Item: 1

Identification

List: National Heritage List
Name of Place: Wollombi Public School
Other Names:
Also Searchable By:
Place ID: 106313
File No: 1/09/060/0005
Class(es): Historic
Principal Group: Education
Primary Nominator: Cessnock City Council 100932
Nominator's Address: PO Box 152 CESSNOCK NSW 2325

Status

Legal Status: 17/02/2017 - Nominated place
Admin Status: 17/02/2017 - Not assessed
Next Admin Step: 17/02/2017 - Acknowledge nominator and advise State
Assessor:

Location

State: NSW
LGA: Cessnock City
Nearest Town: Cessnock
Distance from town (km): 23
Direction from town: SW
Area (ha): 0.8
Address: 2847-2853 Wollombi Rd Wollombi NSW 2325

Map Reference:	Map No	Map Type	Map Name
	SI-56-01	1:250,000	Singleton
	9132	1:100,000	Cessnock

Property Information:

Land Parcels Lots 1-4 Section 8 DP759103

Location/Boundaries:

Approximately 0.8ha, 2847-2853 Wollombi Road, Wollombi, being the area comprising the following Land Parcels;
 Lots 1-4 Sec 8 DP759103

Official Summary of Significance:

Official Values:

Criterion	Values	Rating
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Historic Themes:



Australian Heritage Database

Place Report - Detailed (Internal)

Nominator's Summary of Significance:

The township of Wollombi has a strong colonial history and is directly linked to the Great North Road, an item of State Heritage Significance. Built from 1826 the Great North Road was significant in providing an overland route for trade and travellers to allow the state and nation to grow.

The Village of Wollombi was established in the 1830's at a significant intersection of the Great North Road, where one arm travels east and the other south. The village quickly grew into a thriving township with the establishment of a police station, courthouse, church and school to meet the growing demand of the overland trade. The School played a significant role in establishing the community at Wollombi to assist in expanding the trade routes. The school in its intact form demonstrates the direct link to early education in the establishment of the colony and the role it played in facilitating trade and growth within New South Wales and Australia. Having an administrative centre located on the Great North Road allowed for day to day services such as a court house, police station and school to be provided to facilitate growth and trade within NSW and the Nation.

Description:

Constructed in 1881 the site contains a series of buildings which comprised the former school and teacher's residence. The buildings on the site are of a consistent architectural style and are well preserved.

Teachers Residence

The teacher's residence is constructed from stone with a timber pavilion addition and consists of four rooms. The building contains four tall stone chimneys with finely executed stone trim. An iron roof has replaced the original shingle roof. The building contains asymmetrical front door, sash windows and a central front verandah supported on four timber posts (not original). The original front picket fence has been replaced in recent years by aluminium railing.

Classrooms

The classrooms are gabled single storey buildings constructed of locally quarried stone with iron roof, roof vents, and narrow vertical windows. An additional stone room has been added at a later date.

Toilet building

The toilet building is of a similar architectural style to the classroom buildings and is constructed from locally quarried sandstone.

Bell

The external school bell is still located on the site and is a key reminder of the daily functioning's of the school.

History:

The school building and teachers residence were constructed in 1881.

An additional sandstone classroom was added, most likely in the early 20th century. The teachers residence has an additional timber pavilion added around the 20th century. The additions are in keeping with the 19th century buildings.

Condition:

The site is in very good condition having been used as a school until 2014 and regular community meetings until 2017. In late 2016 the site was identified as surplus land to the Department of Education. A property disposal plan for Wollombi Public School is being prepared for Property NSW.

Bibliographic References:

<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=1340101>
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=1340030>
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=1340165>

Wollombi Public School

<http://www.wollombi-p.schools.nsw.edu.au/>

Wollombi School Community Education Trust



Australian Heritage Database

Place Report - Detailed (Internal)

<http://wscetinc.wixsite.com/wscet>

1 item(s) printed

End of Report

Printed by Lawrence Bourke on 09 March 2017

Page 3



National Heritage List

NOMINATION FORM

The National Heritage List is a record of places in Australia with outstanding natural, Indigenous or historic heritage values for the nation. It is a list of important places special to the story of Australia such as the Sydney Harbour Bridge, the West Kimberley and Kakadu National Park. The values of these places are protected under the *Environment Protection and Biodiversity Conservation Act 1999*. Nominating a place for the National Heritage List requires identifying its heritage values on this form and providing supporting evidence as to why it is of national significance.

Nomination Notes are provided to help you complete the form. They give information on the high level of heritage significance required before a place is eligible for inclusion in the List as well as on the general nomination process. For further help contact heritage@environment.gov.au

Complete this checklist before you start the form:

- ☒ I have read the National Heritage List Nomination Notes.
- ☒ I have considered the different heritage lists, and have concluded that State or Local Heritage Listing would not be more appropriate for my place.
- ☒ If I am not the place owner, I have consulted the place owner and made them aware I am considering nominating their place and I have attached evidence of this consultation.
- ☒ I have read the copy of the relevant Regulations to ensure that I have provided all information required, and I am aware that nominations which do not satisfy the requirements of the Regulations will be ineligible.
- ☒ I have provided my details, signed and dated the form (noting the form can be submitted electronically - see Notes).
- ☒ I understand that many more places are nominated than can be assessed.
- ☒ I understand that although a place may be nominated for national listing, this does not provide legal protection under the Australian Government's environment legislation (the national heritage values of a place are protected only if the Minister includes the place in the National Heritage List).

Nominated place details

Q1. What is the name of the place? (It is compulsory to answer this question.)

Wollombi Public School (former)

Q2. TIP

Give the street address, or, if remote, describe where it is in relation to the nearest town. Include its area and boundaries. Attach a map with the location and boundaries of the place clearly marked. See the *Nomination Notes* for map requirements and the importance of a clear and defined boundary.

Q2a. Where is the place? Address/location: (It is compulsory to answer this question.)

The Wollombi Public School (former) is located in Wollombi, a small village in the NSW Hunter Region. The village of Wollombi has a rich colonial heritage, being located on the Great North Road, which is an item of State Heritage significance. The Wollombi valley has a number of Heritage items and a Heritage Conservation Area of Local Significance. The Wollombi Public School is listed in the Cessnock Local Environmental Plan 2011 as an item of Local Significance. Council is currently pursuing a listing on the State Heritage Register.

The Wollombi Public School is located at 2847-2853 Wollombi Road Wollombi NSW, also known as Lot 4 Sec 8 DP 759103, Lot 3 Sec 8 DP 759103, Lot 2 Sec 8 DP 759103, Lot 1 Sec 8 DP 759103.

Q2b. Boundary: (It is compulsory to answer this question.)

The Wollombi Public School (former) is located at:
2847-2853 Wollombi Road Wollombi NSW, also known as Lot 4 Sec 8 DP 759103, Lot 3 Sec 8 DP 759103, Lot 2 Sec 8 DP 759103, Lot 1 Sec 8 DP 759103

Attachment 1: Location map

Attachment 2: Aerial map of the site

Attachment 3: Cadastre map of the site

Attachment 4: Heritage sites and the Wollombi Heritage Conservation area

Q2c. Type of map you have supplied: (It is compulsory to provide a map.)

The following maps are attached in jpg and pdf format:

- Attachment 1: Location map
- Attachment 2: Aerial map of the site
- Attachment 3: Cadastre map of the site
- Attachment 4: Heritage sites and the Wollombi Heritage Conservation area

Q3. TIP

For information on where to obtain details of who owns a place, contact your local government. See the *Nomination Notes* for ideas.

Q3. Who owns it? Who occupies it? (If more than one owner or occupier, attach a list.)

The site is owned by the NSW Department of Education. The site ceased operations as a school in 2014. Since then the site has been used by a number of community groups.

Owner address: Department of Education and Communities
35 Bridge Street
Sydney

State: NSW

Postcode: 2000

Telephone: 1300 679 332

Fax: -

Email: -

Q4. TIP

Before a place is listed in the National Heritage List all parties with a right or an interest in the place will be consulted and their written comments provided to the Australian Heritage Council and the Minister. Informing all of the relevant individuals and groups is an important step that you should undertake before submitting this nomination.

Q4a. Are the owner(s) and/or occupier(s) aware of the nomination? Do they support your nomination? If an owner(s) or occupier(s) does not support your nomination, it is unlikely that your nomination will be successful.

NO ☐

YES ☐

SOME ARE ☐ (Please list):

Unknown

The NSW Minister for Education has been advised in writing on 20 December 2016 that Council has resolved to commence proceedings to nominate the school for State and National Heritage listing. No response has been received advising of the Department of Education or the Minister for Education's opinion on the Heritage listing.

Q4b. Who else has an interest in the place? This could include the property's manager, local environment or historical groups, local council, Indigenous people and developers or industry groups. Please provide names and contact details.

Since the closure of the school in 2014 the site has been used by a number of community groups (with permission from the Department of Education). Bookings for the School are managed by the Wollombi School Community Education Trust. Since April 2015 almost 270 community events have been held at the school site including:

- A pre-school 2 days a week
- Local history talks
- Indigenous programs
- Art workshops
- Land care meetings
- Knitting groups
- Film nights
- Prenatal classes

Contact details for Wollombi School Community Education Trust

Postal Address: WSCET c/o Wollombi General Store, 2886 Wollombi Road, Wollombi, NSW.

Email: wscetevents@gmail.com

Facebook: www.facebook.com/wscet

Webpage: <http://wscetinc.wixsite.com/wscet>

About the importance of the place

Q5. TIP

Your response to this question should be a summary of the values that give the place outstanding heritage significance. Your summary should be expanded in question six to a level of detail that is expected of all National Heritage List nominations. See the *Nomination Notes* for further details.

Q5. What is its significance? (It is compulsory to answer this question.) Why is this place of outstanding heritage significance to Australia?

The township of Wollombi has a strong colonial history and is directly linked to the Great North Road, an item of State Heritage Significance. Built from 1826 the Great North Road was significant in providing an overland route for trade and travellers to allow the state and nation to grow.

The Village of Wollombi was established in the 1830's at a significant intersection of the Great North Road, where one arm travels east and the other south. The village quickly grew into a thriving township with the establishment of a police station, courthouse, church and school to meet the growing demand of the overland trade. The School played a significant role in establishing the community at Wollombi to assist in expanding the trade routes. The school in its intact form demonstrates the direct link to early education in the establishment of the colony and the role it played in facilitating trade and growth within New South Wales and Australia. Having an administrative centre located on the Great North Road allowed for day to day services such as a court house, police station and school to be provided to facilitate growth and trade within NSW and the Nation.

Q6. TIP

See the *Nomination Notes* for examples on how criteria might be interpreted. A place does not need to meet more than one of the National Heritage criteria. Concentrating on the most applicable criteria may help you focus on what features of the place give it its outstanding heritage significance to all Australians.

Q6. Which criteria does it meet? (It is compulsory to answer this question.) From the nine National Heritage criteria, identify each criterion which applies to the place and explain why the place meets that criterion. Your nomination is initially considered solely on the basis of the information you provide. To enable appropriate consideration of your nomination, please provide reference details for information you use. You may need to attach additional information.

- ☒ a - the place has outstanding heritage value to the nation because of the place's importance in the course, or pattern, of Australia's natural or cultural history

The School is significant to the course and pattern of Australia's cultural history as it demonstrates the Governments role in establishing and stabilising settlements in towns. The School has strong links with the Great North Road and has been fundamental to the functioning of overland trade routes in Australia.

The township of Wollombi and the school are located on the State Heritage listed Great North Road, a convict built road that runs from Sydney to the Hunter Valley. Built from 1826 the Great North Road was significant in providing an overland route for trade and travellers to allow the state and nation to grow. Located where the Great North Road splits with one branch travelling east and the other north, Wollombi was established as the administrative centre for the district. It quickly grew into a thriving township with the establishment of a police station, courthouse, church and school to meet the growing demand. The School is a keystone element of the Wollombi Village as it overlooks the Great North Road and was fundamental to the functioning of Wollombi in the 19th Century.

The school in its intact form demonstrates the direct link to early education in the establishment of the colony and the role it played in facilitating trade and growth within New South Wales and Australia. The site highlights the importance the government and settlers placed on education in the 19th Century. The school has direct links to early education in the colony and the earlier schools of Wollombi. A number of small schools operated during the early years of the establishment of Wollombi. The Methodists (1843) the Anglicans (1858), and the National School (1852) all had links to the Wollombi Public School site.

It is an outstanding representative example that demonstrates the principal characteristics of educational structures and uses of the late 19th century and incorporates adoptions to suit changes over the years to the present.

- ☐ b - the place has outstanding heritage value to the nation because of the place's possession of uncommon, rare or endangered aspects of Australia's natural or cultural history

The site does not meet this criteria.

☒ c - the place has outstanding heritage value to the nation because of the place's potential to yield information that will contribute to an understanding of Australia's natural or cultural history

As described above the township of Wollombi, particularly the school played an important role in facilitating trade and growth of NSW and the nation through providing an administrative centre on the Great North Road. This allowed trade to be undertaken in an organised and efficient manner. The village of Wollombi, particularly the School has the potential to demonstrate an important role in Australia's cultural history. The Village is intact with a number of original buildings still remaining. The village has a consistent architectural style with many of the building made with locally sourced material. The School is a key feature of the village as it consists of a series of buildings with similar material and was fundamental to the functioning of the village.

☐ d - the place has outstanding heritage value to the nation because of the place's importance in demonstrating the principal characteristics of:

- i. a class of Australia's natural or cultural places or
- ii. a class of Australia's natural or cultural environments

The site does not meet this criteria.

☒ e - the place has outstanding heritage value to the nation because of the place's importance in exhibiting particular aesthetic characteristics valued by a community or cultural group

The colonial history of Wollombi and the School is strongly valued by the Wollombi Community. The village is an intact representation of Australia's colonial history and the role Wollombi played in facilitating trade and growth of the nation.

The School has direct links to small schools that operated in the early years of the establishment of Wollombi, such as Methodists (1843) the Anglicans (1858), and the National School (1852). Today, the school site is used by numerous community groups as a meeting space.

☐ f - the place has outstanding heritage value to the nation because of the place's importance in demonstrating a high degree of creative or technical achievement at a particular period

The site does not meet this criteria.

☐ g - the place has outstanding heritage value to the nation because of the place's strong or special association with a particular community or cultural group for social, cultural or spiritual reasons

The site does not meet this criteria.

☐ h - the place has outstanding heritage value to the nation because of the place's special association with the life or works of a person, or group of persons, of importance in Australia's natural or cultural history

The site does not meet this criteria.

☐ i - the place has outstanding heritage value to the nation because of the place's importance as part of Indigenous tradition

The site does not meet this criteria.

Q7a. TIP

In describing the place consider what are its physical aspects and surrounds, its uses by people, its aesthetic qualities, spiritual or cultural associations? Include photographs and a site map or sketch plan if appropriate. See the *Nomination Notes* for further details.

Q7a. How would you describe the place?

Constructed in 1881 the site contains a series of buildings which comprised the former school and teacher's residence. The buildings on the site are of a consistent architectural style and are well preserved.

Teachers Residence

The teacher's residence is constructed from stone with a timber pavilion addition and consists of four rooms. The building contains four tall stone chimneys with finely executed stone trim. An iron roof has replaced the original shingle roof. The building contains asymmetrical front door, sash windows and a central front verandah supported on four timber posts (not original). The original front picket fence has been replaced in recent years by aluminium railing.

Classrooms

The classrooms are gabled single storey buildings constructed of locally quarried stone with iron roof, roof vents, and narrow vertical windows. An additional stone room has been added at a later date.

Toilet building

The toilet building is of a similar architectural style to the classroom buildings and is constructed from locally quarried sandstone.

Bell

The external school bell is still located on the site and is a key reminder of the daily functioning's of the school.

Q7b. What condition is the place in? Describe whether the place is intact or if there has there been any damage or disturbance. How is the place currently used and are you aware of any plans to change the way the place is used in the future?
The site is in very good condition having been used as a school until 2014 and regular community meetings until 2017.

In late 2016 the site was identified as surplus land to the Department of Education. A property disposal plan for Wollombi Public School is being prepared for Property NSW.

Q8. What is its history? Summarise its origins and development. You may need to attach additional information.

The school building and teachers residence were constructed in 1881.

An additional sandstone classroom was added, most likely in the early 20th Century. The teacher's residence has an additional timber pavilion added around the early 20th century. The additions are in keeping with the 19th Century buildings.

Q9. TIP

Only the most outstanding places are included in the National Heritage List. Consider if other places have similar species or rock formations; are there similar buildings or places with similar histories, traditions or beliefs attached to them? In order for your nomination to be properly considered, you must clearly demonstrate how the place being nominated is more outstanding than others. See the *Nomination Notes* for further details.

Q9. What other places have similar characteristics? How do they compare with the place you are nominating?

There are a number of original school buildings in the Cessnock Local Government Area and the State; however the Wollombi Public School is significant for its connection and role in facilitating trade and growth of the Nation along the Great North Road.

The Wollombi School is particularly important because of its location in the Village of Wollombi. Wollombi was established in the beginning of the 19th Century and has a number of original buildings still remaining. The Village has a consistent architectural style and the fabric of many of the original buildings remains intact. The school is a key element of the village because of the role it played in establishing the community.

Q10. Provide references for information you have provided. What other information is available on the place? List any articles, books, reports or studies that may provide evidence supporting your nomination. You may also have information from Traditional Owners and Custodians, scientists or heritage specialists. If they have agreed to share their knowledge, please include their contact details.

Information has been sourced from the following resources:

- NSW Heritage Inventory
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=1340101>
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=1340030>
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=1340165>
- Wollombi Public School
<http://www.wollombi-p.schools.nsw.edu.au/>
- Wollombi School Community Education Trust
<http://wscetinc.wixsite.com/wscet>

Considerations

Q11. Are there sensitive issues associated with the place? These may be issues that need to be kept confidential such as matters relating to sacred or religious sites, or the location of rare fossils, plants or fragile places.

NO ☒

YES ☐

If you answer 'yes', and if the Minister asks for the place to be assessed, we will contact you to discuss the issues.

Your details

Your details are needed in case we require more information on the nominated place. Your identity is protected under the federal *Privacy Act 1988* and will not be divulged without your consent or as allowed for under that Act.

Title: Mr First name: Martin Family name: Johnson
 Are you nominating a place on behalf of an organisation? NO ☐ YES ☒
 If you answered no, please complete the address details below, if yes, please name the organisation and your position in it and then complete the address details for the organisation below:
 Organisation: Cessnock City Council Position: Strategic Land Use Planning Manager
 Address: PO Box 152
 Cessnock State: NSW Postcode: 2325
 Telephone: 02 4993 4127 Email: martin.johnson@cessnock.nsw.gov.au

FINAL CHECKLIST

Before signing and dating your nomination form, please make sure that you have:

- ☒ completed name, location, boundary, significance and criteria questions
- ☒ attached and labelled the location/boundary map and/or site plan
- ☒ attached and labelled any photographs and supporting evidence or extra information.

Signature of nominator

Date

Send your completed nomination form and attachments:

By mail to: The Heritage Nominations Manager Heritage Branch Wildlife, Heritage and Marine Division Department of the Environment and Energy GPO Box 787 CANBERRA ACT 2601	Or e-mail to: heritage@environment.gov.au or hand delivery to: The Heritage Nominations Manager Heritage Branch Wildlife, Heritage and Marine Division Department of the Environment and Energy John Gorton Building PARKES ACT 2600
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If the person making this nomination is, or is representing, a *small business* (a business having fewer than 20 employees), please provide an estimate of the time taken to complete this form. hours minutes

Please Include

The time spent reading the instructions, working on the questions and obtaining the information; and
 The time spent by all employees in collecting and providing this information.

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Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 14 December 2016	Nicole Benson	Branxton Pool - Heating Upgrade	15/03/2017		9/03/2017
BN18/2016 87	Councillor Sander	Justin Fitzpatrick-Barr				
09 Mar 2017 - 11:03 AM - Nicole Benson Action completed by: Kristy Meyers Report presented to 1/3/17 Council meeting. 24 Jan 2017 - 8:17 AM - Robyn Larsen Information being collated for March 2017						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 February 2017	Katrina Kerr	Upgrade of Signage Prohibiting the Riding of Bicycles on Footpaths	1/03/2017	3/02/2017	9/03/2017
BN4/2017 102	Councillor Fitzgibbon	Justin Fitzpatrick-Barr				
27 Mar 2017 - 12:33 PM - Kristy Meyers Report being presented to Council at 5/4/17 Council meeting. Item 1 to be investigated and actioned by Works Delivery. Item 2 to be investigated and actioned by Design Delivery. Item 3 with P&E to prepare a report back to Council as per the resolution.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 October 2016	Bo Moshage	Conversion of Sheds into Dwellings	30/04/2017	7/10/2016	24/03/2017
BN8/2016 18	Councillor Olsen	Gareth Curtis				
24 Mar 2017 - 4:16 PM - Bo Moshage Action completed by: Bo Moshage Report addressing the resolution of Council on 5 October 2016 regarding the conversion of sheds into dwellings to 5 April 2017 Council Meeting 11 Oct 2016 - 9:01 AM - Colin Davis Report to be brought back to Council by April 2017						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Geoffrey Bent	Failure of Mount View Road, Cessnock	12/04/2017	20/03/2017	21/03/2017
BN11/2017	Councillor Olsen	Justin Fitzpatrick-Barr				
21 Mar 2017 - 10:04 AM - Geoffrey Bent Action completed by: Kristy Meyers No action required. This was put and lost.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Bronwyn Rumbel	December 2016 Quarter Review of 2013-17 Delivery Program and Fit for the Future Improvement Proposal	31/03/2017	21/02/2017	6/03/2017
PE2/2017 110		Gareth Curtis				
06 Mar 2017 - 9:46 AM - Bronwyn Rumbel Action completed by: Bronwyn Rumbel Adopted changes to Capital Works Program noted.						

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Paul Grosbernd	Quarterly Budget Review Statements - December 2016	15/03/2017	21/02/2017	6/03/2017
CC12/2017 112 06 Mar 2017 - 10:12 AM - Paul Grosbernd Noted. General Ledger updated with QBRS changes.		Robert Maginnity				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 February 2017	Renae Leayr	Regional Procurement Tender for the Supply and Delivery of Bulk Cationic Bitumen CRS (REGPRO221617)	1/03/2017	3/02/2017	7/03/2017
CC1/2017 98 07 Mar 2017 - 12:56 PM - Renae Leayr Action completed by: Renae Leayr Regional Procurement has been advised of the outcome of the tender evaluation and that Council is participating in the tender.		Robert Maginnity				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 February 2017	Megan Savill	North Rothbury Priority Sewerage Scheme - Responses from The Minister for Lands & Water and Hunter Water Corporation	1/03/2017	3/02/2017	7/03/2017
CO2/2017 104 07 Mar 2017 - 9:02 AM - Megan Savill Action completed by: Colin Davis letter forwarded to Minister for Primary Industries and Minister for Lands and Water		Gareth Curtis				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Gina Radford	Minutes of the Organisational and General Managers Review Committee meeting held 8 February 2017	15/03/2017	21/02/2017	7/03/2017
MM4/2017 109 07 Mar 2017 - 9:12 AM - Gina Radford Action completed by: Gina Radford Noted		Stephen Glen				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 March 2017	Ian Lyall	Audit Committee Chair	29/03/2017	6/03/2017	7/03/2017
GMU1/2017 07 Mar 2017 - 4:53 PM - Ian Lyall Action completed by: Ian Lyall Noted. Appointment letter being drafted. 07 Mar 2017 - 4:50 PM - Ian Lyall Noted. Applicant has verbally accepted our offer. A letter of appointment is being drafted and will be sent w/c 13 March 2017.		Stephen Glen				

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 March 2017	Robert Maginnity	LG NSW Tourism Conference - Attendance Request	29/03/2017	6/03/2017	7/03/2017
GMU2/2017 125 07 Mar 2017 - 9:09 AM - Robert Maginnity Action completed by: Gina Radford Resolution noted		Robert Maginnity				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 March 2017	Martin Johnson	Council Submission to Planning Legislation Update	29/03/2017	6/03/2017	7/03/2017
PE6/2017 128 07 Mar 2017 - 4:35 PM - Martin Johnson Action completed by: Martin Johnson submission finalised and issued		Gareth Curtis				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Katrina Kerr	Minutes of Local Traffic Committee held 23 January 2017	15/03/2017	21/02/2017	9/03/2017
WI2/2017 116 09 Mar 2017 - 10:07 AM - Katrina Kerr Action completed by: Kristy Meyers Noted.		Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Geoffrey Bent	Councils Policy on Verge Maintenance	15/03/2017	21/02/2017	9/03/2017
Q15/2017 09 Mar 2017 - 10:03 AM - Geoffrey Bent Action completed by: Kristy Meyers Report being presented to Council 1/3/17.		Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Nicole Benson	Recreational Needs - Handball Courts	15/03/2017	21/02/2017	9/03/2017
Q16/2017 09 Mar 2017 - 10:04 AM - Nicole Benson Action completed by: Kristy Meyers Report being presented to Council 1/3/17.		Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Geoffrey Bent	Money Spent on Contractors for Roads	15/03/2017	21/02/2017	9/03/2017
Q18/2017 09 Mar 2017 - 10:03 AM - Geoffrey Bent Action completed by: Kristy Meyers Report being presented to Council 1/3/17.		Justin Fitzpatrick-Barr				

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
Q19/2017	Ordinary Meeting of Council 15 February 2017	Geoffrey Bent	Report to Council on the Failure of Mount View Road	15/03/2017	21/02/2017	9/03/2017
Justin Fitzpatrick-Barr 09 Mar 2017 - 10:04 AM - Geoffrey Bent Action completed by: Kristy Meyers Report being presented to Council 1/3/17.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
W14/2017 129	Ordinary Meeting of Council 01 March 2017	Nicole Benson	Branxton Pool - Heating Upgrade	30/09/2017	6/03/2017	9/03/2017
Justin Fitzpatrick-Barr 09 Mar 2017 - 11:23 AM - Nicole Benson Action completed by: Kristy Meyers Report presented to 1/3/17 Council meeting.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
Q21/2017	Ordinary Meeting of Council 01 March 2017	Michael Alexander	Green waste fees	29/03/2017	6/03/2017	9/03/2017
Justin Fitzpatrick-Barr 09 Mar 2017 - 10:01 AM - Michael Alexander Action completed by: Kristy Meyers Report being presented to Council 15/3/17.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
Q23/2017	Ordinary Meeting of Council 01 March 2017	Geoffrey Bent	Cinema car park	29/03/2017	6/03/2017	9/03/2017
Justin Fitzpatrick-Barr 09 Mar 2017 - 9:41 AM - Geoffrey Bent Action completed by: Kristy Meyers Report being presented to Council 15/3/17.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
Q24/2017	Ordinary Meeting of Council 01 March 2017	Geoffrey Bent	Roadwork completion by contractors	29/03/2017	6/03/2017	9/03/2017
Justin Fitzpatrick-Barr 09 Mar 2017 - 9:41 AM - Geoffrey Bent Action completed by: Kristy Meyers Report being presented to Council 15/3/17						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
Q25/2017	Ordinary Meeting of Council 01 March 2017	Katrina Kerr	Painting of gateway sign at Laguna	29/03/2017	6/03/2017	9/03/2017
Justin Fitzpatrick-Barr 09 Mar 2017 - 9:42 AM - Katrina Kerr Action completed by: Kristy Meyers Report being presented to Council 15/3/17.						

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 March 2017	Michael Alexander	Container Collection Scheme	29/03/2017	6/03/2017	9/03/2017
Q27/2017		Justin Fitzpatrick-Barr				
09 Mar 2017 - 10:01 AM - Michael Alexander Action completed by: Kristy Meyers Report being presented to Council 15/3/17.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Justin Fitzpatrick-Barr	Stomp Festival - 23 April 2017	12/04/2017	20/03/2017	20/03/2017
W115/2017 141		Justin Fitzpatrick-Barr				
20 Mar 2017 - 10:55 AM - Justin Fitzpatrick-Barr Action completed by: Kristy Meyers Actions to be carried out by Works Delivery, Environment & Waste Services and Design Delivery.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Nicole Benson	Cessnock Pensioner's Hall	12/04/2017	20/03/2017	20/03/2017
Q29/2017		Justin Fitzpatrick-Barr				
20 Mar 2017 - 4:57 PM - Nicole Benson Action completed by: Nicole Benson Answer prepared for 5 April Council meeting.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Nicole Benson	Cessnock Pensioner's Hall - status update	12/04/2017	20/03/2017	20/03/2017
Q36/2017		Justin Fitzpatrick-Barr				
20 Mar 2017 - 4:58 PM - Nicole Benson Action completed by: Nicole Benson Answer prepared for 5 April Council meeting.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 March 2017	Janine McCarthy	Section 96(1A) Application proposing to modify Development Consent 2016/216 which granted approval for a two lot subdivision. The Application seeks to delete the requirement to seal the Hall Street road shoulder	29/03/2017	6/03/2017	22/03/2017
PE4/2017 126		Gareth Curtis				
22 Mar 2017 - 8:56 AM - Janine McCarthy Action completed by: Janine McCarthy Determination notice issued.						

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Robert Maginnity	ABORIGINAL AND TORRES STRAIT ISLANDER ADVISORY COMMITTEE	12/04/2017	20/03/2017	23/03/2017
Q28/2017		Robert Maginnity				
<p>23 Mar 2017 - 2:30 PM - Robert Maginnity Action completed by: Annise Russell Report to Council meeting 5/04/2017.</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Bo Moshage	Cessnock Local Environmental Plan 2011 - City Wide Administrative Amendments - 2017	12/04/2017	20/03/2017	24/03/2017
PE9/2017 137		Gareth Curtis				
<p>24 Mar 2017 - 4:12 PM - Bo Moshage Action completed by: Bo Moshage Planning Proposal sent to DoP&E seeking Gateway Determination 23 March 2017</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 March 2017	Gina Radford	Submission by Council to Senate Inquiry - Public Governance, Performance and Accountability (Location of Corporate Commonwealth Entities) Order 2016	1/03/2017	6/03/2017	27/03/2017
MM5/2017		Stephen Glen				
<p>27 Mar 2017 - 1:08 PM - Gina Radford Action completed by: Robert Maginnity Submission lodged 27 Mar 2017 - 1:06 PM - Robert Maginnity Submission lodged & acknowledged.</p> <p>From: Committee, FPA (SEN) [mailto:FPA.SEN@aph.gov.au] Sent: Friday, 17 March 2017 4:55 PM To: Sandra Richardson Subject: Acknowledgement of Submission: Inquiry into The operation, effectiveness, and consequences of the Public Governance, Performance and Accountability (Location of Corporate Commonwealth Entities) Order 2016</p> <p>THE SENATE STANDING COMMITTEE ON FINANCE AND PUBLIC ADMINISTRATION</p> <p>Mr Robert Maginnity Acting General Manager Cessnock City Council</p> <p>Dear Mr Maginnity,</p> <p>Inquiry into The operation, effectiveness, and consequences of the Public Governance, Performance and Accountability (Location of Corporate Commonwealth Entities) Order 2016</p> <p>Thank you for the provision of your organisation's submission relating to the Committee's inquiry. The Committee will consider carefully all matters raised.</p>						

The submission will be published on the Committee's webpage. If you have any concerns about the publication of the submission, please contact me as soon as possible.

The Committee is required to report by 9 May 2017. A copy of the report will be loaded onto the Committee's website and may be accessed at http://www.aph.gov.au/Parliamentary_Business/Committees/Senate/Finance_and_Public_Administration.

Yours sincerely

07 Mar 2017 - 9:25 AM - Sandra Richardson

Submission being prepared by Economic Development Manager. To be distributed to Councillors prior to lodgement.

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Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 February 2017	Nicole Benson	Upgrade of Bellbird BMX Track	31/08/2017	3/02/2017	
BN1/2017 100	Councillor Olsen	Justin Fitzpatrick-Barr				
06 Feb 2017 - 11:29 AM - Nicole Benson ROSSP 2017 expected to be adopted by 30 June 2017. This report will follow.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 19 October 2016	Stephen Long	Traffic Flow	30/05/2017	24/10/2016	
BN10/2016	Councillor Gray	Justin Fitzpatrick-Barr				
06 Mar 2017 - 10:13 AM - Karen Burgess Traffic flow issues for Wollombi Road from Mount View Road to Vincent Street included in the Draft Traffic and Transport Strategy. Report to Council expected 5 April 2017 seeking public exhibition of the draft document. 29 Nov 2016 - 11:55 AM - Karen Burgess Councillor briefing held 23 November 2016. Draft document being revised to incorporate staff comments. Report to Council early 2017 seeking public exhibition of the document. 09 Nov 2016 - 2:32 PM - Kristy Meyers Councillor Briefing session has been scheduled for 23 November 2016, with a report to Council to follow seeking public exhibition of the document.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 19 October 2016	Stephen Long	Establishment of Commuter Car Park - Stanford Road, Heddon Greta	30/06/2017	24/10/2016	
BN12/2016 33	Councillor Olsen	Justin Fitzpatrick-Barr				
06 Mar 2017 - 10:17 AM - Karen Burgess Further RMS liaison meeting scheduled for 30 March 2017. 09 Nov 2016 - 2:35 PM - Kristy Meyers Discussed at quarterly meeting held with RMS on 2 November 2016. RMS advised that they do not normally provide commuter car parking space, however, investigation will be undertaken for possible short term measures. Longer term solutions and associated funding to be further investigated.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Natalie Drage	New Lower Hunter Hospital to be moved to Greenfield Site Hydro Aluminium	3/05/2017	7/04/2017	
BN14/2017 175	Councillor Dunn	Robert Maginnity				
07 Apr 2017 - 3:34 PM - Natalie Drage Action reassigned to Natalie Drage by: Gareth Curtis						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Jane Holdsworth	Maximising Economic Opportunities from the V8 Supercars in Newcastle	3/05/2017	7/04/2017	
BN15/2017 176	Councillor Suvaal	Stephen Glen				
10 Apr 2017 - 9:22 AM - Jane Holdsworth Action reassigned to Jane Holdsworth by: Yvonne Blake						

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 16 November 2016	Nicole Benson	Weston Junior Football Club Relocation from Varty Park to Weston Park	26/05/2017	18/11/2016	
BN17/2016 64	Councillor Sander	Justin Fitzpatrick-Barr				
<p>11 Apr 2017 - 4:05 PM - Nicole Benson Report is on 17 May agenda for Council's consideration. 22 Feb 2017 - 6:45 AM - Nicole Benson Report to go to Council meeting after the Strategic Property Review briefing scheduled for 22 March 2017. 02 Feb 2017 - 5:00 PM - Nicole Benson Information still being collated and report to be presented in March. 07 Dec 2016 - 7:46 AM - Nicole Benson Investigations underway and a report to be presented to Council in February 2017.</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 February 2017	Nicole Benson	Installation of Racquet Ball Court	23/06/2017	3/02/2017	
BN2/2017 101	Councillor Olsen	Justin Fitzpatrick-Barr				
<p>06 Feb 2017 - 11:32 AM - Nicole Benson Investigations underway and report to come back to Council at 7 June meeting.</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 14 December 2016	Wonona Fuzzard	Wollombi School	30/04/2017		
BN20/2016 88	Councillor Dunn	Gareth Curtis				
<p>24 Jan 2017 - 9:30 AM - Wonona Fuzzard Letters as required by points 3 and 4 of the Resolution where sent in December 2016 by Corporate Services. 24 Jan 2017 - 9:26 AM - Wonona Fuzzard A review of the process required to list the School as an item of national and state heritage listing has occurred. A quote from a qualified heritage expert is now being sort to determine the cost and time required to undertake the work.</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 14 December 2016	Stephen Long	Road Ranking	12/04/2017		
BN22/2016 89	Councillor Olsen	Justin Fitzpatrick-Barr				
<p>06 Mar 2017 - 10:18 AM - Karen Burgess Councillor Briefing scheduled for 12/4/17 03 Feb 2017 - 3:02 PM - Kristy Meyers Briefing has been scheduled for March 2017.</p>						

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Katrina Kerr	Corner Wollombi Road and Mount View Road Millfield - Tourist Directional Signage	19/04/2017	21/02/2017	
BN6/2017 119	Councillor Stapleford	Justin Fitzpatrick-Barr				
23 Mar 2017 - 1:34 AM - Katrina Kerr Liaison with TASAC to occur ahead of sign design in accordance with Wine Country Signage Strategy. 09 Mar 2017 - 11:22 AM - Kristy Meyers Works Delivery require design and location for placement.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Katrina Kerr	Road Safety Improvements - East of Stonehurst Winery Wollombi Road Cedar Creek	30/06/2017	21/02/2017	
BN7/2017 120	Councillor Stapleford	Justin Fitzpatrick-Barr				
09 Mar 2017 - 10:20 AM - Kristy Meyers Item 1 - Being investigated Item 2 - Project included in draft 17-21 Delivery Program Item 3 - To be reported back to Council in 17/18 as per resolution.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Jane Holdsworth	Consideration of a Promotional Exhibition Stand for the Cessnock Local Government Area at the 2018 Sydney Royal Easter Show	12/04/2017	20/03/2017	
BN8/2017 145	Councillor Fagg	Stephen Glen				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Natalie Drage	Minutes of the Council Initiated Awards Committee Meeting held on 14 March 2017	3/05/2017	7/04/2017	
CC20/2017 166		Robert Maginnity				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Natalie Drage	Health and Well-being Plan 2017-2021	3/05/2017	7/04/2017	
CC21/2017 167		Robert Maginnity				
13 April 2017 – Natalie Drage Health and Well-being Plan 2017-2021 placed on public exhibition from 12 April 2017 – 10 May 2017.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Natalie Drage	Disability Inclusion Action Plan 2017-2021	3/05/2017	7/04/2017	
CC22/2017 168		Robert Maginnity				
13 April 2017 – Natalie Drage Disability Inclusion Action Plan 2017-2021 placed on public exhibition from 12 April 2017 – 10 May 2017.						

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Melanie Parker	2017 National General Assembly of Local Government - Consideration of Motions and Councillor Nominations to Attend	3/05/2017	7/04/2017	
CC23/2017 169 07 Apr 2017 - 11:40 AM - Melanie Parker Action reassigned to Melanie Parker by: Annise Russell		Stephen Glen				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 16 November 2016	Jane Holdsworth	Council's Role in Christmas - CBD Decorations and Council Christmas Appeal	1/02/2017	18/11/2016	
GMU15/2016 10 Jan 2017 - 12:13 PM - Kristy Meyers Lights were installed before Christmas and have been removed in the New Year. Remaining resolution items to be actioned and finalised by Economic Development.		Stephen Glen				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Jane Holdsworth	2018 Sydney Royal Easter Show - Exhibition Costs	3/05/2017	7/04/2017	
GMU5/2017 157		Stephen Glen				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Ian Turnbull	Standing Committee on the Environment and Energy Report on Flying-Foxes in the Eastern States	3/05/2017	7/04/2017	
PE10/2017 158		Gareth Curtis				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Bo Moshage	Conversion of Sheds into Dwellings in the Cessnock LGA	3/05/2017	7/04/2017	
PE11/2017 159		Gareth Curtis				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Kerry Porter	Section 96(1A) Application Proposing to Modify Development Consent 2011/129 which granted approval for Seven (7) Residential Units. The application seeks to modify the approved drainage design	3/05/2017	7/04/2017	
PE12/2017 160		Gareth Curtis	109-11 Allandale Road, Cessnock			

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 16 March 2016	Iain Rush	18/2015/7: Planning Proposal - 1443 Wine Country Drive Rothbury	31/05/2017	18/03/2016	
PE15/2016 1613		Gareth Curtis				
<p>09 Mar 2017 - 10:03 AM - Iain Rush Planning Proposal and Draft Planning Agreement to be placed on exhibition between 15 March 2017 and 12 April 2017. 08 Feb 2017 - 9:22 AM - Iain Rush Presently reviewing the proponent's draft Planning Agreement before exhibiting the Planning Proposal. 03 Aug 2016 - 3:41 PM - Iain Rush Awaiting further information from the proponent prior to completing agency consultation and exhibition. 01 Jun 2016 - 8:26 AM - Iain Rush Gateway determination received on 29 April 2016. Awaiting further information from the proponent prior to agency consultation and exhibition. 09 May 2016 - 3:26 PM - Iain Rush Gateway determination received on 29 April 2016. Post Gateway assessment commenced. 21 Mar 2016 - 2:05 PM - Iain Rush Planning Proposal referred to Department of Planning and Environment for Gateway Determination on 21 March 2016. Awaiting Gateway Determination.</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 06 April 2016	Ian Turnbull	Flying-foxes East Cessnock	1/05/2017	8/04/2016	
PE27/2016 1638		Gareth Curtis				
<p>06 Dec 2016 - 10:54 AM - Ian Turnbull Flying-fox Engage Survey Results received by Council staff- Camp Management Plan in development. 03 Aug 2016 - 4:15 PM - Ian Turnbull Items 1, 4, 5, 6 Completed. Items 2 and 3 in progress through the working group established to develop the Cessnock East Camp Management Plan.</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 01 March 2017	Keren Brown	Heritage listing process for Wollombi Public School (former)	19/04/2017	6/03/2017	
PE5/2017 127		Gareth Curtis				
<p>07 Apr 2017 - 8:39 AM - Keren Brown Report deferred for at least 6 week, to be reported to Council Meeting on 19 April 2017. 07 Mar 2017 - 4:36 PM - Keren Brown Action reassigned to Keren Brown by: Martin Johnson</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 03 August 2016	Bronwyn Rumbel	Strategic Property Review - Phase 2 Progress Report - Part 2	31/12/2017	5/08/2016	
PE56/2016 1766		Gareth Curtis				
<p>29 Mar 2017 - 11:50 AM - Bronwyn Rumbel In February 2017 Council's Senior Property Officer, while searching the archives for other information, came across this LEP amendment (from 2003) which reclassified the Pensioner Hall site in North Avenue (from community to operational land). Council's property system has now been updated to reflect this classification. 11 Oct 2016 - 1:49 PM - Bronwyn Rumbel Reclassification process to be commenced in conjunction with the properties included in the report on Part 3 (of 4) of Phase 2 of the Strategic Property Review. The Part 3 report is scheduled to be considered by Council, following a Councillor Briefing, in 2017.</p>						

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 03 August 2016	Bronwyn Rumbel	Integrated Planning & Reporting Program - Proposed Engagement Strategy	30/06/2017	5/08/2016	
PE57/2016 1767		Gareth Curtis				
<p>06 Mar 2017 - 9:49 AM - Bronwyn Rumbel Councillor Briefing and Workshop held during February 2017. Preliminary Draft IP&R documents to be distributed to Councillors at briefing on 8 March 2017. 01 Dec 2016 - 1:56 PM - Bronwyn Rumbel Biennial community research undertaken in May-June 2016. Flyers distributed at Seniors' Week and Microchip Day. Consultation undertaken at Spring Awakening in September 2016. On-line engagement tool yoursay.cessnock.nsw.gov.au launched in November 2016. Promotion and flyers made available at Council's administration building, Cessnock Performing Arts Centre, Cessnock and Kurri Kurri libraries, Cessnock and Branxton swimming pools. Listening posts to be held at Cessnock and Kurri Kurri libraries during December 2016.</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 17 February 2016	Bronwyn Rumbel	Strategic Property Review - Phase 2 Progress Report - Part 1	31/12/2017	19/02/2016	
PE6/2016 1555		Gareth Curtis				
<p>06 Mar 2017 - 9:59 AM - Bronwyn Rumbel Councillor Briefing scheduled for 22 March 2017. 11 Oct 2016 - 9:33 AM - Bronwyn Rumbel Part 2 (of 4) report on the outcomes of Phase 2 of the Strategic Property Review was considered by Council on 3 August 2016. This viability of this project may be impacted by the proposed expansion of Cessnock Correctional Centre. Estimated completion date amended to determine potential impacts and for briefing of newly elected Council. 12 Apr 2016 - 9:39 AM - Bronwyn Rumbel A provision of \$75,000 (funded from the Property Investment Fund) has been included in the March Budget Review. The Executive has determined that further reports (parts 2-4) on the progress of Phase 2 of the Strategic Property Review be considered by Council following the adoption of the Community Infrastructure Strategic Plan (currently scheduled for June 2016).</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 18 November 2015	Bo Moshage	Planning Proposal - Hydro Kurri Kurri	31/05/2017	20/11/2015	
PE89/2015		Gareth Curtis				
<p>24 Mar 2017 - 4:14 PM - Bo Moshage Resolution 1, 2 & 3 completed.</p> <p>Resolution 4 to 10 under investigation</p> <p>23 Jan 2017 - 9:29 AM - Bo Moshage Council is working in consultation with MCC and the Proponent to prepared and finalise the supporting documents outlining in the Planning Proposal critical steps and pathways as they relate to Resolution 4 to 10 29 Nov 2016 - 9:23 AM - Bo Moshage Council is working in consultation with MCC and the Proponent to prepared and finalise the supporting documents outlining in the Planning Proposal critical steps and pathways as they relate to Resolution 4 to 10 03 Aug 2016 - 11:47 AM - Bo Moshage In consultation with MCC and the Proponent, Council has finalised a draft project plan outlining the critical steps and pathways in undertaking the Planning Proposal in relation to Resolution 4 to 10. 08 Jul 2016 - 2:45 PM - Bo Moshage On Going meets with the DoPE, MCC and Proponent to discuss the Gateway Conditions. 09 May 2016 - 4:19 PM - Bo Moshage Following the Gateway Determination Council has met with the DoPE and MCC to discuss the Gateway Conditions. Both CCC and MCC have met with the RMS to discuss traffic implications. Meetings with the OEH to</p>						

discuss flooding and biodiversity considerations are anticipated to occur within the next week

11 Apr 2016 - 9:03 AM - Bo Moshage

Gateway received by Council 23/3/16 and currently being reviewed with meeting with MCC and DoP&E to follow to give effect to Resolutions 4 to 10

05 Feb 2016 - 2:15 PM - Bo Moshage

Gateway Determination requested 27/11/15

05 Feb 2016 - 2:14 PM - Bo Moshage

Resolution 1, 2 & 3 completed.

Resolution 4 to 10 under investigation

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 09 December 2015	Iain Rush	18/2015/1: Joint Planning Proposal - Cessnock and Singleton Councils - Vineyards District	10/12/2017		
PE99/2015		Gareth Curtis				
<p>08 Feb 2017 - 9:23 AM - Iain Rush</p> <p>The Proposal will be informed by the findings of the Vineyards District Study. To allow this to occur, the Department of Planning and Environment has extended the timeframe to complete the Planing Proposal to 10 December 2017.</p> <p>03 Aug 2016 - 11:14 AM - Iain Rush</p> <p>The Joint Planning Proposal is interrelated with the Vineyards District Study component of the City Wide Planning Strategy and its findings. A meeting occurred with several vineyards peak bodies on 29 July 2016 to discuss proposed amendments to land uses in the RU4 Zone in Cessnock and Singleton and in light of the broader preliminary findings of the Vineyards District Study.</p> <p>11 Jul 2016 - 10:25 AM - Wonona Fuzzard</p> <p>Pre exhibition consultation is currently occurring with peak bodies and Singleton Council. This will conclude at the end of July 2016.</p> <p>21 Mar 2016 - 2:09 PM - Iain Rush</p> <p>Gateway Determination issued on 3 March 2016. Consultation commenced with vineyards 'peak bodies' on 14 March 2016.</p> <p>04 Mar 2016 - 12:14 PM - Iain Rush</p> <p>Request for Gateway determination forwarded to DoPE on 15 December 2016. Council is still awaiting the determination.</p> <p>02 Feb 2016 - 11:30 AM - Iain Rush</p> <p>Request for Gateway determination forwarded to DPE on 15 December 2016.</p> <p>28 Jan 2016 - 8:56 AM - Iain Rush</p> <p>Action reassigned to Iain Rush by: Robyn Larsen</p>						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Martin Johnson	Planning Proposal 49B Aberdare Road, Aberdare	3/05/2017	7/04/2017	
RM1/2017 153		Gareth Curtis				
<p>07 Apr 2017 - 3:35 PM - Martin Johnson</p> <p>Action reassigned to Martin Johnson by: Gareth Curtis</p> <p>07 Apr 2017 - 10:44 AM - Martin Johnson</p> <p>Action reassigned to Gareth Curtis by: Gina Radford</p>						

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Geoffrey Bent	Richmond Vale Rail Museum	19/04/2017	21/02/2017	
W11/2017 115	23 Feb 2017 - 10:58 AM - Geoffrey Bent	Justin Fitzpatrick-Barr				
Natalie Drage has been requested to provide details of when and where works are required.						
23 Feb 2017 - 10:03 AM - Kristy Meyers						
For Works Delivery to arrange delivery of material via liaison with C&CS (Nat Drage) to discuss with Richmond Vale board member for delivery date and location.						
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Stephen Long	Swamp Creek Floodplain Risk Management Plan - Property Flood Management Plan	12/04/2017	20/03/2017	
W116/2017 142		Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Stephen Long	Black Creek Floodplain Risk Management Study and Plan - Property Flood Management Plan	12/04/2017	20/03/2017	
W117/2017 143		Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 March 2017	Stephen Long	Draft Voluntary House Raising Policy	12/04/2017	20/03/2017	
W118/2017 144		Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Stephen Long	Wine Country Drive Median Maintenance (Huntlee)	3/05/2017	7/04/2017	
W120/2017 171		Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 05 April 2017	Stephen Long	Restart NSW Regional Growth - Environment and Tourism - Expression of Interest Guidelines	3/05/2017	7/04/2017	
W122/2017 173		Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 15 February 2017	Stephen Long	Building Better Region Fund	19/04/2017	21/02/2017	
W13/2017 117		Justin Fitzpatrick-Barr				
06 Mar 2017 - 10:20 AM - Karen Burgess						
2 approved Grant funding applications prepared and submitted prior to closing time and date.						

Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 16 November 2016	Paul Burton	Cessnock Aquatic Centre Feasibility & Design Report	26/05/2017	18/11/2016	
WI49/2016 49	09 Jan 2017 - 1:04 PM - Nicole Benson Briefing scheduled for 10 May 2017.	Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 14 December 2016	Katrina Kerr	Pedestrian Crossing - Keene Street, Cessnock	19/04/2017		
WI53/2016 83	22 Mar 2017 - 11:22 PM - Katrina Kerr Concept Design complete. Final Design in preparation. 09 Mar 2017 - 10:18 AM - Kristy Meyers Design in progress. 23 Feb 2017 - 9:34 AM - Kristy Meyers To Design Delivery for inclusion in 3rd Quarter Delivery Program Review and to arrange installation of required works.	Justin Fitzpatrick-Barr				
Type	Meeting	Officer/Director	Subject	Est. Compl.	Emailed	Completed
	Ordinary Meeting of Council 14 December 2016	Stephen Long	Cessnock Correctional Centre Expansion Access Options	17/05/2017		
WI57/2016	06 Mar 2017 - 10:19 AM - Karen Burgess Further meeting with Corrective Services scheduled for 7/3/2017. 24 Jan 2017 - 8:18 AM - Robyn Larsen Copy of Council's resolutions and report provided to Corrective Services NSW.	Justin Fitzpatrick-Barr				

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Report No. WI56/2015

Works and Infrastructure



SUBJECT: RICHMOND VALE RAIL TRAIL

RESPONSIBLE OFFICER: Recreation Services Manager - Nicole Benson

SUMMARY

The Richmond Vale Rail Trail (RVRT) is a proposed thirty four (34) kilometre shared cycleway from Kurri Kurri to Hexham.

The proposed cycleway utilises a former rail corridor and traverses the Local Government Areas (LGA's) of Cessnock, Lake Macquarie and Newcastle. A Project Committee, consisting of Council representatives from each of the aforementioned LGA's, along with other government and community representatives, has coordinated the preparation of the Richmond Vale Rail Trail Feasibility Analysis (Analysis).

The purpose of this report is to provide Council with an update on the outcomes of the Analysis and to seek Council's endorsement to participate in the undertaking of an environmental impact assessment and design for the proposed cycleway. A copy of the Analysis' Executive Summary is included in Enclosure 1.

RECOMMENDATION

1. That Council acknowledges and endorses the recommendations of the Analysis provided by Mike Halliburton Associates dated December 2014;
2. That Council supports ongoing involvement in the project and commits a further contribution of \$30,000 towards the environmental impact assessment and concept design phase of the project;
3. That upon completion of the next stage of the project, Council receives a further report on completed works and considers progressing to the next development stage.

BACKGROUND

At its meeting on 20 March 2013, Council was notified of the successful expression of interest to the Regional Development Australia Fund – Round 4 and resolved to:

1. *Note the success of the Regional Development Australia Fund Round 4 Expression of Interest for the Richmond Vale Rail Trail.*
2. *Write to relevant State and Federal Ministers seeking their in-principle support for this activity.*
3. *Endorse progressing to a Full Application as 'Project Lead' in partnership with Lake Macquarie City Council, Newcastle City Council, Roads and Maritime Services, National Parks and Wildlife Services and Coal & Allied with support from Maitland City Council, Port Stephens Shire Council, local community organisations and cycling bodies.*

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After receiving a further report at its 3 April 2013 meeting informing of the inability to proceed to Regional Australia Development Fund – Round 4 Full Application, Council resolved to:

- 1. Note the information contained in this report with respect to the inability of the Partnership with Lake Macquarie City Council, The City of Newcastle, Roads and Maritime Services, National Parks and Wildlife Services and Coal & Allied to proceed to Regional Development Australia Fund Round 4 Full Application for the Richmond Vale Rail Trail project.*
- 2. Endorse the establishment of a 'Richmond Vale Rail Trail Project' to formerly progress a feasibility study to increase the 'project readiness' of this initiative as a stakeholder with Lake Macquarie City Council, The City of Newcastle, Roads and Maritime Services, National Parks and Wildlife Services, Coal & Allied, local community organisations and cycling bodies with support from Maitland City Council and Port Stephens Shire Council.*

REPORT/PROPOSAL

A Rail Trail is a term generally used to define a multi-use recreation trail running on a disused rail corridor for non-motorised recreation and commuting. There are over forty (40) established rail trails in Australia with three (3) established in the Hunter region; these being the Fernleigh Track – (Adamstown to Belmont), the recent Glendale to Wallsend Cycleway and the Fassifern to Toronto Rail Trail. A feasibility study has been commissioned by the Upper Hunter Shire Council for the Upper Hunter Rail Trail between Merriwa and Denman.

The proposed RVRT route traverses land owned mostly by Coal & Allied, as well as State government agencies, local Councils and private landholders. The northern trailhead is proposed to be located at Log of Knowledge Park, Kurri Kurri. The proposed trail utilises the rail corridor through Werakata State Conservation Area before joining another rail corridor (that runs adjacent to the Richmond Vale Rail Museum) west of Wallis Creek Bridge, Buchanan. The proposed trail then continues along the rail corridor, passing under George Booth Drive, crossing Surveyors Creek Bridge and continuing east passing under the Hunter Expressway within the Lake Macquarie LGA and the M1 Pacific Motorway within the Newcastle LGA. Once east of the M1, the trail would continue along the northern boundary of the 'Tank Paddock' at Minmi and then onto Hexham. The proposed trail then continues south along the pipeline corridor at Hexham into Shortland. Trail spurs are proposed to connect with Minmi, Tarro and the Wetland Centre at Shortland. A map of the proposed trail route is included in Enclosure 2.

In order to determine if there was value in progressing the project an Analysis was completed by Mike Halliburton Associates in December 2014. The objectives of the Analysis were to:

1. Identify and evaluate the social, economic, environmental benefits associated with the project, including opportunities for community involvement.
2. Identify opportunities and constraints impacting the feasibility of the project and detail the works necessary to establish the rail trail for optimum route alignment, including restoration of the existing bridge and tunnel infrastructure.

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3. Identify any relevant regulations and/or legislation pertaining to the ownership, construction, use and maintenance of the rail trail including an assessment of the Austroads Guidelines.
4. Identify design requirements most suited to the target user and route alignment in accordance with applicable standards.
5. Estimate project development costs including associated bridge and tunnel structures along the route with recommendations for staging of construction.

The Analysis considered a range of factors, including the ability to identify a viable trail route, the potential for positive user experience, the commercial viability of the trail, the level of support from key stakeholders and the affordability of constructing and maintaining the trail. In considering these factors, it is recommended that the project proceed subject to the following conditions being met:

1. The project group has discussions and negotiations with the three key landholders to ensure that the trail corridor can be secured. These landholders are Black Hill Land Pty Ltd (for both the main trail and the spur into Minmi), Hunter Water and Stearling Property Group Pty Ltd (for the spur into Minmi).
2. The project group continue negotiations with Coal & Allied.
3. The project group continue to liaise with a private owner of part of the corridor east of the Werakata State Conservation Area.
4. Broader community consultation be undertaken with respect to the outcomes of discussions identified in points 1, 2 and 3 above.
5. The project group seeks funding from external sources for the construction of the proposed trail and detailed trail development plan.

It is envisaged that the conditions outlined above would be addressed at various stages throughout the project. The next project phase is 'trail development' which aims to identify a precise route of the proposed trail, identify construction techniques and materials, provide reliable cost estimates and detailed works lists, identify signage requirements and costs and provide trail inspection and maintenance schedules.

The key project milestones are illustrated in the figure below.

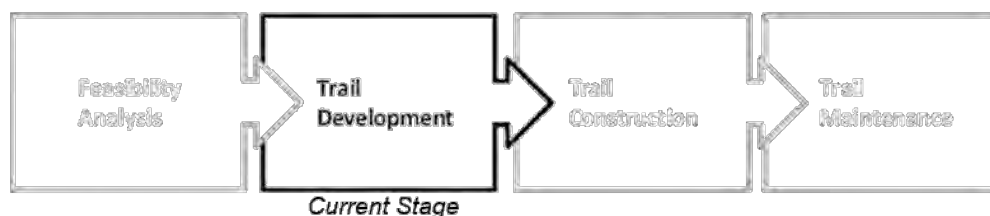


Figure 1 – Key Project Milestones

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The works required to complete the 'trail development' phase are substantial and therefore considered best undertaken in stages. The first stage of 'trail development' is the focus of this Council report. It includes preparing the concept design of the recommended trail alignment identifying required environmental impact assessment, preliminary construction cost estimates, planning approval and documentation, securing access for future development and operation of the trail.

A project working group, including representatives from Newcastle City Council, Cessnock City Council and Lake Macquarie City Council is proposing to coordinate the 'trail development' phase. Newcastle City Council is proposing to be the project lead and formal governance arrangements with Cessnock City Council and Lake Macquarie City Council will be determined upon confirmation of adequate funding being secured to complete the first stage of trail development.

The first stage of 'trail development' is anticipated to commence towards the end of 2015. Whilst the completion date is currently unknown, the following milestones are identified:

1. Contact relevant state agencies to advise of project inception.
2. Establish formal governance arrangements with Council stakeholders.
3. Obtain planning assessment requirements and prepare application for Secretary's Environmental Assessment Requirements.
4. Prepare tender specifications for the environmental impact statement and concept design preparation.

HOLD POINT – Project working group to review tender specification.

5. Project working group to assess tender submissions.

HOLD POINT – Project working group to review environmental impact statement and concept designs.

6. Public exhibition of environmental impact statement and concept design.

HOLD POINT – Project working group review of revised environmental impact assessment and concept design.

7. Submission of final environmental impact assessment and concept design to the Department of Planning and Environment.

Upon completion of the first stage of 'trail development', the project would be adequately progressed to finalise tenure arrangements and complete detailed design.

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OPTIONS

The following options are proposed for Council's consideration:

1. Council acknowledges and endorses the recommendations of the Analysis and supports the ongoing involvement of Council staff as well as a monetary contribution of \$30,000 towards the environmental impact assessment and concept design stage of the proposed 'trail development'. This is the preferred option as without Council's full support the project may not proceed;
2. Council acknowledges and endorses the recommendations of the Analysis and supports the ongoing involvement of Council staff in the planning and design of the trail but does not commit a monetary contribution towards the project;
3. Council acknowledges and endorses the recommendations of the Analysis but does not support the ongoing involvement of Council staff or commit a monetary contribution towards the planning and design of the trail;
4. Council does not endorse the recommendations of the Analysis.

CONSULTATION

The RVRT Planning Committee that coordinated the Analysis consisted of representatives from:

- Cessnock City Council
- Lake Macquarie City Council
- Newcastle City Council
- Newcastle Cycleways Movement
- National Parks and Wildlife Services
- Rio Tinto (Coal & Allied)
- Roads and Maritime Services
- Tom Farrell Institute

In the preparation of the Analysis the following additional stakeholders were consulted:

- Select private land owners adjoining the proposed trail.
- Representatives from the Richmond Vale Rail Museum.

There are a number of stakeholders that were not consulted as part of the Analysis. A more comprehensive consultation process is proposed as part of the next phase of the project and these stakeholders would be consulted at this later stage.

STRATEGIC LINKS

a. Delivery Program

The Project is consistent with Council's Delivery Program *Item 1.1.4 Commence implementation of the Cessnock City Bicycle Plan.*

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b. Other Plans

The Project is linked to the following strategic documents:

- Cessnock Bicycle Plan 1995
- Recreation & Open Space Strategic Plan 2009
- Hunter Regional Transport Plan 2014
- Cessnock Economic Development Strategy 2014
- Draft Cessnock Cycling Strategy 2015

IMPLICATIONS

a. Policy and Procedural Implications

N/A

b. Financial Implications

Funding for the Analysis was provided by Coal & Allied.

Based on estimates provided by Newcastle City Council, the first stage of 'trail development' will cost in the order of \$250,000. The overall total cost for stage 2 of trail development, including detailed design, is estimated to be in the vicinity of \$1 Million.

Newcastle City Council and National Parks & Wildlife Services have confirmed a contribution of \$130,000 and \$80,000 respectively towards completing the first stage of 'trail development'. Contributions from Lake Macquarie City Council and Coal & Allied are unconfirmed.

Should the preferred option be adopted, Council would be committing a contribution of \$30,000, which would be funded from the 2015/16 Infrastructure Forward Planning budget. Additionally, Council staff resources will also be committed to participate in project coordination meetings and general project duties.

The contribution of \$30,000 was determined to be appropriate by Council staff in consideration of the scale, nature and potential benefit of the project.

The cost of construction is currently estimated to be in the vicinity of \$10 Million for compacted earth/gravel finish or \$14 Million for bitumen/concrete finish, however, more detailed cost estimates will be prepared during the next stage of 'trial development'.

c. Legislative Implications

Preliminary planning advice indicates that the project will be designated development due to part of the alignment impacting SEPP14 Wetlands. It therefore requires an Environmental Impact Statement.

All works are to be undertaken in accordance with *the Environmental Planning and Assessment Act 1979*, *the Local Government Act 1993* and other relevant legislation.

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d. Risk Implications

There are a number of risks that will be managed and addressed throughout the trail development phase. Whilst not all risk can be eliminated, measures can be put in place to reduce the impact or likelihood of identified risks. The most significant risks typical to a project of this nature and scale include:

- Not being able to secure land tenure with various land owners.
- Lack of support or initiative from key stakeholders.
- Technical detail not of a standard sufficient to progress the project to further stages.
- Environmental constraints causing significant increases to estimated construction costs.

Due to funding not being secured for the entire cost of the project, there is a risk that the remaining stages of the project will not proceed. Whilst this is a possible outcome, the likelihood of attracting opportunistic funding would increase as the project progresses and the potential cost benefit of the project outweighs the contribution required.

e. Other Implications

Benefits of collaboration

The works completed to date have been the result of collaborative approach between public and private stakeholders. Adopting a collaborative approach has provided Council with access to a diverse range of skill sets and knowledge, additional staff and technological resources, improved relations between other private and public stakeholders and access to funding that otherwise would not be accessible. The success of the works to date indicate that a collaborative approach is suitable and as such is proposed for the next stage of the project. Ongoing involvement in the project will allow Council to continue to experience the benefits of the collaborative approach.

Support from the land owner(s)

Coal & Allied is the majority land owner along the route and as such this approach is significantly involved in the project through funding the Analysis and participating in coordination meetings. Without the support of the majority land owner, a project of this scale would be unfeasible. Whilst there are other private land owners that have an interest in the proposed alignment, consultation to date indicates a reasonable support for the project. Should the project not proceed, there is a risk that land may be developed or deemed unavailable. This would present significant constraints to the project and likely result in the cycleway project becoming unfeasible.

Return on investment

Whilst the initial estimated capital outlay of up to \$14 Million for construction may seem significant, there is evidence to suggest that the return on investment for a project of this scale is quite significant. As discussed in the Analysis, the estimated expenditure from local, day trip and overnight visitations would be in the vicinity of \$5.2 Million annually. The project also has potential to improve utilisation of existing and proposed cycling networks, thus improving the return on investment.

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There are also a number of less quantifiable returns on investment including health related economic benefits to the wider community, improved amenity and beautification, environmental and cultural benefits including providing opportunities for community group ownership and participation in environmental and cultural preservation, promotion and education. Data from the United States of America indicates that for every dollar spent on trails, the economy yields almost three times as much through direct health benefits.

Rail trails have also been recognised as major tourism generators. In Western Australia, the Mundaring Shire trails accommodate 2.45 Million visits annually, injecting \$12.62 Million into the local economy and a further \$15.21 Million into the State economy.

Opportunistic funding and partnerships

A number of Commonwealth and State funding programs may be suitable for the funding of the project. The NSW Regional Tourism Infrastructure Fund for example was announced for the 2014/15 State budget and included a program targeting rail trails. On 6 January 2015 the Minister announced that up to \$50 Million is available for pilot projects along sections of the disused Casino to Murwillumbah (Northern NSW) and Rosewood to Tumbarumba (South-Central NSW) rail corridors. The State Government has indicated that the RVRT is a suitable project and will be considered for funding once assessment of the 'pilot' projects are complete.

Other opportunities through private sponsorship, community based programs such as work for the dole and volunteer groups are all potential partnerships that would improve the viability of the project and deliver a favourable outcome for the community.

The construction of the trail is estimated to cost \$10 Million for unsealed treatment, such as compacted earth, or \$14 Million for sealed treatment, such as asphalt. In respect of securing adequate funding, it is critical that a project of this scale is progressed to detailed design, ready for construction, to ensure it is best positioned to attract opportunistic funding and interest from suitable project partners.

Active transport and living

Anticipated population growth and increased motor vehicle use in the LGA are concerns for Cessnock Council. The 2011 Census recorded an average motor vehicle ownership in Cessnock LGA of 1.8 per household and in some villages as high as 2.2. This is well above the NSW State average of 1.6 per house hold. Less reliance on motor vehicles would reduce the cost of living for residents and improve health.

The SEIFA index (score - 936.4) indicates that Cessnock experiences significant socio-economic disadvantage and medical statistics indicate Cessnock has above average instances of diabetes, high cholesterol, circulatory system diseases, respiratory diseases and obesity; all of which can be reduced through less reliance on motor vehicles by improving cycling infrastructure.

CONCLUSION

The Analysis recommends that the proposed project proceed in planned stages. This report has outlined the key findings of the Analysis and outlined the details of the next project stage for Council's consideration.

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There are a range of potential benefits of the project including improving the utilisation of existing and proposed cycling infrastructure throughout the Lower Hunter area. Council has an opportunity to participate in a project that will provide a unique recreational asset, that has potential to generate economic, social and environmental benefits for the region.

ENCLOSURES

- 1** Executive Summary
- 2** Concept Map

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The Hon Troy Grant MP
Minister for Police
Minister for Emergency Services



31 MAR 2017

IM17/4886, IM17/4272
MIN17/000407

Mr Clayton Barr MP
Member for Cessnock
cessnock@parliament.nsw.gov.au

Dear Mr Barr

Thank you for your letter on behalf of Mr Robert Maginnity, Acting General Manager of Cessnock City Council, concerning Weston Fire Station. Mr Maginnity also wrote to me directly in similar terms and I trust he will accept this response in reply to that letter.

The NSW Government is committed to ensuring that the Weston community and the Cessnock Local Government Area receives the best fire and emergency services available in times of need.

I am advised by Fire & Rescue NSW Commissioner Hamilton that recruitment of Retained Firefighters at Weston or other stations located in your electorate is currently occurring as required.

As Council is aware, Fire & Rescue NSW and the State Government recently constructed a new fire station at Abermain with the intention of relocating Weston resources to Abermain with no loss of capability occurring. Due to community concerns, it was determined that this relocation would not occur at this time. As a result, recruitment for Weston Fire Station will continue as planned and appropriate steps will be undertaken to ensure the station is staffed at full complement and provides the high levels of fire protection that the community expects.

Finally, I am aware that Acting Deputy Commissioner Byrne and the Area Commander for Metropolitan North, Brett Davies, will be meeting General Manager, Mr Stephen Glenn, and Council to discuss strategies, including recruitment, Fire & Rescue NSW has in place to ensure the community of Cessnock is provided with an appropriate level of service.

If Mr Maginnity wishes to discuss this matter further, he can contact Chief Superintendent Brett Davies on ---(02) 4979 3703 or email to Brett.Davies@fire.nsw.gov.au.

Yours sincerely

TROY GRANT MP
Minister for Emergency Services

29 MAR 2017

GPO Box 5341, SYDNEY NSW 2001
Phone: (61 2) 8574 6800 Fax: (61 2) 9339 5536 Email: www.nsw.gov.au/ministercontactform/minister-grant